



TOWN OF CHAPEL HILL NORTH CAROLINA

MEMORANDUM

TO: Town of Chapel Hill Commissions and Advisory Boards

FROM: Gene Poveromo, Development Manager
Judy Johnson, Principal Planner

SUBJECT: Application for Special Use Permit, Tri-City Medical Building, 5002 Barbee Chapel Road

Recommended Action

- That the Community Design Commission, Transportation and Connectivity Board, and Environmental Stewardship Advisory Board forward a recommendation to the Planning Commission and the Council; and that the Planning Commission also forwards a recommendation to the Town Council.

Explanation of Recommendation

- For this proposed development, the Council's Advisory Board Policy Manual requires that the Community Design Commission, Transportation and Connectivity Board, and Environmental Stewardship Advisory Board forward a recommendation to the Planning Commission for the Commission's consideration. The Land Use Management Ordinance requires that the Planning Commission forward a recommendation to the Council.

Project Description

- This Special Use Permit proposes to develop a 2.3-acre site, located at the southwest corner of NC 54 and Barbee Chapel Road. The proposal includes a 2-story office building with 60,000 sq. ft. of floor area and a 3-story parking structure with 240 spaces.
- The property is within the Meadowmont Development.
- Accompanying this application is an application to modify the Meadowmont Master Land Use Plan to change the use and increase the amount of allowable floor area.

Key Considerations

- *Modifying the Meadowmont Master Land Use Plan:* This proposal requires modifying the Meadowmont Master Land Use Plan. The proposed modification would increase the previously approved floor area as well as generate more traffic that initially planned for this particular site.
- *[NC 54/I-40 Corridor Study](http://www.nc54-i40corridorstudy.com/)¹:* The NC 54/I-40 Corridor Study, proposes, in the vicinity of Barbee Chapel Road and NC 54, a grade-separated interchange, as an alternative to the widening of Barbee Chapel Road. The proposed grade separation at Barbee Chapel would have a significant impact on this site.

¹ <http://www.nc54-i40corridorstudy.com/>

- [Durham-Orange Light Rail Project](#):² The light rail project, proposed to be operating in 2026, currently has proposed alignments near this property which may require significant changes to the proposed stormwater facility and the Marriott Way/Barbee Chapel Road connection.
- *NC 54 Buffer*: The Meadowmont Master Land Use Plan requires a 75 foot buffer along the NC 54 corridor. The developer is requesting a modification to allow selective thinning of existing trees and shrubs within the perimeter buffer.
- *Traffic Impacts*: The Traffic Impact Analysis completed for this project identifies traffic-related improvements necessary to address the impact of this project.
- *Concept Plan Review*: During the Community Design Commission and the Council review of the Concept Plan, some of the following topics were discussed: the appearance of the NC 54 buffer; designing around the future light rail alignments; providing a signature building and traffic impacts on NC 54

Background

- [March 19, 2007](#)³, [June 18, 2007](#)⁴: The Council reviewed a Concept Plan for Gateway Bank. The development included construction of a 12,000 sq. ft. bank and office building with drive-up windows.
- *April 16, 2014*: Concept Plan application reviewed by the Community Design Commission.
- [September 15, 2014](#)⁵: The Council reviewed a Concept Plan for construction of a two-story medical office building totaling 56,000 sq. ft. and three levels of parking with 240 spaces.
- *November 11, 2014*: Special Use Permit and Master Land Use Plan Modification applications submitted.

Fiscal Note

- This application was submitted prior to the [Council discussing a set of guidelines](#)⁶ for Fiscal Impact Analysis reports for Special Use Permit applications.

Council Goals

- Create A Place for Everyone
- Develop Good Places, New Spaces

Attachments

- Staff Report
- Resolution A Special Use Permit Approval
- Resolution B Special Use Permit Denial
- NC 54 Corridor Study Map

² <http://ourtransitfuture.com/projects/durham-orange/>

³ <http://townhall.townofchapelhill.org/agendas/2007/03/19/7/>

⁴ <http://townhall.townofchapelhill.org/agendas/2007/06/18/5/>

⁵ <http://chapelhillpublic.novusagenda.com/Bluesheet.aspx?itemid=2877&meetingid=358>

⁶ <http://chapelhillpublic.novusagenda.com/Bluesheet.aspx?itemid=2962&meetingid=308bb>

- Durham-Orange Light Rail Map
- Applicant Materials
- Applicant's response to Concept Plan Comments
- Traffic Impact Executive Summary
- Applicant's Fiscal Impact Analysis
- Area Map

STAFF REPORT

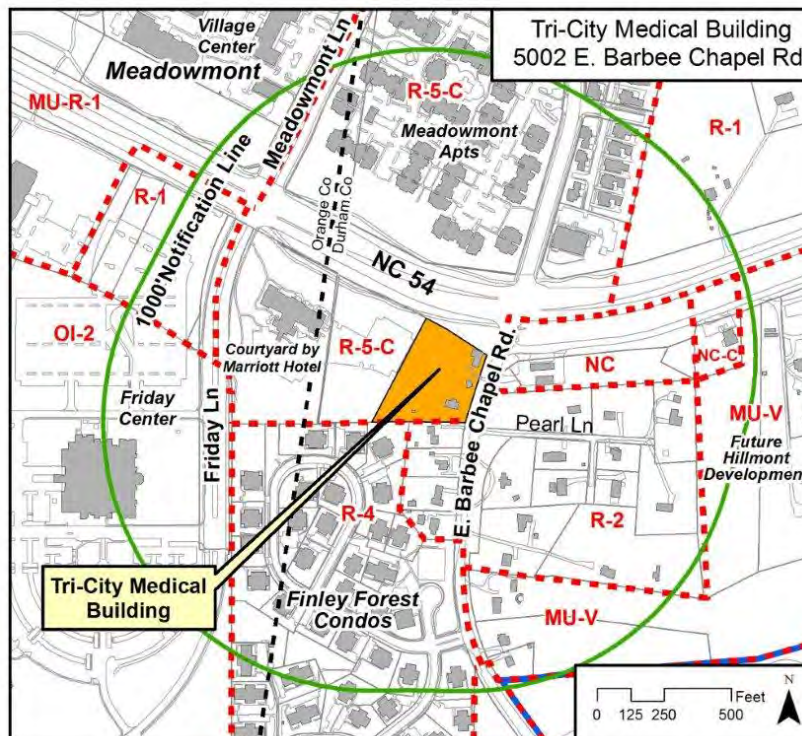
TO: Environmental Stewardship Advisory Board (June 9, 2015)
Transportation and Connectivity Advisory Board (June 23, 2015)
Community Design Commission (June 23, 2015)
Planning Commission (August 4, 2015)

SUBJECT: Application for Special Use Permit – [Tri-City Medical Building](#) ¹
5002 Barbee Chapel Road (Project No. 14-070)

DATE: June 9, 2015

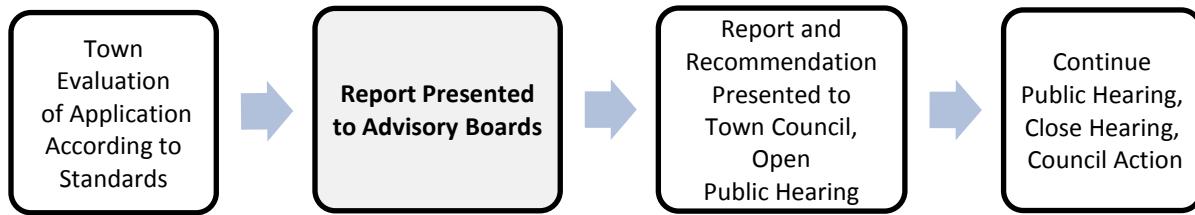
INTRODUCTION

Attached for your consideration is a Special Use Permit Application, submitted by Tri-City, Inc., which proposes to develop a 2.3-acre site located at 5002 Barbee Chapel Road. The proposed development is located in the southwest corner of the intersection of NC 54 and Barbee Chapel Road. The development includes two stories of offices, totaling 60,000 square foot, and three stories of parking, totaling 240 spaces. The property is within the Meadowmont Master Land Use plan and is zoned Residential-5-Conditional (R-5-C). The applicant is also proposing a change to the Meadowmont Master Land Use Plan to increase the amount of the allowable square footage.



¹ <http://www.townofchapelhill.org/town-hall/departments-services/planning-and-sustainability/gis-analytics/development-activity-report>

PROCESS



BACKGROUND

- | | |
|--------------------------------|---|
| March 19, 2007 & June 18, 2007 | The Town Council reviewed a Concept Plan for Gateway Bank. The development included construction of a 12,000 sq. ft. bank and office building with drive-up windows. |
| April 16, 2014 | Concept Plan application reviewed by the Community Design Commission. |
| September 15, 2014 | The Town Council reviewed a Concept Plan for construction of a two-story medical office building totaling 56,000 sq. ft. and three levels of parking with 240 spaces. |
| November 11, 2014 | Special Use Permit and Master Land Use Plan Modification applications submitted. |

KEY CONSIDERATIONS

We have identified several key considerations for the advisory boards and commissions to consider:

NC 54/I-40 Corridor Study: The NC 54/I-40 Corridor Study, adopted by the Durham-Chapel Hill-Carrboro Metropolitan Planning Organization although not adopted by the Town of Chapel Hill or the City of Durham, proposes significant improvements along this corridor. In the vicinity of Barbee Chapel Road and NC 54, a grade-separated interchange was been proposed as an alternative to the widening of Barbee Chapel Road to accommodate additional left turn lanes. (see attachment). This improvement has been proposed to be funded by the North Carolina Department of Transportation, in the State Transportation Improvement Program (STIP), for construction in fiscal years 2024-25.

Construction of the proposed interchange was not included in the 2040 Metropolitan Transportation Plan, adopted by the Durham-Chapel Hill-Carrboro Metropolitan Planning Organization. It is our understanding that projects cannot be funded through the STIP unless it is also included in the Metropolitan Transportation Plan. The proposed grade separation at Barbee

Chapel would have a significant impact on this site, as well as the recently built Learning Center in Meadowmont.

The Corridor Study identified several other recommendations for the Barbee Chapel Road and NC 54 including install crosswalks and pedestrian signals with pedestrian refuge islands as well as street lighting. Marriott Way was also recommended to become a public street within a public right-of-way and upgraded to NCDOT standards as well as be extended to Barbee Chapel Road, providing access between Friday Center Drive and Barbee Chapel Road. The proposed extension of Marriott Way is proposed to be constructed matching the existing dimensions.

Durham-Orange Light Rail Project: The light rail project, proposed to be operating in 2026, currently has proposed two alignments near this property – C2 Alternative, and C2A Alternative (see attached). C2 Alternative, along the southern property line would require significant changes to the proposed stormwater facility and the Marriott Way/Barbee Chapel Road connection. C2A Alternative, along the northern property line and adjacent to NC 54, would not impact the site design of this site but may conflict with the proposed road improvement project that includes grade separation.

Requests for Modification to Regulations: The Meadowmont Master Land Use Plan requires a 75 foot buffer along the NC 54 corridor. The Master Land Use Plan describes landscape buffers shall be provided with "windows" through the vegetation in accordance with the town's Entranceway Plan. The developer is requesting a modification to allow selective thinning of existing trees and shrubs within the perimeter buffer.

The developer is also requesting the Council consider a Modification to Regulation for an exception to the primary height limitation for the east property line along Barbee Chapel Road.

EXISTING CONDITIONS/DEVELOPMENT PROPOSAL SUMMARY

The Special Use Permit application is proposing the following changes with details indicated in the table below:

Site Description	
Address	5002 Barbee Chapel Road, Durham County
Property Description	Parcel 6A of Meadowmont; the parcel is 101,800 square feet (2.33 acres) and located in the southwest corner of the intersection of NC 54 and Barbee Chapel Road.
Durham County Parcel Identifier Number	9798-04-72-5824
Existing use and proposed Special Use Permit	Existing – two vacant single-family dwelling units and associated outbuildings. Proposed – five-story 60,000 sq. ft. medical office building with two stories of offices and three stories of parking.
Floor Area	The Meadowmont Master Land Use Plan allocates an office/commercial building of 19,200 sq. ft. A modification to the Meadowmont Master Land Use Plan application has been

	submitted to increase the size of the building to 60,000 sq. ft. Please see the accompanying application for additional information.
Height	Primary height is 39 feet and secondary height is 60 feet in Residential-5-Conditional (R-5-C) zoning district. Proposed height is a maximum of 60 feet.
Vegetation	The site is a wooded with a mix of hardwoods and pines.
Vehicle Parking	Minimum - 240 spaces; Maximum - 267 spaces; Provided - 240 spaces in three levels of parking
Bicycle Parking	Required - 52 spaces; Provided – 52 spaces.
Vehicular and Pedestrian Access	Existing – two gravel driveways to be removed; Proposed – Completion of Marriott Way from existing terminus to Barbee Chapel Road. Marriott Way at Barbee Chapel Road allows for right and left turns into the site but only right turns exiting the site. Connection to Marriott Way gives full access to Friday Center Drive.
Land Disturbance	87,100 sq. ft.
Impervious Surface	43,500 sq. ft.
Overlay Zone	None
Resource Conservation District overlay zone	Yes, in the southwest corner of site
Jordan Riparian Buffer overlay zone	None
Steep Slopes	None
Zoning	Residential-5-Conditional; Meadowmont Master Land Use Plan
Requested Modification to Regulations	
Height	Developer is requesting a maximum primary height of 60' on the east side of the proposed building facing Barbee Chapel Road.
Surroundings	
General Development Pattern	Meadowmont Mixed Use Development
North	Residential-5-Conditional (R-5-C) - Meadowmont Apartments
East	Neighborhood Commercial (NC) – Gas Station
South	Residential-1 (R-1) and Residential-4 (R-4) - Finley Forest Development and single-family dwellings
West	Residential-5-Conditional (R-5-C) – Marriott Hotel

STAFF ANALYSIS OF APPLICATION

Town staff has reviewed this application for compliance with the themes from the [2020 Comprehensive Plan](#)², the standards of the [Land Use Management Ordinance](#)³, and the [Design Manual](#)⁴, and offers the following evaluation:

² <http://www.ci.chapel-hill.nc.us/town-hall/departments-services/design-chapel-hill/chapel-hill-2020>

Comprehensive Plan: The following are themes from the 2020 Comprehensive Plan, adopted June 25, 2012:

Conforms	No.	2020 Comprehensive Plan Themes
√	1	A Place for Everybody
√	2	Community Prosperity and Engagement
√	3	Getting Around
√	4	Good Places, New Spaces
√	5	Nurturing Our Community
	6	Town and Gown Collaboration

We believe that the Tri-City Medical Building proposal complies with five of the six themes of the 2020 Comprehensive Plan. For information on how this proposed development addresses these themes and goals of the Comprehensive Plan, please refer to the attached applicant’s Statement of Justification.

Land Use Plan: The [2020 Land Use Plan](#)⁵, a component of the 2020 Comprehensive Plan, designates this site as commercial and also includes the site within Highway 54 future focus area.

Zoning: The property is zoned Residential-5-Conditional (R-5-C) and is encumbered by the Meadowmont Master Land Use Plan.

Setbacks: The street setbacks along NC 54 and Barbee Chapel Road are 20 feet. The internal setbacks are eight feet.

Height: Primary height limitations do not apply to lots internal to the master planned Meadowmont site. This would include the western property line adjacent to the hotel. The other property lines have a 39 foot primary height limit and a 60 foot secondary height limit. The building is proposed to be constructed at 60 feet, meeting the secondary height limitation. The applicant is requesting a Modification to Regulations for the primary height along the eastern property line. Please see the Modification to Regulations section of this memorandum for additional information.

Comparison of Concept Plan and Special Use Permit Proposals

	Concept Plan	Special Use Permit Application
Floor Area Proposed	70,000	60,000
Parking Spaces	304	240
Impervious Surface	71,260	43,500

³ <http://www.ci.chapel-hill.nc.us/town-hall/departments-services/planning-and-sustainability/resources/planning-library-plans-regulations-and-guidelines/chapel-hill-land-use-management-ordinance>

⁴ <http://www.ci.chapel-hill.nc.us/home/showdocument?id=2645>

⁵ <http://www.townofchapelhill.org/home/showdocument?id=1215>

PROPOSED MODIFICATION TO REGULATIONS

The applicant requests that the Council approve a modification to the Land Use Management Ordinance to allow a greater primary height. Additional modifications to the Meadowmont Master Land Use Plan are proposed and for additional information, please refer to the accompanying memorandum.

Council Findings and Public Purpose: The Council has the ability to modify the regulations, according to Section 4.5.6 of the Land Use Management Ordinance. We believe that the Council could modify the regulations if it makes a finding in this particular case that public purposes are satisfied to an equivalent or greater degree. If the Council chooses to deny a request for modifications to regulations, the applicant's alternative is to revise the proposal to comply with the regulations.

For additional information on the proposed modification, please refer to the applicant's attached materials.

Transportation

Traffic Impact: A Traffic Impact Study was prepared for the Town by HNTB North Carolina, PC (Executive Summary attached). The study area includes two signalized intersections, NC 54 and Barbee Chapel Road and NC 54 and Friday Center Drive as well as three unsignalized intersections. The study assumed that the project would be completed by 2017.

Weekday Trip Generation Summary

Trip Generation Statistics	Daily	AM Peak	Noon Peak	PM Peak
ITE Trips	1,063	135	155	174

Recommended Improvements: Based on traffic capacity analyses for the 2017 design year and analyses of the existing study area, the following improvements are recommended by the TIA and the NCDOT as being necessary:

- Barbee Chapel Road and NC 54 – install a concrete median from NC 54 to just north of the existing service station driveway.
- Barbee Chapel Road and Marriott Way – Construct an exclusive northbound left-turn lane with a minimum of 125 feet of full width storage and appropriate taper on Barbee Chapel Road.
- Friday Center Drive and Marriott Way – Construct an exclusive southbound left-turn lane with a minimum of 125 feet of storage and appropriate taper on Friday Center Drive
- Marriott Way and Site Driveway – the site driveway shall allow full access movement and consist of a two-lane cross-section; the site driveway shall have stop sign control with adequate sight distance; the site driveway shall have a minimum of 100 feet of storage before a parking or crossing maneuvers occur.
- Retime the two studied signalized intersections, NC 54 and Barbee Chapel Road and NC 54 and Friday Center Drive, to optimize overall capacity.

Resolution A includes stipulations for these proposed improvements.

Vehicle Access: The proposal includes completion of Marriott Way providing access to east and west of the site.

Cross Access Easement: Prior to issuance of a Zoning Compliance Permit, we recommend that the applicant provide a cross access easement along Marriott Way, a private street.

Vehicle Parking: The proposal includes three levels of parking decks with the capacity to hold 240 vehicles.

Landscape and Architecture

Landscape Bufferyards: The Land Use Management Ordinance and the Meadowmont Master Land Use Plan require the following landscape bufferyards:

Location	Required Buffer	Proposed Buffer
East – Barbee Chapel Road	50 foot	50 foot
North – NC 54	75 foot	75 foot
West – Marriott Hotel	Not required	10 foot Type B
South – Single-family and Finley Forest Complex	20 foot	8 foot Alternative

Building Elevations: Prior to issuance of a Zoning Compliance Permit, the developer shall obtain Community Design Commission approval for building elevations, lighting, and any alternate buffers, including special consideration of the location and screening of HVAC.

Environment/Stormwater Management

Stormwater Management: To address the stormwater requirements of the Land Use Management Ordinance, the developer is proposing a bio-retention pond on the southern portion of the site. A total of 43,560 square feet of impervious surface is proposed (43 percent of the site). A total of 87,100 square feet of land disturbance is proposed.

Energy Management Plan: Although an Energy Management Plan is not required of this project, we encourage the applicant to submit a plan that highlights efficiency and sustainability features.

Additional Stipulations in Resolution A (see Resolution A for detailed requirements)

Accessibility Requirements	Prior to issuance of a Certificate of Occupancy, the developer shall provide the minimum required handicapped facilities and infrastructure required by the Americans with Disability codes and standards.
Off-Site Construction Easements	Prior to land disturbing activities on adjacent properties associated with construction, the developer shall provide documentation of approval from property owners affected by the off-site construction, if necessary.
Landscape Protection	We have included our standard stipulation in approval of a

	Landscape Plan, including screening of parking areas and buildings (as required) as well as a maintenance schedule, and canopy trees.
<i>Public Art</i>	We recommend that the developer work with the Town's Public Arts Officer.
<i>Erosion Control</i>	Prior to final authorization to begin land disturbing activities, the developer shall be required to provide a performance bond in accordance with Section 5-97.1 Bonds of the Town Code of Ordinance if more than one acre of land is disturbed. Prior to approval of a Zoning Compliance Permit, the developer shall provide a copy of the approved erosion and sedimentation control permit from Orange County Erosion Control Division.
<i>Construction Phasing</i>	We recommend that the applicant submit a Phasing Plan with the Final Plans Zoning Compliance Permit application, if desired. The phasing plan shall detail when public improvements and stormwater management structures shall be completed and inspected as part of that phase prior to requesting a Certificate of Occupancy.
<i>Fire</i>	Prior to the issuance of a Certificate of Occupancy, the applicant shall be required to obtain Fire Marshal approval for required fire safety features and infrastructure, such as fire hydrants, fire department connections, detailed fire access elements, fire protection devices, heavy-duty pavement, and drive aisle design features. Prior to issuance of a Zoning Compliance Permit, design is subject to Fire Marshal approval.
<i>Lighting Plan</i>	Prior to issuance of a Zoning Compliance Permit, the developer shall submit site plans and other required documents to satisfy the lighting requirements of Section 5.11 of the Land Use Management Ordinance as modified.
<i>Overhead Power Lines</i>	We have included the standard stipulation that all proposed or relocated utility lines comply with Section 5.12.2 of the Land Use Management Ordinance.
<i>Refuse Management</i>	Prior to issuance of a Zoning Compliance Permit and if private collection is planned, the developer shall provide a letter from a private waste collection service indicating that they have reviewed the final plans and are able to provide service.
<i>Construction Management Plan</i>	Prior to issuance of a Zoning Compliance Permit, a Construction Management Plan shall be reviewed and approved by the Town Manager.
<i>Traffic and Pedestrian Control Plan</i>	Prior to issuance of a Zoning Compliance Permit, the developer shall provide a Work Zone Traffic Control Plan for movement of motorized and non-motorized vehicles on any public street that will be disrupted during construction.
<i>Repairs in the Public Right-of-Ways</i>	Prior to issuance of a Certificate of Occupancy, it will be necessary to repair all damage for work in the public rights-of-ways related to the construction of this project. The design must

	be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
<i>Street Closure Plan</i>	Prior to issuance of a Zoning Compliance Permit, the developer shall provide a street closure plan, subject to review and approval by the Town Manager for any work requiring street closure.

SPECIAL USE PERMIT FINDINGS

The applicant’s materials are included as attachments to this memorandum. All information submitted at the public hearing will be included in the record of the hearing. Based on the evidence submitted, the Council will consider whether or not it can make each of the four required findings for the approval of the Special Use Permit. The four findings are:

Finding #1: That the use or development is located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare;

Finding #2: That the use or development would comply with all required regulations and standards of the Land Use Management Ordinance;

Finding #3: That the use or development is located, designed, and proposed to be operated so as to maintain or enhance the value of contiguous property, or that the use or development is a public necessity; and

Finding #4: That the use or development conforms to the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the Comprehensive Plan.

RECOMMENDATIONS

Staff Recommendation: That the Community Design Commission, Transportation and Connectivity Board, and Environmental Stewardship Advisory Board for a recommendation to the Planning Commission and the Council; and that the Planning Commission, upon considering the recommendations, forwards a recommendation to the Town Council.

RESOLUTION A

A RESOLUTION APPROVING AN APPLICATION FOR A SPECIAL USE PERMIT FOR TRI-CITY MEDICAL BUILDING (PROJECT #14-070)

BE IT RESOLVED by the Council of the Town of Chapel Hill that it finds that a Special Use Permit application, proposed by Tri-City, Inc., for the Tri-City Medical Building, located at 5002 Barbee Chapel Road on property identified as Durham County Property Identifier Number 9798-04-72-5824, located in the Residential-5-Conditional (R-5-C) zoning district and within the Meadowmont Master Land Use Plan, if developed according to the Site Plan dated October 31, 2014 and revised March 27, 2015, the conditions listed below would:

1. Be located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare;
2. Comply with all required regulations and standards of the Land Use Management Ordinance;
3. Be located, designed, and operated so as to maintain or enhance the value of contiguous property; and
4. Conform with the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the 2020 Comprehensive Plan.

Modification to Regulations

BE IT FURTHER RESOLVED by the Council of the Town of Chapel Hill that it finds, in this particular case, that the proposed development with the following requested modification to regulations would satisfy public purposes to an equivalent or greater degree:

- Modification of Table 3.8-1 of the Land Use Management Ordinance to exceed the maximum primary 35-foot building height along the eastern property line.

This finding is based on a determination that public purposes are satisfied to an equivalent or greater degree as this will permit the project to create significant economic benefit through tax base growth, new jobs and community service offerings.

Stipulations Specific to the Development

1. Construction Deadline: That construction begin by _____ (two years from the date of approval) to be completed by _____ (four years from the date of approval).
2. Land Use Intensity: This Special Use Permit authorizes the following:

Use: Business, Office-type, Clinic	
Gross Land Area	101,800 square feet

Total Floor Area Allowed	60,000 square feet
Minimum Vehicular Parking Spaces	240 spaces
Minimum Bicycle Parking Spaces	52 spaces
Impervious Surface	43,560 square feet
Land Disturbance	87,100 square feet

3. Detailed Plan Review and Approval: Prior to issuance of a Zoning Compliance Permit, final detailed site plans, grading plans, utility/lighting plans, stormwater management plans (with hydrologic calculations), landscape plans, and landscape maintenance plans shall be approved. Such plans shall conform to plans approved by the Council and demonstrate compliance with all applicable regulations and standards of the Land Use Management Ordinance and Design Manual.

Plans, plats, and associated detailed requirements as set forth in the stipulations of this resolution and incorporated into the Special Use Permit shall be reviewed and approved by the Town Manager, or his designee, as well as outside agencies, such as NCDOT, OWASA, and Duke Energy, where indicated.

4. Performance Bond: Prior to commencing construction activity for required improvements in the public right-of-way, a performance bond shall be provided to the Town to ensure that improvements are in accordance with Town standards.

Access

5. Cross-Access Easement: That a vehicular and pedestrian cross-access easement for ingress and egress to and through the site from the adjoining property to the west shall be recorded with Durham County prior to issuance of a Certificate of Occupancy. The proposed easement documents shall be reviewed and approved by the Town Manager prior to recordation.
6. Low Vision Design Features: Pedestrian facilities that encourage low vision design features are encouraged.

Transportation

7. Public Right-of-Way Dedication Plat: That prior to issuance of a Certificate of Occupancy, the developer shall submit a recorded right-of-way dedication plat for all required road improvements associated with the development. That the plat shall be reviewed and approved by the Town Manager and NCDOT prior to recordation.
8. Barbee Chapel Road Improvements: Prior to issuance of a Certificate of Occupancy, the developer shall provide the following improvements to site's Barbee Chapel Road frontage:
 - a. Five-foot wide concrete sidewalk;
 - b. Five-foot wide paved and striped bicycle lane;
 - c. Curb and Gutter;
 - d. Three-foot utility easement;

- e. Construct a concrete median from NC 54 to just north of the existing service station driveway;
 - f. Construct an exclusive northbound left turn land with a minimum of 125 feet of full wide storage and appropriate taper on Barbee Chapel Road.
9. Traffic Signal Payment-in-Lieu: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a \$2,000 payment to the Town for retiming traffic signals in proximity to the site.
 10. Repairs in Public Right-of-Way: Prior to issuance of a Certificate of Occupancy, it will be necessary to repair all damage for work in the public right-of-way related to the construction of this project, which may include pavement milling and overlay. The design must be reviewed and approved by the Town Manager prior to a Zoning Compliance Permit.
 11. Street Closure Plan: That prior to the issuance of a Zoning Compliance Permit, the developer shall provide a street closure plan, subject to Town Manager approval, for any work requiring street, sidewalk, or lane closures.
 12. Work Zone Traffic Control Plan: Prior to issuance of a Zoning Compliance Permit, it will be necessary to provide a Work Zone Traffic Control Plan and a Construction Management Plan for approval by the Town Manager. The Work Zone Traffic Control Plan shall comply with the Manual on Uniform Traffic Control Devices. The Construction Management Plan shall provide staging, construction worker parking, construction vehicle routes, and hours of construction.
 13. Heavy Duty Structural Support: Prior to issuance of a Zoning Compliance Permit, the developer shall provide documentation that, if underground stormwater detention is proposed beneath parking areas or drive aisles, the pavement is designed to structurally support the live loads of fire trucks and garbage trucks.
 14. Bicycle Parking: That prior to the issuance of a Zoning Compliance Permit, the developer shall provide dimensioned details for the bicycle parking spaces that comply with Town parking standards. The bicycle parking design must comply with the spring 2010 Association of Pedestrian and Bicycle Professionals Guidelines, and the Class I and Class II bicycle parking standards required by the Town Design Manual.
 15. Parking Lot Standards: Prior to issuance of a Certificate of Occupancy, the developer shall construct the parking lot and drive aisles to Town standards for pavement design and dimensions.
 16. Transportation Management Plan: Prior to issuance of a Zoning Compliance Permit, it will be necessary to submit a Transportation Management Plan, subject to Town Manager approval.

Landscape and Architecture

17. Landscape Bufferyards: That the developer shall provide the following bufferyards:

Location	Required Buffer
East – Barbee Chapel Road	50 foot
North – NC 54	75 foot
West – Marriott Hotel	10 foot Type B
South – Single-family and Finley Forest Complex	8 foot Alternative

18. Landscape Protection: Prior to issuance of a Zoning Compliance Permit, a detailed Landscape Protection Plan shall be approved. The plan shall include which trees will be removed and which will be preserved, critical root zones of all rare and specimen trees, and clearly indicate names and species.
19. Tree Protection Fencing Prior to Construction: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a note on the Final Plans indicating that tree protection fencing will be installed prior to land-disturbing activity on the site. Tree protection fencing shall be provided around construction limits and indicated construction parking and materials staging/storage areas, and Town standard landscaping protection notes, subject to Town Manager approval.
20. Landscape Planting Plan: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a detailed Landscape Planting Plan with a detailed planting list, subject to Town Manager approval. The Plan shall include canopy trees that are proposed to shade any surface parking areas.
21. Lighting Plan Approval: Prior to issuance of a Zoning Compliance Permit, the Community Design Commission shall approve a lighting plan for this site and shall take additional care during review to ensure that the proposed lighting plan will minimize 1) upward light pollution and 2) off-site spillage of light.
22. Community Design Commission Approval: That the developer obtain Community Design Commission approval of building elevations and lighting, including the location and screening of all HVAC/Air Handling Units for this project, prior to issuance of a Zoning Compliance Permit. That prior to issuance of a Zoning Compliance Permit the Commission shall approve a lighting plan for this site and shall take additional care during review to ensure that the proposed lighting plan will minimize 1) upward light pollution and 2) off-site spillage of light.
23. Alternative Buffer: That prior to the issuance of Zoning Compliance Permit, the Community Design Commission approve any proposed alternate buffer. The approval shall determine that any alternate buffer will provide the same degree of visual and noise obstruction as the required buffer.

Environment

24. Energy Management Plan: That prior to issuance of a Zoning Compliance Permit, the developer shall provide an Energy Management Plan (EMP) for Town approval. The plan shall incorporate a “20 percent more energy efficient” feature to outperform the American Society of Heating, Refrigeration, and Air Conditioning Engineers (ASHRAE) energy efficiency standard in place at the time of approval. The property owner shall provide, by the Certificate of Occupancy date, a letter sealed by a licensed professional engineer showing the anticipated energy performance of the buildings, as designed and built, satisfies the “20 percent more energy efficient” expectation.

Stormwater Management

25. Stormwater Management Plan: That this project must comply with the Section 5.4 Stormwater Management of the Land Use Management Ordinance.
26. Jordan Watershed Stormwater Management for New Development: That if the total disturbed area exceeds 0.5 acres, this project must comply with Section 5.19 Jordan Watershed Stormwater Management for New Development of the Land Use Management Ordinance to provide the required reductions in nitrogen and phosphorus loads for new development and redevelopment projects.
27. Silt Control: That the developer takes appropriate measures to prevent and remove the deposit of wet or dry silt on adjacent paved roadways.
28. Jordan Surety: That if the total disturbed area exceeds 0.5 acres, prior to issuance of a Certificate of Occupancy, the owner shall post a maintenance bond or other surety instrument satisfactory to the Town Manager, in an amount equal to one hundred twenty-five (125) percent of the construction cost of each stormwater management facility to assure maintenance, repair, or reconstruction necessary for adequate performance of the stormwater management facility, or establish a stormwater maintenance (sinking fund) budget and escrow account in accordance with the requirements of Section 5.19 of the Land Use Management Ordinance.
29. Erosion Control Bond: If one (1) acre or more is uncovered by land-disturbing activities for this project, then a performance guarantee in accordance with Section 5-97.1 Bonds of the Town Code of Ordinances shall be required prior to final authorization to begin land-disturbing activities. This financial guarantee is intended to cover the costs of restoration of failed or failing soil erosion and sedimentation controls, and/or to remedy damages resulting from land-disturbing activities, should the responsible party or parties fail to provide prompt and effective remedies acceptable to the Town.
30. Erosion Control: The developer shall provide a copy of the approved erosion and sediment control permit from Durham County Erosion Control Division prior to receiving a Zoning Compliance Permit. During the construction phase, additional erosion and sediment controls

may be required if the proposed measures do not contain the sediment. Sediment leaving the property is a violation of the Town's Erosion and Sediment Control Ordinance.

31. Erosion Control Inspections: That, in addition to the requirement during construction for inspection after every rainfall, the developer shall inspect the erosion and sediment control devices daily, make any necessary repairs or adjustments to the devices, and maintain inspection logs documenting the daily inspections and any necessary repairs.
32. Curb Inlets: The developer shall provide pre-cast curb inlet hoods and covers stating, "Dump No Waste! Drains to Jordan Lake", in accordance with the specifications of the Town Standard Detail SD-5A, for all new curb inlets for private, Town and State rights-of-way.
33. As-Built Plans: That prior to the issuance of a Certificate of Occupancy, the developer shall provide certified as-built plans for building footprints, parking lots, street improvements, storm drainage systems and stormwater management structures, and all other impervious surfaces, and a tally of the constructed impervious area. The as-built plans should be in DXF binary format using State plane coordinates and NAVD 88.
34. Phasing Plan: That prior to issuance of a Zoning Compliance Permit, the developer shall obtain approval of a Phasing Plan that provides details of which improvements are to be constructed during each phase. The phasing plan shall detail public improvements and stormwater management structures will be completed in each phase prior to requesting a Certificate of Occupancy. Construction for any phase may not begin until all public improvements in previous phases are complete, with a note to this effect on the final plans and plats.
35. On-Site/Adjacent Stormwater Features: That the final plans locate and identify existing site conditions including all on-site and adjacent stormwater drainage features on the plans prior to issuance of a Zoning Compliance Permit. The final plans must provide proper inlet protection for the stormwater drainage inlets on or adjacent to the site to ensure the stormwater drainage system will not be obstructed with construction debris.
36. P.E. Certification: That prior to the issuance of a Certificate of Occupancy for any phase, the developer shall provide a certification, signed and sealed by a North Carolina-licensed Professional Engineer, that the stormwater management facilities are constructed in accordance with the approved plans and specifications.
37. Repair/Replacement of Damaged Stormwater Infrastructure: Existing stormwater infrastructure that is damaged as a result the project demolition or construction must be repaired or replaced, as specified by the Stormwater Management Engineer, prior to requesting a Certificate of Occupancy.
38. Potential Light Rail Conflict: The developer will demonstrate where an alternate stormwater treatment facility might be provided if future transportation improvement projects reduce or eliminate the capacity of the approved stormwater treatment system to meet the stormwater

management requirements. It is also understood that in this eventuality there might also be a possibility of a shared Stormwater treatment facility with adjacent property owner(s).

Water, Sewer, and Other Utilities

39. Utility/Lighting Plan Approval: That the final utility/lighting plan shall be approved by Orange Water and Sewer Authority, Duke Energy Company, and other local utility service providers, and the Town Manager before issuance of a Zoning Compliance Permit. The property owner shall be responsible for assuring that these utilities can continue to serve the development. In addition, detailed construction drawings shall be submitted to OWASA for review/approval prior to issuance of a Zoning Compliance Permit.
40. Lighting Plan: That prior to issuance of a Zoning Compliance Permit, the developer shall submit site plans and other required documents to satisfy the lighting requirements of Section 5.11 of the Land Use Management Ordinance including submission of a lighting plan, providing for adequate lighting on public sidewalks, including driveway crossings, and beneath awnings, demonstrating compliance with Town standards, sealed by a Professional Engineer, for Town Manager approval.
41. Street Lighting: That the developer work with the Duke Energy to design and install AASHTO standard street lighting along all public streets on the side of the development. The design and construction details are subject to approval by the Town Manager and N.C. Department of Transportation. The developer is responsible for the design and initial construction cost of the street lighting. The Town is responsible for future operating cost of the street lighting.
42. Relocation of Overhead Utilities Underground: Prior to issuance of a Certificate of Occupancy, it will be necessary to provide for the underground installation of all public utilities as specified by Section 5.12.2 in the Land Use Management Ordinance.
43. Water/Sewer Line Construction: That all public water and sewer plans be approved by OWASA and constructed according to their standards. Where sewer lines are located beneath drive aisles and parking areas, construction methods approved by OWASA shall be employed to ensure that sewer lines will not be damaged by heavy service vehicles. That prior to issuance of a Zoning Compliance Permit, final plans shall be approved by OWASA and the Town Manager.
44. OWASA Approval: That prior to issuance of a Zoning Compliance Permit, easement plats and documentation as required by OWASA and the Town Manager shall be recorded if necessary.

Fire Safety

45. Fire Sprinklers: That the developer shall install sprinklers under the North Carolina Fire Code prior to a Certificate of Occupancy. Prior to issuance of a Zoning Compliance Permit, the plans shall show all proposed fire department connections to such systems.

46. Hydrants Active: That the developer shall provide active fire hydrant coverage, acceptable to the Fire Department, for any areas where combustible construction materials will be stored or installed, prior to having such materials delivered to the site. All required fire hydrants must be installed, active, and accessible for the Fire Department use prior to the arrival of combustible materials on site. That fire protection systems shall be installed according to Town Ordinance, the NC Fire Code, and NFPA 13.
47. Fire Hydrant and FDC Locations: That the Final Plans shall indicate the locations of existing and proposed fire hydrants and Fire Department Connections (FDC). Fire Department Connections shall be located on the street side of the building within 100 feet of a hydrant. Hydrant spacing shall comply with the Town Design Manual. Design shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
48. Firefighting Access during Construction: That as required by NC Fire Code (Section 1410.1 Required Access), vehicle access for firefighting shall be provided to all construction or demolition sites including vehicle access to within 100 feet of temporary or permanent fire department connections and hydrants. Vehicle access shall be provided by either temporary or permanent roads capable of supporting vehicle loading under all weather conditions.
49. Fire Flow Report: That the Final Plan application shall include a fire flow report sealed by an Engineer registered in the State of North Carolina. An OWASA flow test must be provided with the report. Fire flow shall meet the 20 psi or exceed the requirements set forth in the Town Design Manual. The Fire Flow Report shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
50. Heavy-Duty Paving: That prior to issuance of a Certificate of Occupancy the developer shall provide heavy duty paving designed and built to withstand fire apparatus weighing at least 75,000 pounds.
51. Fire Lane: Prior to issuance of a Certificate of Occupancy, the fire lane shall be marked and signed in accordance with Town standards, with the associated plans approved by the Town Manager prior to issuance of a Zoning Compliance Permit.

Solid Waste Management and Recycling

52. Solid Waste Management Plan: That prior to issuance of a Zoning Compliance Permit a detailed Solid Waste Management Plan, including a recycling plan and a plan for managing and minimizing construction debris, shall be approved by the Town Manager. The plan shall include dimensioned, scaled details of any proposed refuse/recycling collection areas, associated screening, and protective bollards, if applicable. Each bulk waste container shall be labeled as to type of material to be collected. Prior to issuance of a Zoning Compliance Permit, the developer shall provide documentation of an agreement for solid waste collection by a private provider.

53. Construction Waste: Clean wood waste, scrap metal and corrugated cardboard, all present in construction waste, must be recycled. All haulers of construction waste must be properly licensed. The developer shall provide the name of the permitted waste disposal facility to which any land clearing or demolition waste will be delivered.

State and Federal Approvals

54. State or Federal Approvals: That any required State or federal permits or encroachment agreements (e.g., 401 water quality certification, 404 permit) shall be approved and copies of the approved permits and agreements be submitted to the Town of Chapel Hill prior to the issuance of a Zoning Compliance Permit.
55. North Carolina Department of Transportation Approvals: Prior to issuance of a Zoning Compliance Permit, plans for any improvements to State-maintained roads or in associated rights-of-way shall be approved by NCDOT.

Miscellaneous

56. Temporary Construction Access Agreements: Prior to issuance of a Zoning Compliance Permit, the developer shall provide construction agreements with adjacent property owners, where necessary, subject to Town Manager approval. If the abutting property is to be used as part of construction access, provide documentation of permission from the owner of said property.
57. Construction Management Plan: That a Construction Management Plan be approved by the Town Manager prior to issuance of a Zoning Compliance Permit. The construction management plan shall: 1) indicate how construction vehicle traffic will be managed, 2) identify parking areas for on-site construction workers including plans to prohibit parking in residential neighborhoods, 3) indicate construction staging and material storage areas, 4) identify construction trailers and other associated temporary construction management structures, and 5) indicate how the project construction will comply with the Town's Noise Ordinance.
58. Traffic and Pedestrian Control Plan: That the developer shall provide a Work Zone Traffic Control Plan for movement of motorized and non-motorized vehicles on any public street that will be disrupted during construction. The plan must include a pedestrian management plan indicating how pedestrian movements will be safely maintained. The plan must be reviewed and approved by the Town Manager prior to the issuance of a Zoning Compliance Permit. At least 5 working days prior to any proposed lane or street closure the developer must apply to the Town Manager for a lane or street closure permit.
59. Construction Sign Required: That the developer shall post a construction sign at the development site that lists the property owner's representative and telephone number, the contractor's representative and telephone number, and a telephone number for regulatory information at the time of issuance of a Building Permit, prior to the commencement of any land disturbing activities. The construction sign may have a maximum of 32 square feet of

display area and maximum height of 8 feet. (§5.14.3(g) of Land Use Management Ordinance). The sign shall be non-illuminated, and shall consist of light letters on a dark background. That prior to the issuance of a Zoning Compliance Permit, a detail of the sign shall be reviewed and approved by the Town Manager.

60. Open Burning: That the open burning of trees, limbs, stumps, and construction debris associated with this development is prohibited.
61. Detailed Plans: That prior to the issuance of a Zoning Compliance Permit, final detailed site plans, grading plans, utility/lighting plans, stormwater management plans (with hydrologic calculations), landscape plans, and landscape maintenance plans shall be approved by the Town Manager. Such plans shall conform to plans approved by this application and demonstrate compliance with all applicable regulations and the design standards of the Land Use Management Ordinance and the Design Manual.
62. As-Built Plans: That prior to issuance of a Certificate of Occupancy, the developer shall provide certified as-built plans for building footprints, parking lots, street improvements, storm drainage systems and stormwater management structures, and all other impervious surfaces. The as-built plans should be in DXF binary format using State plane coordinates and NAVD 88. The developer shall also contact the Town's Engineering and Design Services Division for address assignment of each unit.
63. Vested Right: This Special Use Permit constitutes a site specific development plan establishing a vested right as provided by N.C.G.S. Section 160A-385.1 and the Chapel Hill Land Use Management Ordinance.
64. Continued Validity: That continued validity and effectiveness of this approval shall be expressly conditioned on the continued compliance with the plans and conditions listed above.
65. Non-Severability: That if any of the above conditions is held to be invalid, approval in its entirety shall be void.

BE IT FURTHER RESOLVED that the Council hereby approves the application for a Special Use Permit for Tri-City Medical Building at 5002 Barbee Chapel Road.

This the _____ day of _____ 2015.

RESOLUTION B
(Denying the Special Use Permit Application)

**A RESOLUTION DENYING AN APPLICATION FOR A SPECIAL USE PERMIT FOR
TRI-CITY MEDICAL BUILDING (PROJECT #14-070)**

BE IT RESOLVED by the Council of the Town of Chapel Hill that it finds that a Special Use Permit application, proposed by Tri-City, Inc., for the Tri-City Medical Building, located at 5002 Barbee Chapel Road on property identified as Durham County Property Identifier Number 9798-04-72-5824, located in the Residential-5-Conditional (R-5-C) zoning district and within the Meadowmont Master Land Use Plan, if developed according to the Site Plan dated October 31, 2014 and revised March 27, 2015, the conditions listed below would not:

1. Be located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare;
2. Comply with all required regulations and standards of the Land Use Management Ordinance;
3. Be located, designed, and operated so as to maintain or enhance the value of contiguous property; and
4. Conform with the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the 2020 Comprehensive Plan.

BE IT FURTHER RESOLVED that the Town Council hereby denies the application for a Special Use Permit for Tri-City Medical Building, 5002 Barbee Chapel Road.

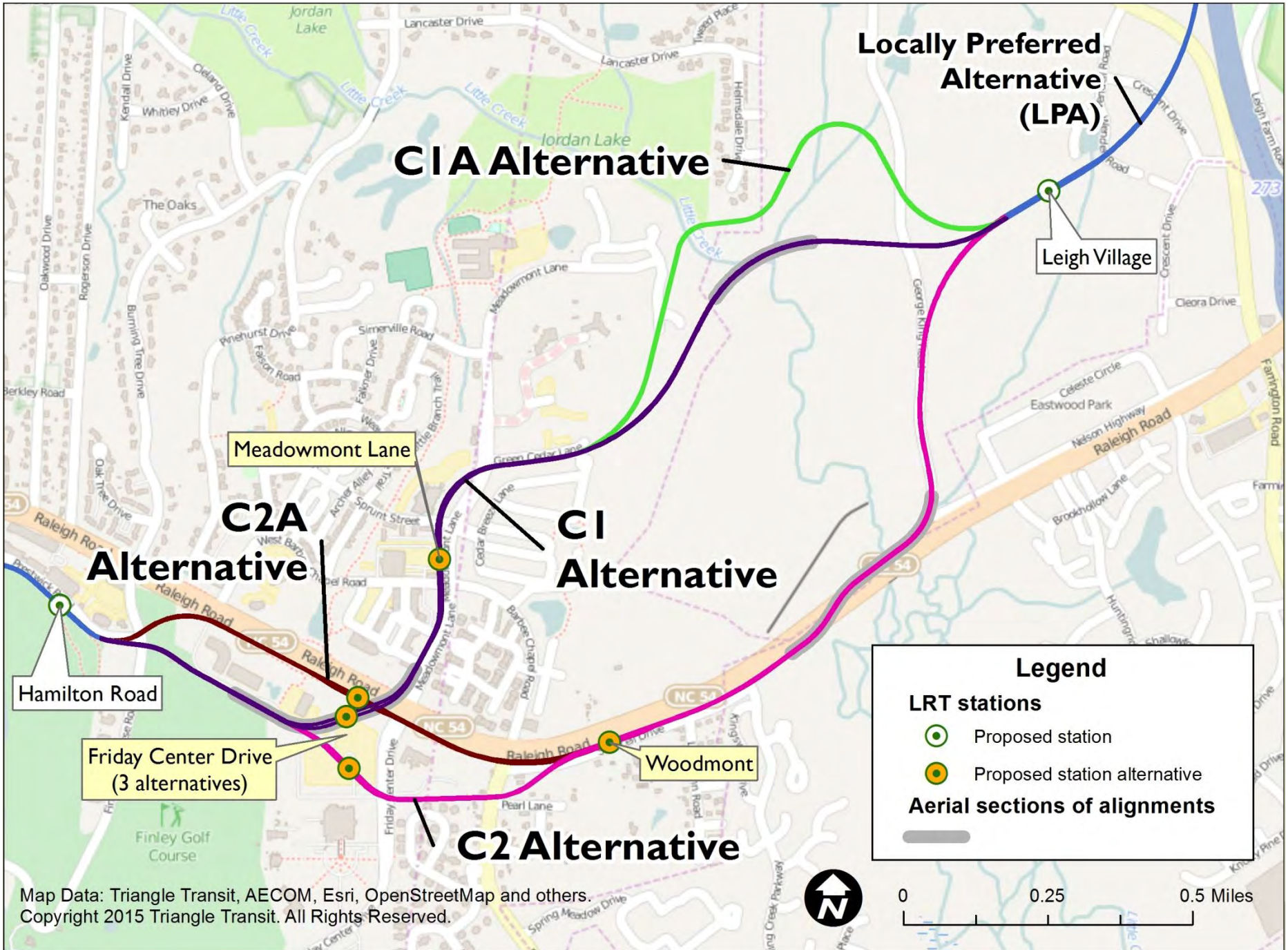
This the _____ day of _____.



Figure 6-28: Combined NC 54 Roadway Recommendations Sheet 3 - Meadowmont Lane/Friday Center Drive to Little John Road

Revised Mar 28, 12; C-2 location approximate

* See Page 6-34 and Report Addendum #30 for discussion



CIA Alternative

Locally Preferred Alternative (LPA)

Leigh Village

Meadowmont Lane

C2A Alternative

CI Alternative

Hamilton Road

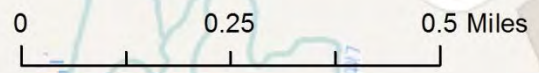
Friday Center Drive (3 alternatives)

Woodmont

C2 Alternative

Legend

- LRT stations**
- Proposed station (Green circle with dot)
- Proposed station alternative (Orange circle with dot)
- Aerial sections of alignments**
- (Grey shaded area)



SPECIAL USE PERMIT APPLICATION



TOWN OF CHAPEL HILL
Planning Department
405 Martin Luther King Jr. Blvd
Chapel Hill, NC 27514
phone (919) 968-2728 fax (919) 969-2014
www.townofchapelhill.org

Parcel Identifier Number (PIN): 9798-04-72-5824

Date: 10-31-14

Section A: Project Information

Project Name: Tri-City Medical Building
Property Address: 5002 Barbee Chapel Road Zip Code: 27514
Use Groups (A, B, and/or C): C Existing Zoning District: R-5-C
Project Description: Construction of a new Medical Office Building with parking deck

Section B: Applicant, Owner and/or Contract Purchaser Information

Applicant Information (to whom correspondence will be mailed)

Name: Tri-City, Inc - Tim Niewald
Address: 36 Richmond Plaza
City: Rockingham State: NC Zip Code: 28379
Phone: 910-997-2544 Email: tjniewald@hotmail.com
910-639-5457

The undersigned applicant hereby certifies that, to the best of his knowledge and belief, all information supplied with this application is true and accurate.

Signature: Tim Niewald 910-639-5457 Date: 10-31-14

Owner/Contract Purchaser Information:

Owner Contract Purchaser

Name: Tri-City, Inc. - Tim Niewald
Address: 36 Richmond Plaza
City: Rockingham State: NC Zip Code: 28379
Phone: 910-997-2544 Email: Tjniewald@hotmail.com
mb 910-639-5457

The undersigned applicant hereby certifies that, to the best of his knowledge and belief, all information supplied with this application is true and accurate.

Signature: Tim Niewald Date: 10-31-14



PROJECT FACT SHEET
TOWN OF CHAPEL HILL
Planning Department

Section A: Project Information

Application type: Special Use Permit Application Date: 10/31/14

Project Name: Tri-City Medical Office Building

Use Type: (check/list all that apply)

Office/Institutional Residential Mixed-Use Other: _____

Overlay District: (check all those that apply)

Historic District Neighborhood Conservation District Airport Hazard Zone

Section B: Land Area

Net Land Area (NLA): Area within zoning lot boundaries		NLA=	101,800	sq. ft.
Choose one, or both, of the following (a or b,) not to exceed 10% of NLA	a) Credited Street Area (total adjacent frontage) x ½ width of public right-of-way	CSA=		sq. ft.
	b) Credited Permanent Open Space (total adjacent frontage) x ½ public or dedicated open space	COS=		sq. ft.
TOTAL: NLA + CSA and/or COS = Gross Land Area (not to exceed NLA + 10%)		GLA=		sq. ft.

Section C: Special Protection Areas, Land Disturbance, and Impervious Area

Special Protection Areas: (check all those that apply)

Jordan Buffer Resource Conservation District 100 Year Floodplain Watershed Protection District

Land Disturbance	Total (sq ft)
Area of Land Disturbance (Includes: Footprint of proposed activity plus work area envelope, staging area for materials, access/equipment paths, all grading, including off-site clearing)	87,100
Area of Land Disturbance within RCD	
Area of Land Disturbance within Jordan Buffer	

Impervious Areas	Existing (sq ft)	Demolition (sq ft)	Proposed (sq ft)	Total (sq ft)
Impervious Surface Area (ISA)	4,797	4,797	43,500	43,500
Impervious Surface Ratio: Percent Impervious Surface Area of Gross Land Area (ISA/GLA) %	4.7%	4.7%	42.7%	42.7%
If located in Watershed Protection District, % of impervious surface on 7/1/1993				





PROJECT FACT SHEET
TOWN OF CHAPEL HILL
Planning Department

Section D: Dimensions

Dimensional Unit (sq ft)	Existing (sq ft)	Demolition (sq ft)	Proposed (sq ft)	Total (sq ft)
Number of Buildings	4	4	1	1
Number of Floors	1	1	3 deck, 2 office	3 deck, 2 office
Recreational Space				

Residential Space				
Dimensional Unit (sq ft)	Existing (sq ft)	Demolition (sq ft)	Proposed (sq ft)	Total (sq ft)
Floor Area (all floors – heated and unheated)				
Total Square Footage of All Units				
Total Square Footage of Affordable Units				
Total Residential Density				
Number of Dwelling Units				
Number of Affordable Dwelling Units				
Number of Single Bedroom Units				
Number of Two Bedroom Units				
Number of Three Bedroom Units				

N/A

Non-Residential Space (Gross Floor Area in Square Feet)					
Use Type	Existing	Proposed	Uses	Existing	Proposed
Commercial					
Restaurant			# of Seats		
Government					
Institutional					
Medical					
Office		60,000			
Hotel			# of Rooms		
Industrial					
Place of Worship			# of Seats		
Other					

Dimensional Requirements		Required by Ordinance	Existing	Proposed
Setbacks (minimum)	Street	20	20	77
	Interior (neighboring property lines)	10	10	10
	Solar (northern property line)			
Height (maximum)	Primary			
	Secondary			
Streets	Frontages			
	Widths			

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PROJECT FACT SHEET
TOWN OF CHAPEL HILL
Planning Department

Section F: Adjoining or Connecting Streets and Sidewalks

(Note: For approval of proposed street names, contact the Engineering Department)

Street Name	Right-of-way Width	Pavement Width	Number of Lanes	Existing Sidewalk*	Existing curb/gutter
NC Hwy 54	Varies	Varies		<input type="checkbox"/> Yes	<input type="checkbox"/> Yes
Barbee Chapel Road	Varies	Varies	3	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes

List Proposed Points of Access (Ex: Number, Street Name):

*If existing sidewalks do not exist and the applicant is adding sidewalks, please provide the following information:

Sidewalk Information			
Street Names	Dimensions	Surface	Handicapped Ramps
NC Hwy 53	5'	Concrete	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A
Barbee Chapel Road	5'	Concrete	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A

Section G: Parking Information

Parking Spaces	Minimum	Maximum	Proposed
Regular Spaces	240	267	240
Handicap Spaces			
Total Spaces			
Loading Spaces			
Bicycle Spaces			
Surface Type			

Section H: Landscape Buffers

Location (North, South, Street, Etc.)	Minimum Width	Proposed Width	Alternate Buffer	Modify Buffer
North - NC Hwy 54	75 Feet	75 Feet	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes
West - Barbee Chapel Road	50 Feet	50 Feet	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes
East	10 Feet	10 Feet	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes
South	20 Feet	20 Feet	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes



PROJECT FACT SHEET
 TOWN OF CHAPEL HILL
 Planning Department

Section I: Land Use Intensity

Existing Zoning District:
 Proposed Zoning Change (if any):

Note: Refer to Table 3.8-1 (Dimensional Matrix) in the Land Use Management Ordinance for help completing this table.

Zoning – Area – Ratio			Impervious Surface Thresholds			Minimum and Maximum Limitations	
Zoning District(s)	Floor Area Ratio (FAR)	Recreation Space Ratio (RSR)	Low Density Residential (0.24)	High Density Residential (0.50)	Non-Residential (0.70)	Maximum Floor Area (MFA) = FAR x GLA	Minimum Recreation Space (MSR) = RSR x GLA
R-5-C	0.6						
TOTAL							
RCD Streamside		0.01					
RCD Managed		0.019					
RCD Upland							

Section J: Utility Service

Check all that apply

Water	<input checked="" type="checkbox"/> OWASA	<input type="checkbox"/> Individual Well	<input type="checkbox"/> Community Well	<input type="checkbox"/> Other
Sewer	<input checked="" type="checkbox"/> OWASA	<input type="checkbox"/> Individual Septic Tank	<input type="checkbox"/> Community Package Plant	<input type="checkbox"/> Other
Electrical	<input checked="" type="checkbox"/> Underground	<input type="checkbox"/> Above Ground		
Telephone	<input checked="" type="checkbox"/> Underground	<input type="checkbox"/> Above Ground		
Solid Waste	<input checked="" type="checkbox"/> Town	<input type="checkbox"/> Private		



**SPECIAL USE PERMIT APPLICATION
SUBMITTAL REQUIREMENTS
TOWN OF CHAPEL HILL
Planning Department**

The following must accompany your application. Failure to do so will result in your application being considered incomplete. For assistance with this application, please contact the Chapel Hill Planning Department (Planning) at (919)968-2728 or at planning@townofchapelhill.org.

X	Application fee (including <u>Engineering Review fee</u>) (refer to fee schedule)	Amount Paid \$ <input type="text"/>
X	Pre-application meeting – with appropriate staff	
X	Digital Files - provide digital files of all plans and documents	
X	Recorded Plat or Deed of Property	
X	Project Fact Sheet	
	Traffic Impact Statement – completed by Town’s consultant (or exemption)	
N/A	Description of Public Art Proposal	
X	Statement of Justification	
	Response to Community Design Commission and Town Council Concept Plan comments	
N/A	Affordable Housing Proposal, if applicable	
N/A	Provide existing Special Use Permit, if Modification	
X	Mailing list of owners of property within 1,000 feet perimeter of subject property (see GIS notification tool)	
X	Mailing fee for above mailing list (mailing fee is double due to 2 mailings)	Amount Paid \$ <input type="text"/>
X	Written Narrative describing the proposal	
	Resource Conservation District, Floodplain, & Jordan Buffers Determination - necessary for all submittals	
N/A	Jurisdictional Wetland Determination – if applicable	
N/A	Resource Conservation District Encroachment Exemption or Variance (determined by Planning)	
N/A	Jordan Buffer Authorization Certificate or Mitigation Plan Approval (determined by Planning)	
X	Reduced Site Plan Set (reduced to 8.5"x11")	

Stormwater Impact Statement (1 copy to be submitted)

- a) Written narrative describing existing & proposed conditions, anticipated stormwater impacts and management structures and strategies to mitigate impacts
- b) Description of land uses and area (in square footage)
- c) Existing and proposed Impervious surface area in square feet for all subareas and project area
- d) Ground cover and uses information
- e) Soil information (classification, infiltration rates, depth to groundwater and bedrock)
- f) Time of concentration calculations and assumptions
- g) Topography (2-foot contours)
- h) Pertinent on-site and off-site drainage conditions
- i) Upstream and/or downstream volumes
- j) Discharges and velocities
- k) Backwater elevations and effects on existing drainage conveyance facilities
- l) Location of jurisdictional wetlands and regulatory FEMA Special Flood Hazard Areas



**SPECIAL USE PERMIT APPLICATION
SUBMITTAL REQUIREMENTS
TOWN OF CHAPEL HILL
Planning Department**

- m) Water quality volume calculations
- n) Drainage areas and sub-areas delineated
- o) Peak discharge calculations and rates (1, 2, and 25-year storms)
- p) Hydrographs for pre- & post-development without mitigation, post-development with mitigation
- q) Volume calculations and documentation of retention for 2-year storm
- r) 85% TSS removal for post-development stormwater run-off
- s) Nutrient loading calculations
- t) BMP sizing calculations
- u) Pipe sizing calculations and schedule (include HGL & EGL calculations and profiles)

Plan Sets (10 copies to be submitted no larger than 24"x36")

Plans should be legible and clearly drawn. All plan sets sheets should include the following:

- Project Name
- Legend
- Labels
- North Arrow (North oriented toward top of page)
- Property Boundaries with bearing and distances
- Scale (Engineering), denoted graphically and numerically
- Setbacks
- Streams, RCD Boundary, Jordan Riparian Buffer Boundary, Floodplain, and Wetlands Boundary, where applicable
- Revision dates and professional seals and signatures, as applicable

Cover Sheet

- a) Include Project Name, Project fact information, PIN, Design team

Area Map

- a) Project name, applicant, contact information, location, PIN, & legend
- b) Dedicated open space, parks, greenways
- c) Overlay Districts, if applicable
- d) Property lines, zoning district boundaries, land uses, project names of site and surrounding properties, significant buildings, corporate limit lines
- e) Existing roads (public & private), rights-of-way, sidewalks, driveways, vehicular parking areas, bicycle parking, handicapped parking, street names.
- f) 1,000' notification boundary

Existing Conditions Plan

- a) Slopes, soils, environmental constraints, existing vegetation, and any existing land features
- b) Location of all existing structures and uses
- c) Existing property line and right-of-way lines





**SPECIAL USE PERMIT APPLICATION
SUBMITTAL REQUIREMENTS
TOWN OF CHAPEL HILL
Planning Department**

- d) Existing utilities & easements including location & sizes of water, sewer, electrical, & drainage lines
- e) Nearest fire hydrants
- f) Nearest bus shelters and transit facilities
- g) Existing topography at minimum 2-foot intervals and finished grade
- h) Natural drainage features & water bodies, floodways, floodplain, RCD, Jordan Buffers & Watershed boundaries

Detailed Site Plan

- a) Existing and proposed building locations
- b) Description & analysis of adjacent land uses, roads, topography, soils, drainage patterns, environmental constraints, features, existing vegetation, vistas (on & off-site)
- c) Location, arrangement, & dimension of vehicular parking, width of aisles and bays, angle of parking, number of spaces, handicapped parking, bicycle parking . Typical pavement sections & surface type
- d) Location of existing and proposed fire hydrants
- e) Location and dimension of all vehicle entrances, exits, and drives
- f) Dimensioned street cross-sections and rights-of-way widths
- g) Pavement and curb & gutter construction details
- h) Dimensioned sidewalk and tree lawn cross-sections
- i) Proposed transit improvements including bus pull-off and/or bus shelter
- j) Required landscape buffers (or proposed alternate/modified buffers)
- k) Required recreation area/space (including written statement of recreation plans)
- l) Refuse collection facilities (existing and proposed) or shared dumpster agreement
- m) Construction parking, staging, storage area, and construction trailer location
- n) Sight distance triangles at intersections
- o) Proposed location of street lights and underground utility lines and/or conduit lines to be installed
- p) Easements
- q) Clearing and construction limits
- r) Traffic Calming Plan – detailed construction designs of devices proposed & associated sign & marking plan

Stormwater Management Plan

- a) Topography (2-foot contours)
- b) Existing drainage conditions
- c) RCD and Jordan Riparian Buffer delineation and boundary (perennial & intermittent streams, note ephemeral streams on site)
- d) Proposed drainage and stormwater conditions
- e) Drainage conveyance system (piping)
- f) Roof drains
- g) Easements
- h) BMP plans, dimensions, details, and cross-sections
- i) Planting and stabilization plans and specifications



Landscape Protection Plan

- a) Rare, specimen, and significant tree survey within 50 feet of construction area
- b) Rare and specimen tree critical root zones
- c) Rare and specimen trees proposed to be removed
- d) Certified arborist tree evaluation, if applicable
- e) Significant tree stand survey
- f) Clearing limit line
- g) Proposed tree protection /silt fence location
- h) Pre-construction/demolition conference note
- j) Landscape protection supervisor note
- k) Existing and proposed tree canopy calculations, if applicable

Planting Plan

- a) Dimensioned and labeled perimeter landscape bufferyard
- b) Off-site buffer
- c) Landscape buffer and parking lot planting plan (including planting strip between parking and building, entryway planting, and 35% shading requirement)

Steep Slope Plan

- a) Classify and quantify slopes 0-10%, 10-15%, 15-25% and 25% and greater
- b) Show and quantify areas of disturbance in each slope category
- c) Provide/show specialized site design and construction techniques

Grading and Erosion Control Plan

- a) Topography (2-foot contours)
- b) Limits of Disturbance
- c) Pertinent off-site drainage features
- d) Existing and proposed impervious surface tallies

Streetscape Plan, if applicable

- a) Public right-of-way existing conditions plan
- b) Streetscape demolition plan
- c) Streetscape proposed improvement plan
- d) Streetscape proposed utility plan and details
- e) Streetscape proposed pavement/sidewalk details
- f) Streetscape proposed furnishing details
- g) Streetscape proposed lighting details



**SPECIAL USE PERMIT APPLICATION
SUBMITTAL REQUIREMENTS
TOWN OF CHAPEL HILL
Planning Department**

Solid Waste Plan

- a) Preliminary Solid Waste Management Plan
- b) Existing and proposed dumpster pads
- c) Proposed dumpster pad layout design
- d) Proposed heavy duty pavement locations and pavement construction detail

Construction Management Plan

- a) Construction trailer location
- b) Location of construction personnel parking and construction equipment parking
- c) Location and size of staging and materials storage area
- d) Description of emergency vehicle access to and around project site during construction
- e) Delivery truck routes shown or noted on plan sheets

Energy Management Plan

- a) Description of how project will be 20% more energy efficient than ASHRAE Standards
- b) Description of utilization of sustainable forms of energy (Solar, Wind, Hydroelectric, and Biofuels)
- c) Participation in NC GreenPower program
- d) Description of how project will ensure indoor air quality, adequate access to natural lighting, and allow for proposed utilization of sustainable energy
- e) Description of how project will maintain commitment to energy efficiency and reduced carbon footprint over time
- f) Description of how the project's Transportation Management Plan will support efforts to reduce energy consumption as it affects the community

Exterior Elevations

- a) An outline of each elevation of the building, including the finished grade line along the foundation (height of building measured from mean natural grade).



PROJECT NARRATIVE

This is a request for Special Use Permit for the Tri-City Medical Building with the R-5-C zoning district.

The application seeks approval for a two story medical office building over three levels of parking to consolidate the footprint in an efficient design. The design calls for approximately 28,000 square feet of office space per floor along with 240 spaces as per code.

The site for the Special Use Permit is 2.337 acres on the corner of NC Hwy 54 and Barbee Chapel Road with drive access on Barbee Chapel Road. The property is Parcel 6A of the Meadowmont Development.

STATEMENT OF JUSTIFICATION

The applicant, Tri-City, Inc., submits the following information as evidence that this requested Special Use Permit modification meets the four Special Use Permit findings of fact.

Finding #1: That the use or development is located designed and proposed to be operated to as to maintain or promote the public health, safety and general welfare.

- a. **Traffic conditions in the vicinity, including the effect of additional traffic on streets, street intersections, and site line at street intersections and driveways.**

Vehicular access to and from the site will be provided from two locations. One location is from existing driveway on Barbee Chapel Road and second is the new cross access to the adjacent properties to the west.

- b. **Provision for services and utilities, including sewer, water, electric, garbage collection and fire protection.**

All utilities are currently on or adjacent to the property. OWASA will provide the water and sewer and Duke Power will provide the electricity.

- c. **Drainage Plans.**

Storm drainage will meet the requirements by the Town and approve Meadowmont stormwater plan.

- d. **Relationship of the site to the Chapel Hill Floodway/Floodplain Resource Conservation District.**

Floodway/Floodplain Resource Conservation is partially on the property as shown in Plat Book 152, PG 109.

Finding #2: The use or development complies with all required regulations and standards of this chapter, including all applicable provisions of articles 3 and 5, the applicable specific standards contained in the supplemental use regulations (article 6), and all other applicable regulations.

- a. **Compliance with Development Ordinance and land development regulations and standards, including street improvement, screening and landscape buffer requirements, setbacks, height, parking and intensity regulations.**

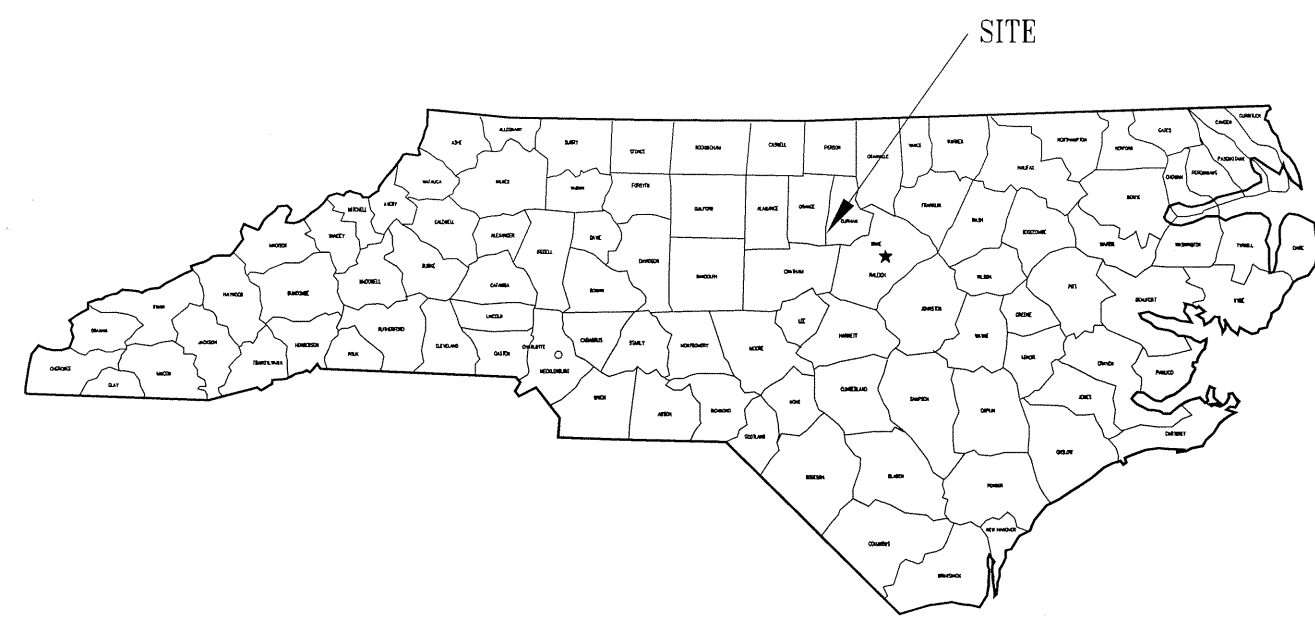
This project will meet or exceed the Development Ordinance requirements for screening and landscape buffers, setbacks, building height and parking.

Finding #3: That the use or development is located, designed, and proposed to be operated so as to maintain or enhance the value of contiguous property, or that the use or development is a public necessity.

Meets the 2020 Comprehensive Plan and the current approved Meadowmont Development Plan.

Finding #4: That the use or development conforms to the general plans for the physical development of the Town as embodied in this chapter and in the Comprehensive Plan.

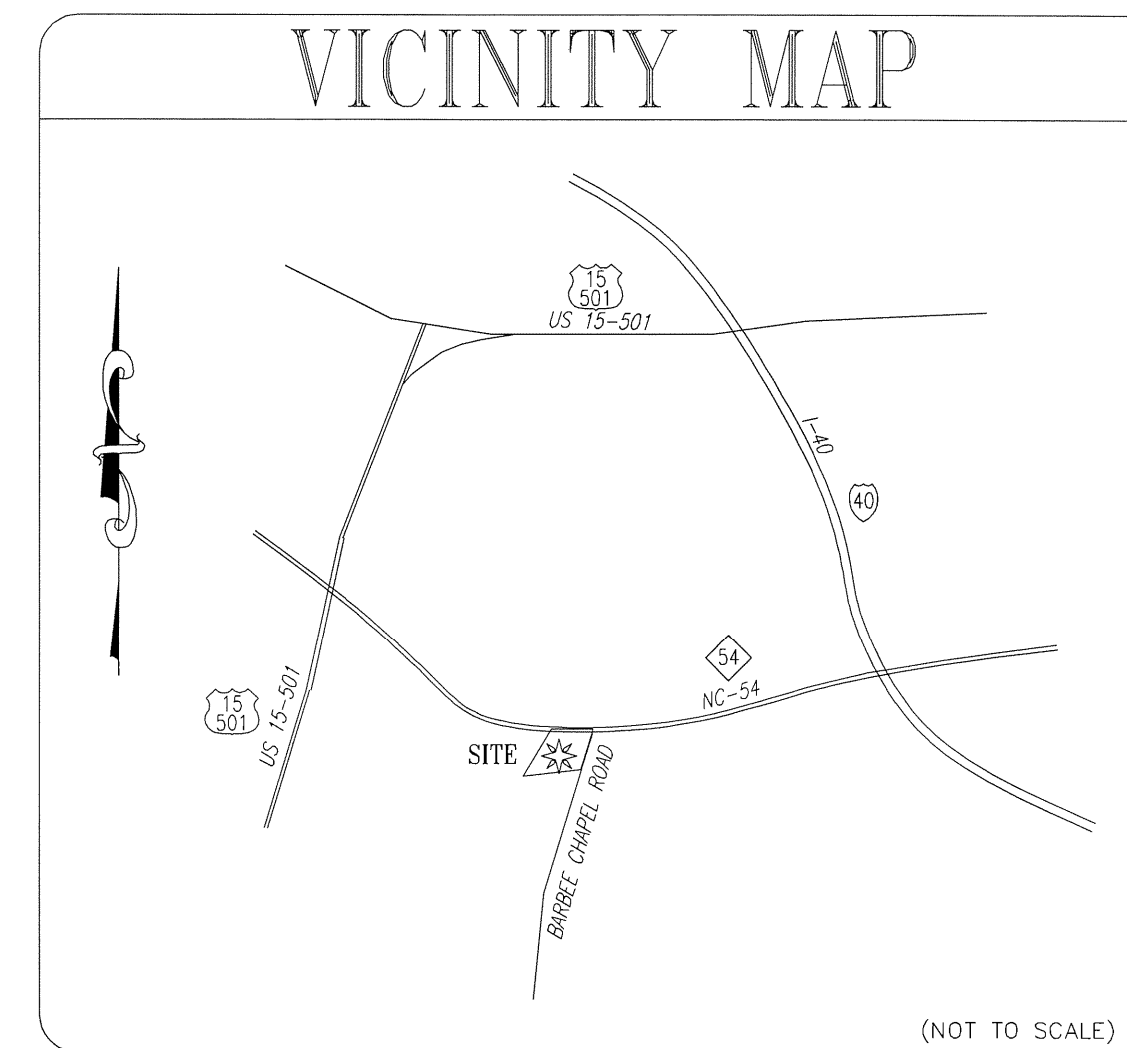
Meets the 2020 Comprehensive Plan and the current approved Meadowmont Development Plan.



SPECIAL USE PERMIT APPLICATION

Proposed MEDICAL OFFICE BUILDING

NC Highway 54 & Barbee Chapel Road Town of Chapel Hill Durham County, North Carolina



IMPERVIOUS AREA SUMMARY			
SITE AREA = 101,800 SF (2.337AC)			
PROPOSED BUILDINGS	31,155 SF	0.72 ACRE(S)	30.6 % OF TOTAL AREA
PROPOSED PAVEMENT	10,355 SF	0.23 ACRE(S)	10.2 % OF TOTAL AREA
PROPOSED SIDEWALK	1,990 SF	0.05 ACRE(S)	2.0 % OF TOTAL AREA
TOTAL IMPERVIOUS AREA	43,500 SF	1.0 ACRE(S)	42.7 % OF TOTAL AREA
GREEN/OPEN SPACE	58,300 SF	1.3 ACRE(S)	57.3 % OF TOTAL AREA
EXISTING IMPERVIOUS AREA	4,797 SF	0.11 ACRE(S)	4.7 % OF TOTAL AREA
INCREASE IN IMPERVIOUS AREA	38,703 SF	0.89 ACRE(S)	
IMPERVIOUS AREA ALLOCATION TO PARCEL FROM MEADOWMONT DEVELOPMENT (PARCEL 6A)	43,560 SF	1.0 ACRE(S)	42.79 % OF TOTAL AREA

CONSTRUCTION NOTES:

- PRIOR TO ANY DEMOLITION OR CONSTRUCTION ACTIVITY ON THE SITE, THE APPLICANT IS REQUIRED TO SCHEDULE AND CO-ORDINATE AN ON SITE PRE-CONSTRUCTION MEETING WITH ALL APPLICABLE APPROVING AGENCIES. ALL TREE PROTECTION FENCING SHOULD BE IN PLACE FOR REVIEW.
- DEVELOPER/CONTRACTOR SHALL NOT STORE VEHICLES, EQUIPMENT, AND/OR CONSTRUCTION RELATED MATERIALS WITHIN THE PUBLIC RIGHT OF WAY OR ON CITY STREETS.
- DEVELOPER/CONTRACTOR SHALL PROVIDE A DESIGNATED CONSTRUCTION DRIVE ACCESS CONSTRUCTED IN ACCORDANCE WITH ORANGE COUNTY EROSION CONTROL REQUIREMENTS TO MINIMIZE THE TRANSPORTATION OF MUD, SOILS, AND CONSTRUCTION RELATED MATERIALS OFF SITE ONTO PUBLIC ROADWAYS. DEVELOPER/CONTRACTOR SHALL REMOVE PROMPTLY, AND MUD, SOILS AND/OR CONSTRUCTION RELATED MATERIALS DEPOSITED UPON THE SURFACES OF THE PUBLIC RIGHT-OF-WAY.
- NO OPEN BURNING SHALL BE PERMITTED DURING THE COURSE OF DEMOLITION AND CONSTRUCTION ON SITE.

OWNER/DEVELOPER

Tri-City, Inc
Attn. Tim Niewald
36 Richmond Plaza
Rockingham, NC 28379
Phone: (910) 639-5457
tniewald@tricityinc.net

ARCHITECT

MHAworks
Attn. Toma Rogers
501 Washington Street
Suite G
Durham, NC 27701
(919) 682-2870
trogers@mhaworks.com

CIVIL ENGINEER

Triangle Site Design, PLLC
4006 Barrett Drive
Suite 203
Raleigh, NC 27609
(919) 553-6570
mlowder@trianglesitedesign.com
NC LICENSE#P-0619

DEVELOPMENT DATA

CURRENT PROPERTY OWNER:	BANK OF NORTH CAROLINA 831 JULIAN AVENUE THOMASVILLE, NC 27360
DEVELOPMENT NAME:	MEDICAL OFFICE BUILDING
STREET ADDRESS:	NC HIGHWAY 54 & BARBEE CHAPEL ROAD CHAPEL HILL, NC DURHAM COUNTY
TAX PARCEL/PROPERTY IDENTIFICATION # (PIN):	9798-04-72-5824
ZONING DISTRICT(S):	R-5-C SITE IS WITHIN THE MEADOWMONT DEVELOPMENT (PARCEL 6A)
SPECIAL PROTECTION AREA:	JORDAN LAKE WATERSHED
EXISTING SITE AREA:	2.337 AC (101,800 SF)
RIGHT-OF-WAY DEDICATION:	0 AC
TOTAL SITE AREA:	2.337 AC
INSIDE TOWN LIMITS:	Yes
EXISTING USE:	ABANDONED STRUCTURES/WOODED
PROPOSED BUILDING USE:	MEDICAL OFFICE BUILDING
PROPOSED TOTAL BUILDING AREA OR EXISTING AREA IF EXPANSION (s.f. gross):	60,000SF MEDICAL OFFICE
MAX. BUILDING HEIGHT:	-----
PERMITTED FAR:	0.303 (30,855SF)
REQUESTED FAR:	0.600 (61,080SF MAX - ZONING VARIANCE REQUESTED)
ALLOCATED BUILDING AREA:	19,200SF (MEADOWMONT MASTER LAND USE PLAN)
REQUESTED BUILDING AREA:	60,000SF (MINOR MODIFICATION TO THE MEADOWMONT LAND USE PLAN)
NET LAND AREA (NLA)	-----
CREDITED STREET AREA (CSA)	-----
CREDITED PERMANENT OPEN SPACE (COS)	-----
TOTAL: NLA + CSA AND/OR COS = GROSS LAND AREA (NOT TO EXCEED NLA + 10%)	-----
AREA OF LAND DISTURBANCE:	87,100SF (2.04C)
ALLOCATED IMPERVIOUS AREA:	43,560SF (1.04C - MEADOWMONT STORMWATER MANAGEMENT PLAN)
PROPOSED IMPERVIOUS AREA:	43,500SF (1.04C)
BUILDING SETBACKS:	
FRONT:	20 FT (NC HIGHWAY 54)
SIDE:	20 FT (BARBEE CHAPEL ROAD)
SIDE:	8 FT (INTERNAL SETBACK)
REAR:	8 FT (INTERNAL SETBACK)
BUFFERS:	
FRONT:	75 FT TYPE D (NC HIGHWAY 54 - MEADOWMONT LAND USE PLAN)
SIDE:	50 FT TYPE D (BARBEE CHAPEL ROAD - MEADOWMONT LAND USE PLAN)
SIDE:	10 FT TYPE B (WEST PROPERTY LINE)
REAR:	20 FT TYPE C (SOUTH PROPERTY LINE - MEADOWMONT LAND USE PLAN)
	A BUFFER MODIFICATION IS REQUESTED TO ALLOW SELECTIVE THINNING OF EXISTING TREES AND SHRUBS WITHIN THE EXISTING PERIMETER BUFFER AREAS.
PARKING INFORMATION:	MEDICAL OFFICE BUILDING - 1 SPACE PER 225SF = 267 SPACES 10% ALLOWABLE PARKING SPACE REDUCTION = 27 SPACES PROPOSED NUMBER OF PARKING SPACES = 240 PARKING PROPOSED WITHIN 3-LEVEL PARKING DECK PROPOSED NUMBER OF BICYCLE PARKING SPACES = 52 (80% CLASS 1 & 20% CLASS 2)
LAND USE INTENSITY:	-----

INDEX OF DRAWINGS

C0.0	COVER SHEET
D1.0	EXISTING CONDITIONS PLAN
C1.A	AREA MAP
C1.B	PLAT MAP
C1.0	SITE PLAN
C1.0A	ROADWAY IMPROVEMENTS PLAN
C1.0B	POTENTIAL PUBLIC IMPROVEMENTS PLAN
C1.1	GRADING AND DRAINAGE PLAN
C1.2	UTILITY PLAN
C1.3	LANDSCAPE PLAN
A-1	BUILDING ELEVATIONS

TRIANGLE SITE DESIGN

CONSULTANT:
Triangle Site Design, PLLC
4006 Barrett Drive
Suite 203
Raleigh, NC 27609
(919)553-6570
mlowder@trianglesitedesign.com
NC LICENSE #P-0619

SEAL:

SPECIAL USE PERMIT APPLICATION DRAWINGS PRELIMINARY DO NOT USE FOR CONSTRUCTION

TRI-CITY MEDICAL BUILDING
 RALEIGH ROAD (NC HWY 54 & BARBEE CHAPEL ROAD)
 CHAPEL HILL, NORTH CAROLINA
 DURHAM COUNTY
 PARCEL # 9798-04-72-5824

OWNER/DEVELOPER:
TRI-CITY, Inc.
36 Richmond Plaza
Rockingham, NC 28379
(910) 997-2544
Neil Robinette - CEO
Barry Embler - CFO
Tim Niewald - General Contractor
(910) 639-5457
tniewald@tricityinc.net

REVISIONS:

3/27/15 Revised per Town review comments, traffic study & Master Plan Update

LAYOUT COORD:	MEL
PLANNING MGR.	MEL
DRAWING BY:	MEL
DATE:	10/31/14
JOB NUMBER:	004080
TITLE:	COVER SHEET
SHEET NUMBER:	C0.0
COMMENTS:	

TRI-CITY MEDICAL OFFICE BUILDING

CAUTIONARY NOTE

THE LOCATIONS OF ALL EXISTING UNDERGROUND UTILITIES SHOWN ON THIS DRAWING ARE BASED ON ABOVE GROUND EVIDENCE OR INFORMATION GATHERED DURING THE FIELD SURVEY PORTION OF THIS PROJECT. AS SUCH, THIS INFORMATION IS SPECULATIVE IN NATURE ONLY AND SHOULD NOT BE CONSTRUED AS FACT. THERE MAY ALSO BE OTHER EXISTING UNDERGROUND UTILITIES FOR WHICH NO ABOVE GROUND EVIDENCE WAS OBSERVED. THE EXACT LOCATIONS OF THE UNDERGROUND UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO THE START OF ANY AND ALL CONSTRUCTION.

SURVEY NOTE:

ALL EXISTING BOUNDARY AND TOPOGRAPHIC INFORMATION TAKEN FROM A LAND SURVEY PREPARED BY PHILIP POST & ASSOCIATES

STREAM/WETLAND NOTE:

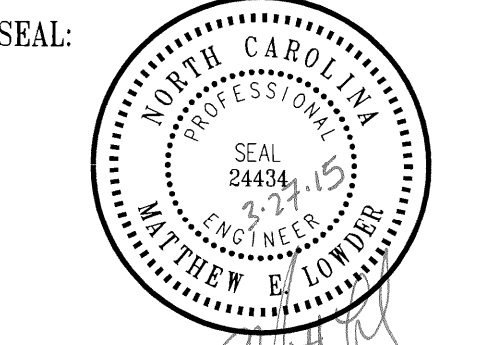
THERE ARE NO WETLANDS AND/OR JURISDICTIONAL STREAMS ON THE SUBJECT PROPERTY

UTILITY STATEMENT

THE UNDERGROUND UTILITIES SHOWN HAVE BEEN LOCATED FROM FIELD SURVEY INFORMATION AND EXISTING DRAWINGS. THE SURVEYOR MAKES NO GUARANTEES THAT THE UNDERGROUND UTILITIES COMPRISE ALL SUCH UTILITIES IN THE AREA, EITHER IN-SERVICE OR ABANDONED. THE SURVEYOR FURTHER DOES NOT WARRANT THAT THE UNDERGROUND UTILITIES SHOWN ARE IN THE EXACT LOCATION INDICATED ALTHOUGH THE SURVEYOR DOES CERTIFY THAT THEY ARE LOCATED AS ACCURATELY AS POSSIBLE FROM INFORMATION AVAILABLE. THE SURVEYOR HAS NOT PHYSICALLY LOCATED THE UNDERGROUND UTILITIES.



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PLANNING MGR. MEL

DRAWING BY: MEL

DATE: 10/31/14

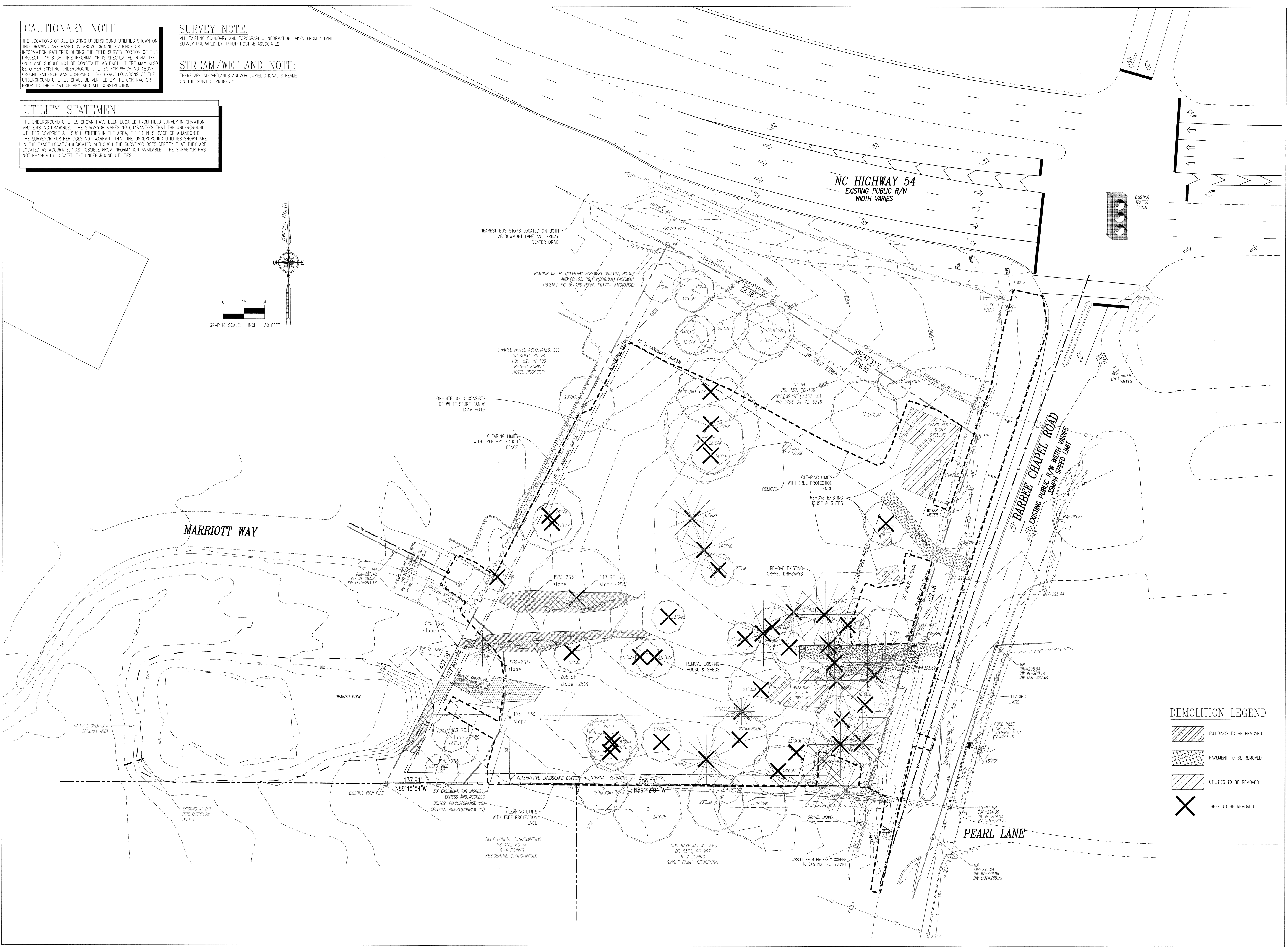
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TITLE: EXISTING
CONDITIONS PLAN

SHEET NUMBER:

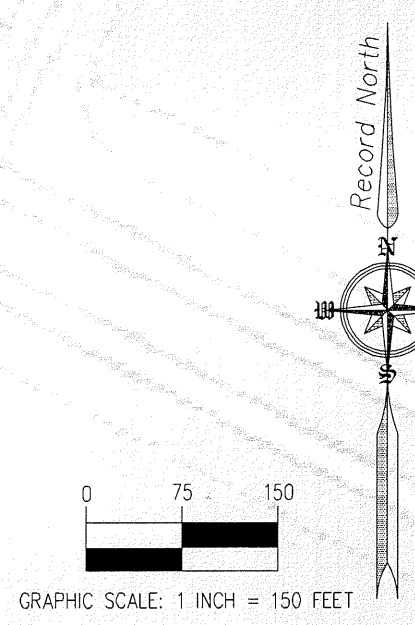
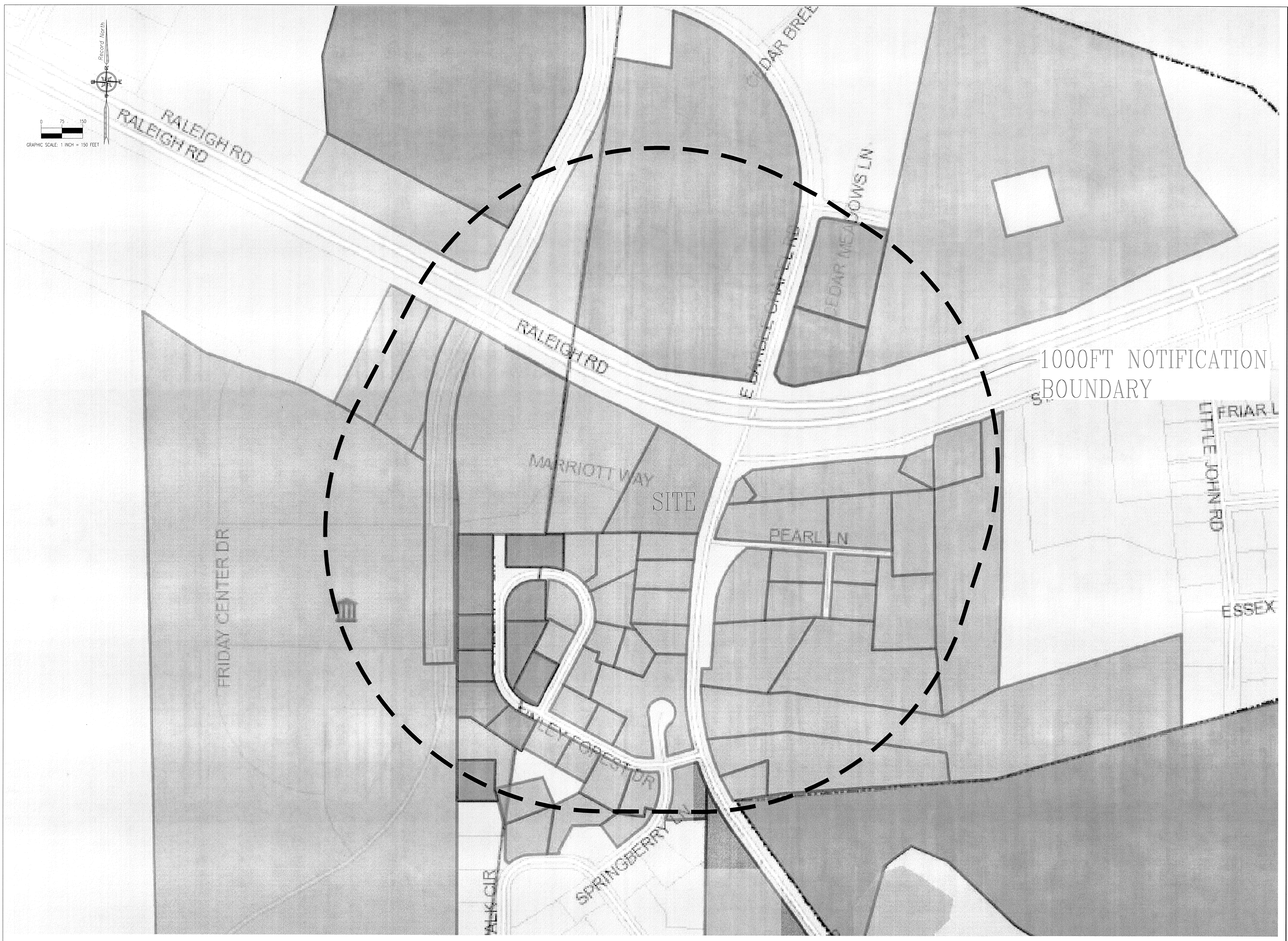
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COMMENTS:



DEMOLITION LEGEND

- BUILDINGS TO BE REMOVED
- PAVEMENT TO BE REMOVED
- UTILITIES TO BE REMOVED
- TREES TO BE REMOVED



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 3/27/15 Revised per Town review
 comments, traffic study
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LAYOUT COORD: MEL
 PLANNING MGR: MEL
 DRAWING BY: MEL
 DATE: 10/31/14
 JOB NUMBER: 004080

TITLE:
 AREA MAP
 SHEET NUMBER:
 C1.A

COMMENTS:



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DRAWING BY: MEL

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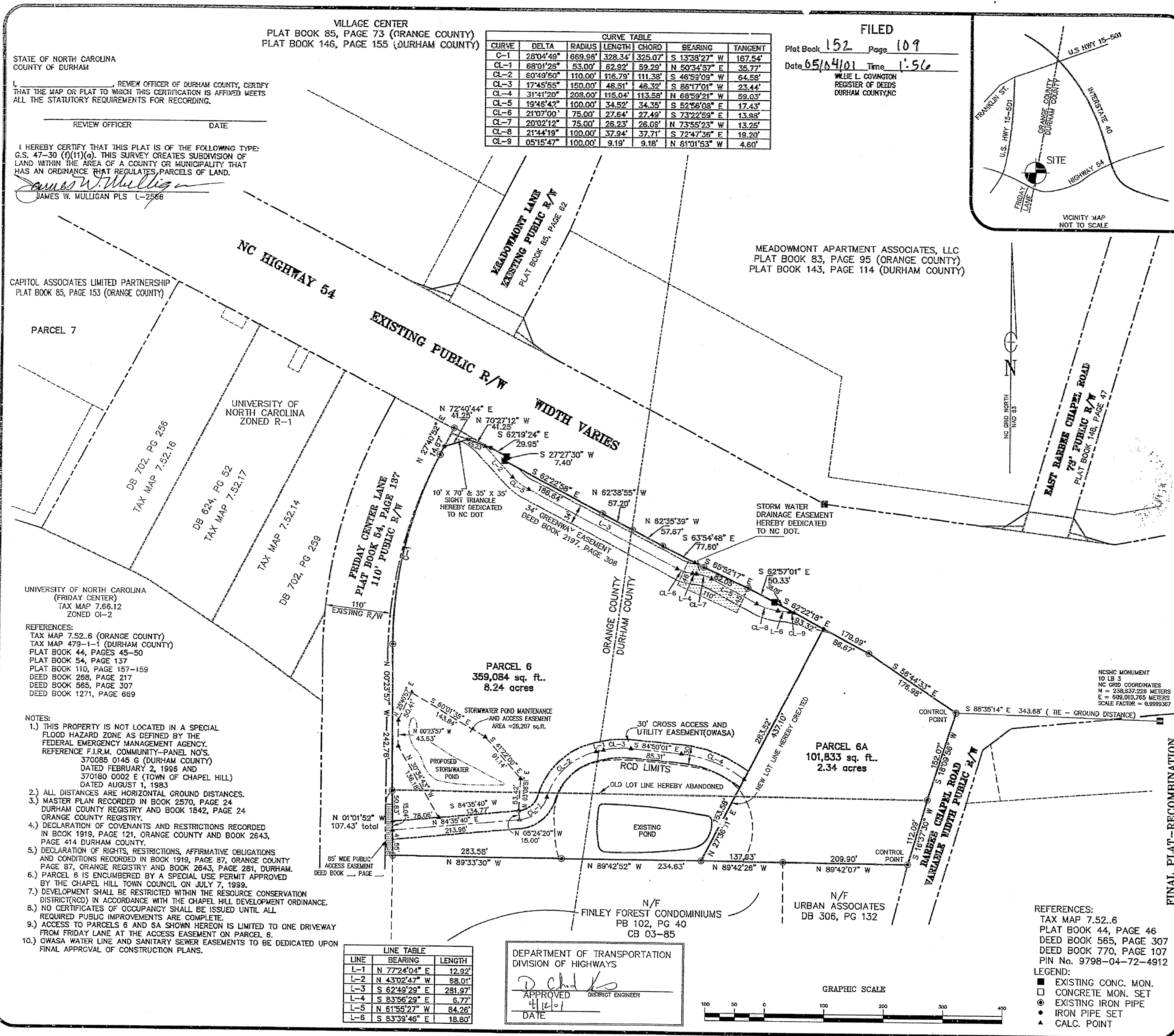
TITLE:

PLAT MAP

SHEET NUMBER:

C1.B

COMMENTS:



FINAL PLAT - RECOMBINATION

MEADOWMONT DEVELOPMENT COMPANY
 DURHAM COUNTY, NORTH CAROLINA
 CHAPEL HILL TOWNSHIP, ORANGE COUNTY, NORTH CAROLINA

THE JOHN R. McADAMS COMPANY, INC.
 CIVIL ENGINEERING • LAND PLANNING • SURVEYING
 P.O. BOX 14005 • RESEARCH TRIANGLE PARK, NC 27709
 (919) 361-5000 • FAX (919) 361-2269

REVISIONS:
 01/17/01 ADD GREENWAY EASEMENT
 02/29/01 TOWN COMMENTS
 03/15/01 REVERSE POND EASEMENT

FINAL PLAT OFFICE and HOTEL SITE SHEET 2 of 2

PREPARED BY: JAMES W. MULLIGAN L-2566

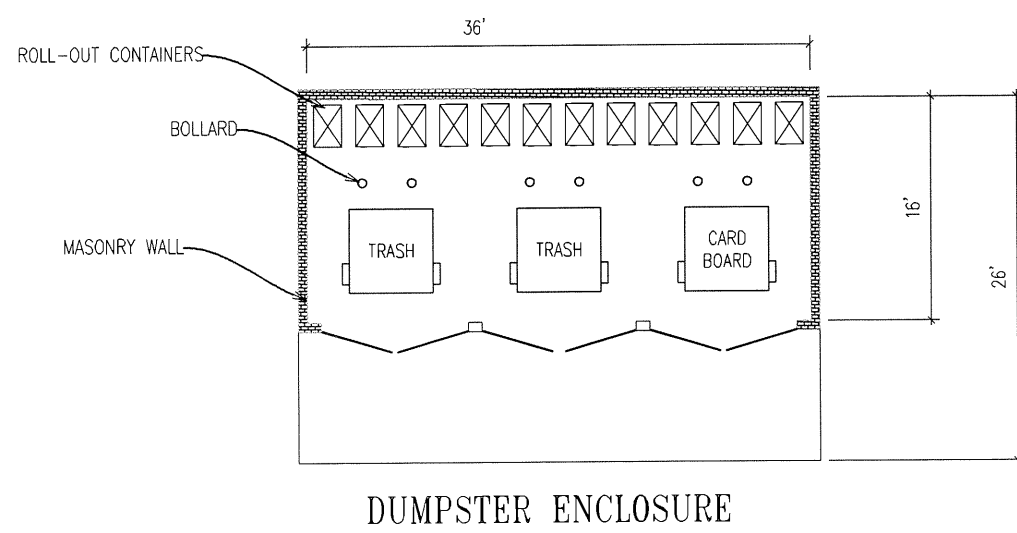
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 SCALE: 1" = 100'
 DATE: 09 MAY 2000

McADAMS

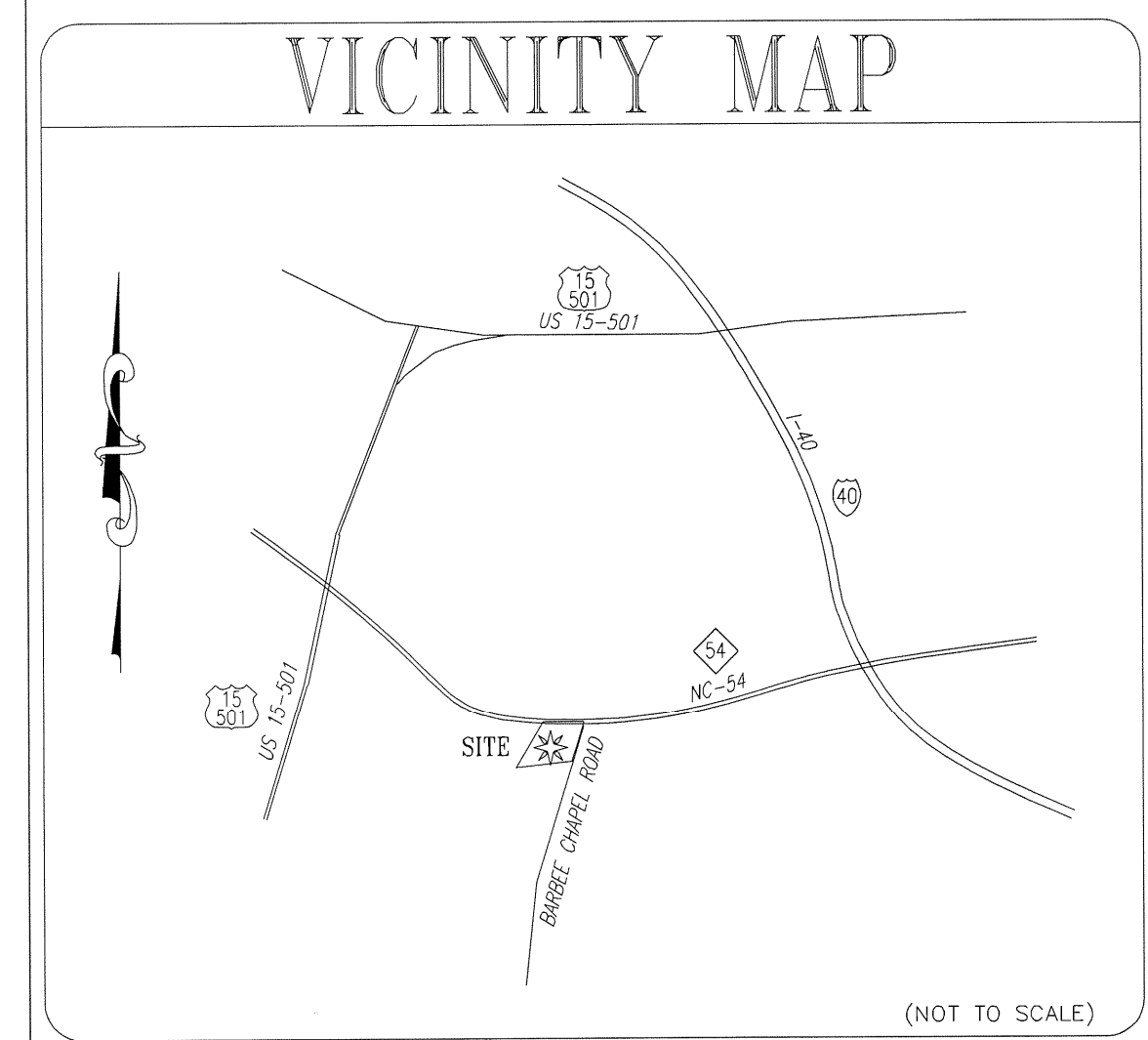
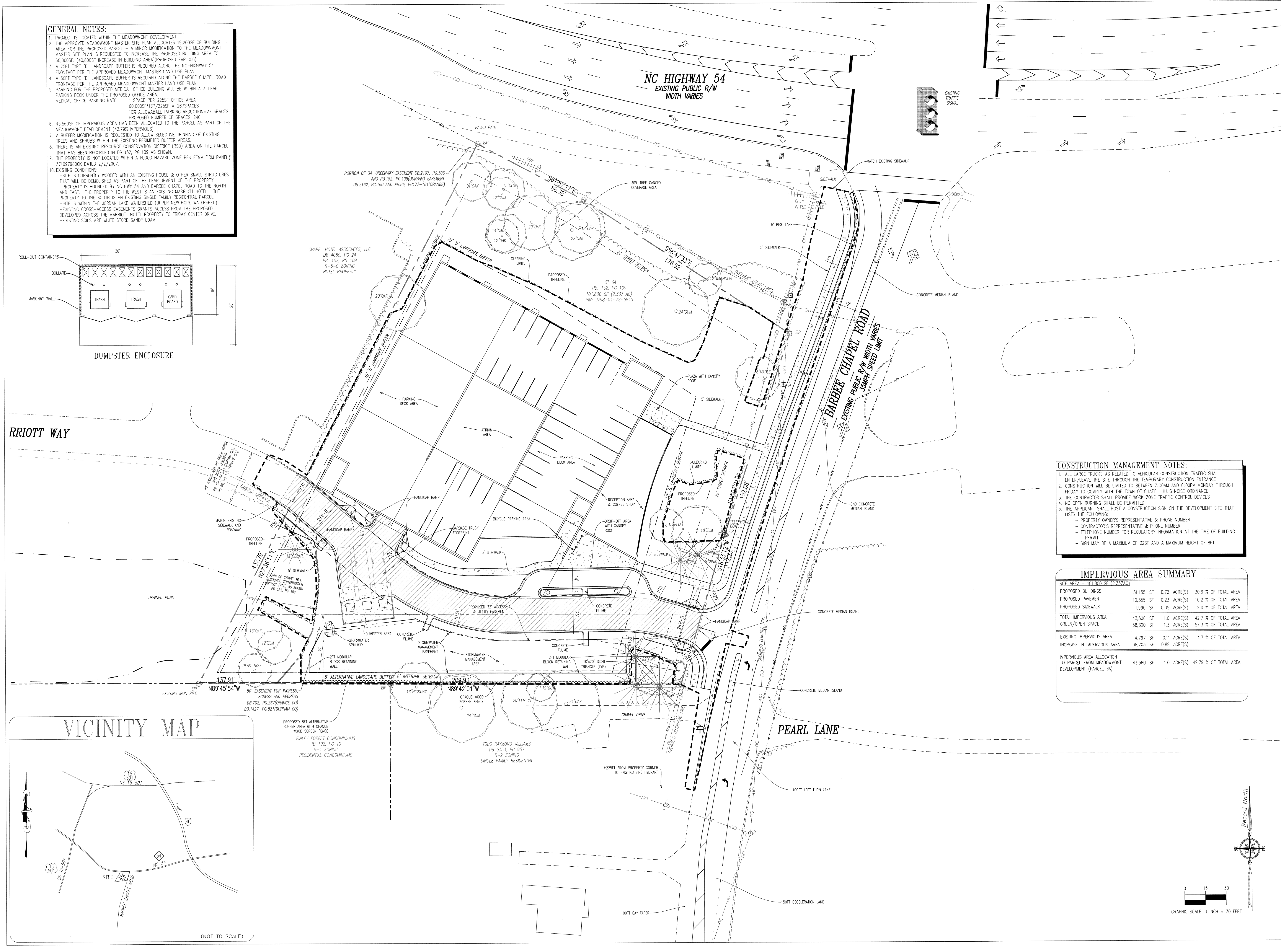
DURHAM COUNTY

GENERAL NOTES:

- PROJECT IS LOCATED WITHIN THE MEADOWMONT DEVELOPMENT
- THE APPROVED MEADOWMONT MASTER SITE PLAN ALLOCATES 19,200SF OF BUILDING AREA FOR THE PROPOSED PARCEL - A MINOR MODIFICATION TO THE MEADOWMONT MASTER SITE PLAN IS REQUESTED TO INCREASE THE PROPOSED BUILDING AREA TO 60,000SF (40,800SF INCREASE IN BUILDING AREA)(PROPOSED FAR=0.6)
- A 75FT TYPE "D" LANDSCAPE BUFFER IS REQUIRED ALONG THE NC-HIGHWAY 54 FRONTAGE PER THE APPROVED MEADOWMONT MASTER LAND USE PLAN
- A 50FT TYPE "D" LANDSCAPE BUFFER IS REQUIRED ALONG THE BARBEE CHAPEL ROAD FRONTAGE PER THE APPROVED MEADOWMONT MASTER LAND USE PLAN
- PARKING FOR THE PROPOSED MEDICAL OFFICE BUILDING WILL BE WITHIN A 3-LEVEL PARKING DECK UNDER THE PROPOSED OFFICE AREA
MEDICAL OFFICE PARKING RATE: 1 SPACE PER 225SF OFFICE AREA
60,000SF * 1SP / 225SF = 267SPACES
10% ALLOWABLE PARKING REDUCTION=27 SPACES
PROPOSED NUMBER OF SPACES=240
- 43,560SF OF IMPERVIOUS AREA HAS BEEN ALLOCATED TO THE PARCEL AS PART OF THE MEADOWMONT DEVELOPMENT (42.79% IMPERVIOUS)
- A BUFFER MODIFICATION IS REQUESTED TO ALLOW SELECTIVE THINNING OF EXISTING TREES AND SHRUBS WITHIN THE EXISTING PERIMETER BUFFER AREAS.
- THERE IS AN EXISTING RESOURCE CONSERVATION DISTRICT (RCD) AREA ON THE PARCEL THAT HAS BEEN RECORDED IN DB 152, PG 109 AS SHOWN.
- THE PROPERTY IS NOT LOCATED WITHIN A FLOOD HAZARD ZONE PER FEMA FIRM PANEL# 370979800K DATED 2/2/2007.
- EXISTING CONDITIONS
-SITE IS CURRENTLY WOODED WITH AN EXISTING HOUSE & OTHER SMALL STRUCTURES THAT WILL BE DEMOLISHED AS PART OF THE DEVELOPMENT OF THE PROPERTY
-PROPERTY IS BOUNDED BY NC HWY 54 AND BARBEE CHAPEL ROAD TO THE NORTH AND EAST. THE PROPERTY TO THE WEST IS AN EXISTING MARRIOTT HOTEL. THE PROPERTY TO THE SOUTH IS AN EXISTING SINGLE FAMILY RESIDENTIAL PARCEL.
-SITE IS WITHIN THE JORDAN LAKE WATERSHED (UPPER NEW HOPE WATERSHED)
-EXISTING CROSS-ACCESS EASEMENTS GRANTS ACCESS FROM THE PROPOSED DEVELOPED ACROSS THE MARRIOTT HOTEL PROPERTY TO FRIDAY CENTER DRIVE.
-EXISTING SOILS ARE WHITE STORE SANDY LOAM



MARRIOTT WAY



- CONSTRUCTION MANAGEMENT NOTES:**
- ALL LARGE TRUCKS AS RELATED TO VEHICULAR CONSTRUCTION TRAFFIC SHALL ENTER/LEAVE THE SITE THROUGH THE TEMPORARY CONSTRUCTION ENTRANCE
 - CONSTRUCTION WILL BE LIMITED TO BETWEEN 7:00AM AND 6:00PM MONDAY THROUGH FRIDAY TO COMPLY WITH THE TOWN OF CHAPEL HILLS NOISE ORDINANCE
 - THE CONTRACTOR SHALL PROVIDE WORK ZONE TRAFFIC CONTROL DEVICES
 - NO OPEN BURNING SHALL BE PERMITTED
 - THE APPLICANT SHALL POST A CONSTRUCTION SIGN ON THE DEVELOPMENT SITE THAT LISTS THE FOLLOWING:
 - PROPERTY OWNER'S REPRESENTATIVE & PHONE NUMBER
 - CONTRACTOR'S REPRESENTATIVE & PHONE NUMBER
 - TELEPHONE NUMBER FOR REGULATORY INFORMATION AT THE TIME OF BUILDING PERMIT
 - SIGN MAY BE A MAXIMUM OF 32SF AND A MAXIMUM HEIGHT OF 8FT

IMPERVIOUS AREA SUMMARY

ITEM	AREA (SF)	ACRE(S)	% OF TOTAL AREA
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SEAL:

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APPLICATION DRAWINGS
PRELIMINARY
DO NOT USE
FOR CONSTRUCTION

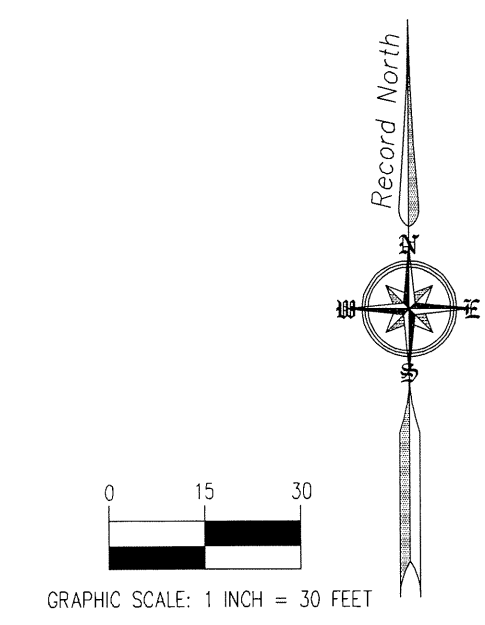
TRI-CITY MEDICAL BUILDING
RALEIGH ROAD (NC HWY 54 & BARBEE CHAPEL ROAD)
CHAPEL HILL, NORTH CAROLINA
DURHAM COUNTY
PARCEL # 9798-04-72-5824

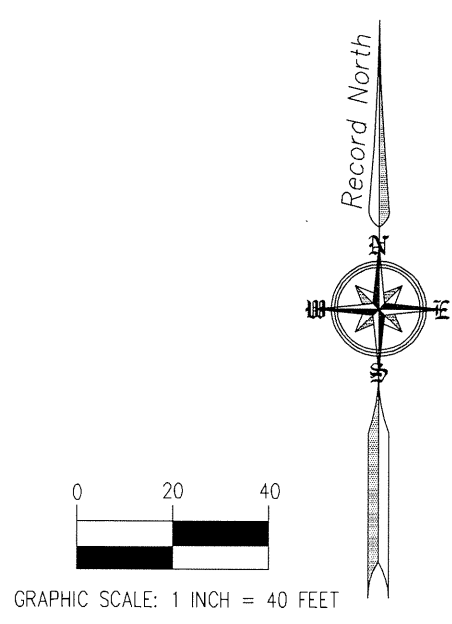
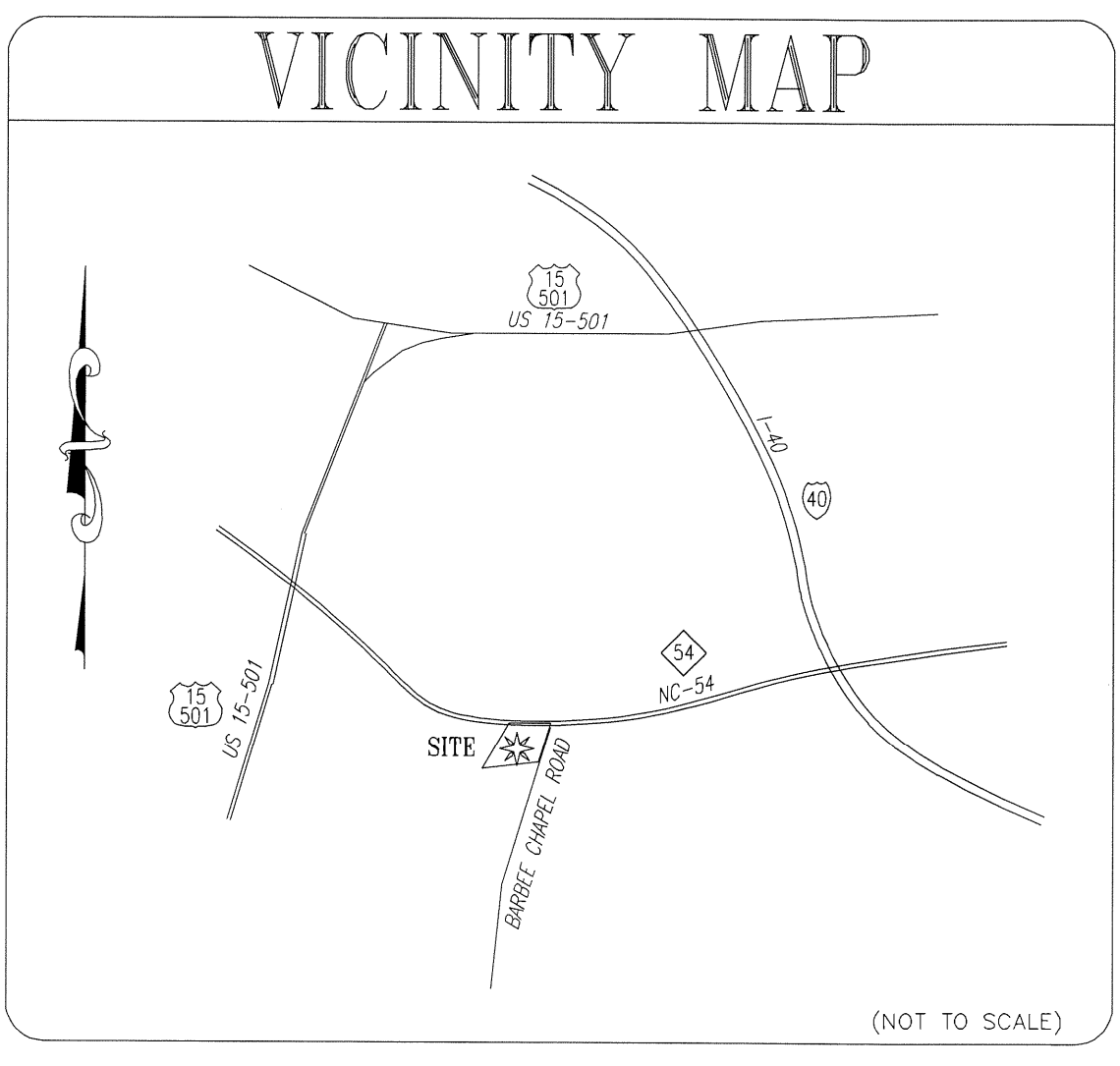
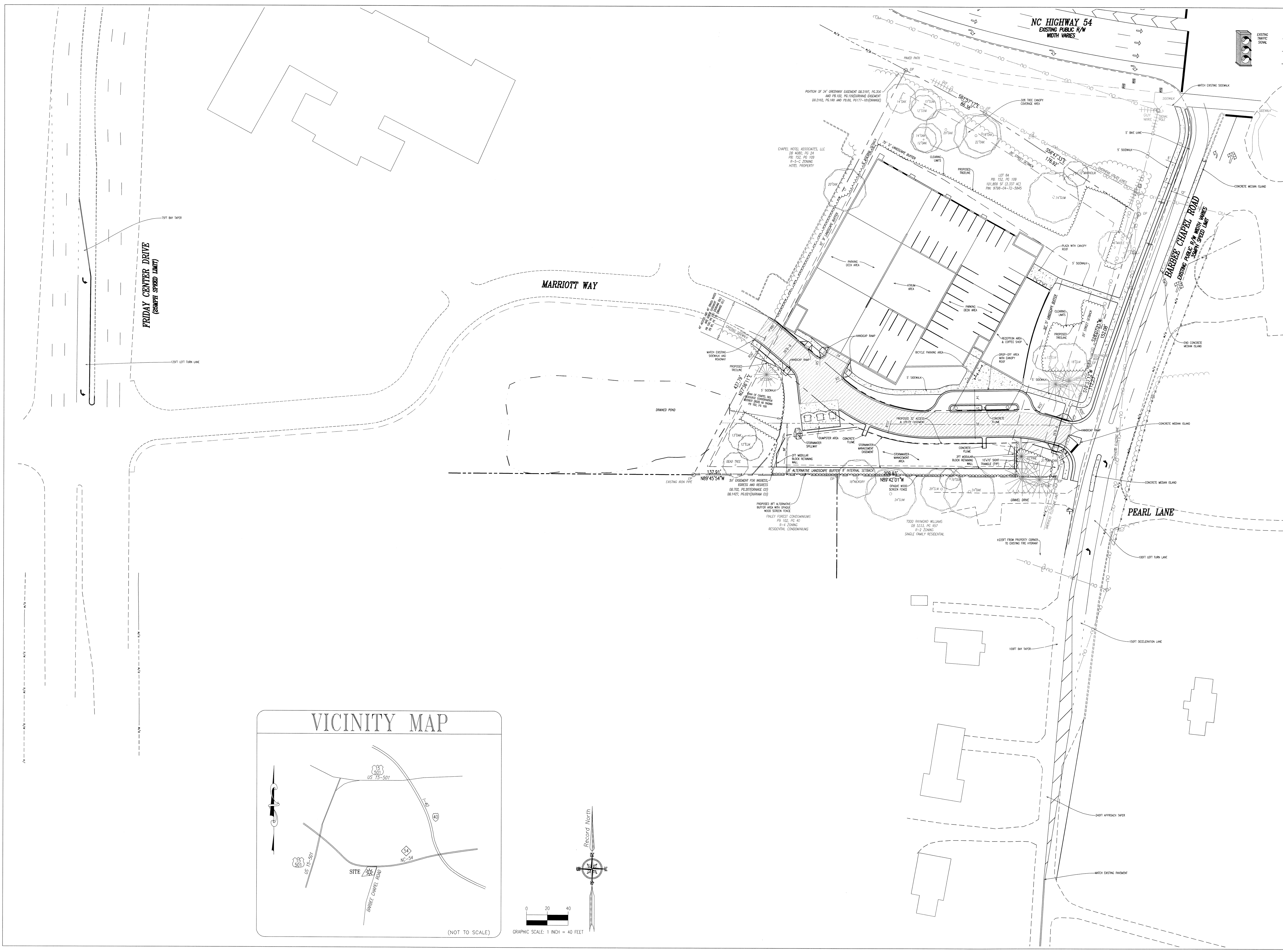
OWNER/DEVELOPER:
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REVISIONS:

3/27/15 Revised per Town review comments, traffic study & Master Plan Update

LAYOUT COORD: MEL
PLANNING MGR. MEL
DRAWING BY: MEL
DATE: 10/31/14
JOB NUMBER: 004080
TITLE: **SITE PLAN**
SHEET NUMBER: **C1.0**
COMMENTS:





TRIANGLE SITE DESIGN

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 NC LICENSE #P-0619

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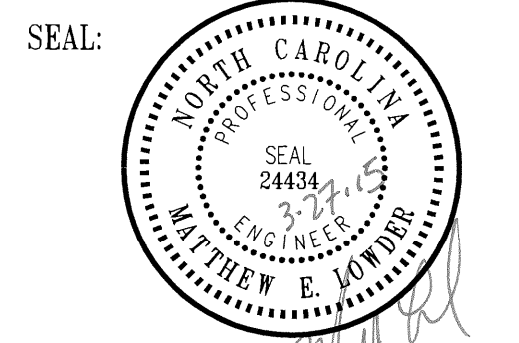
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TRI-CITY MEDICAL BUILDING
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 PARCEL # 9798-04-72-5824

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REVISIONS:
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LAYOUT COORD: MEL
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 SHEET NUMBER: C1.0A
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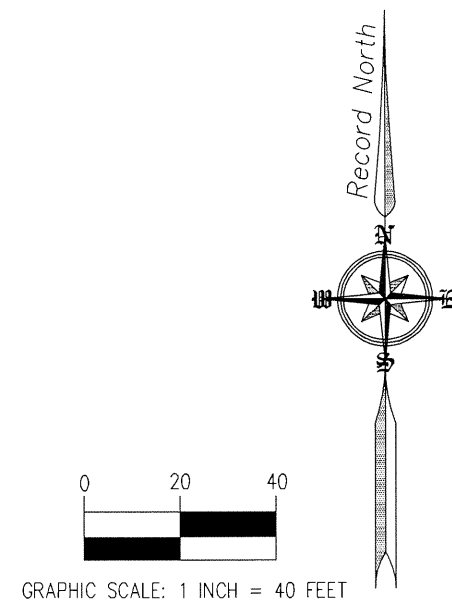
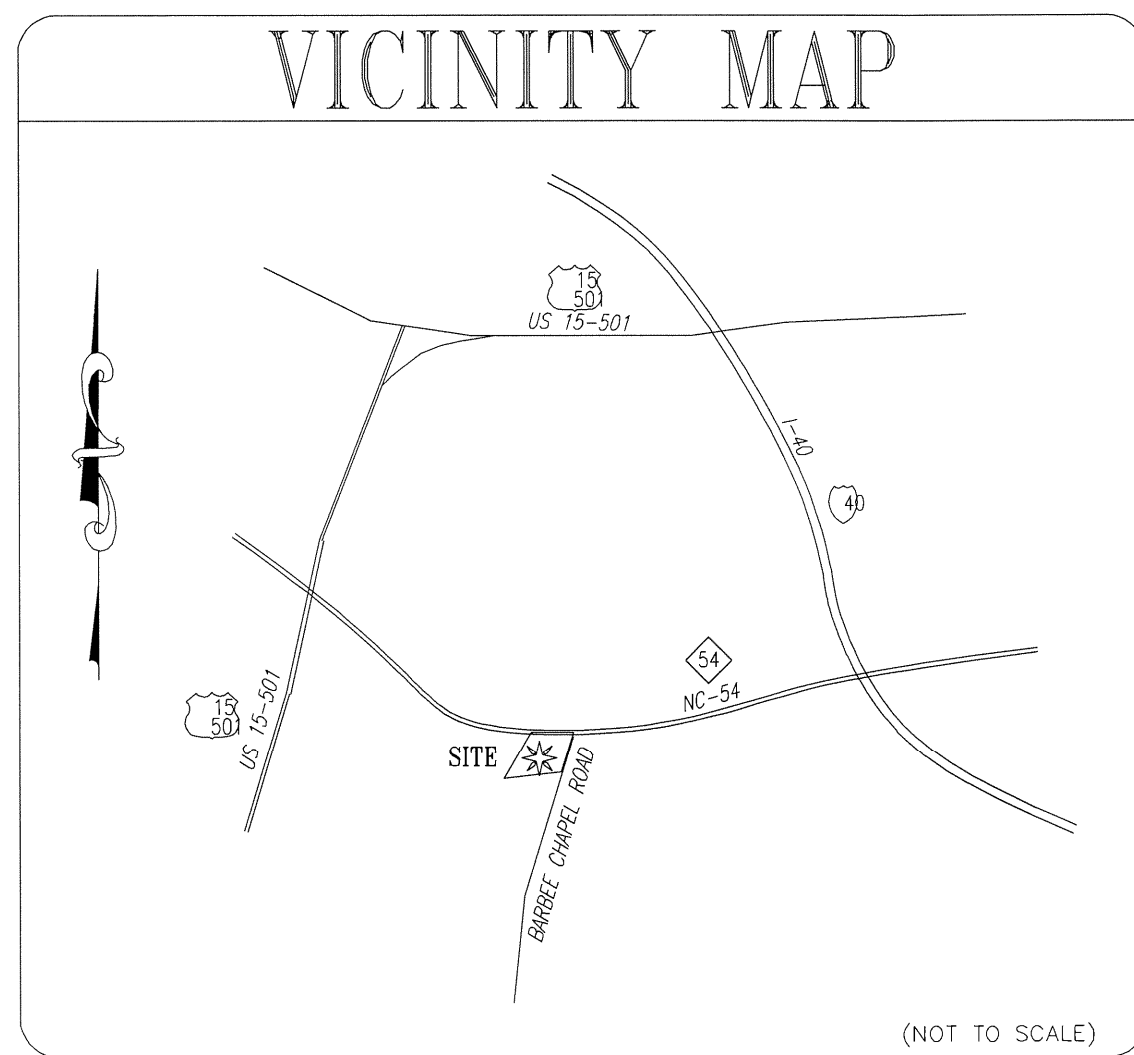
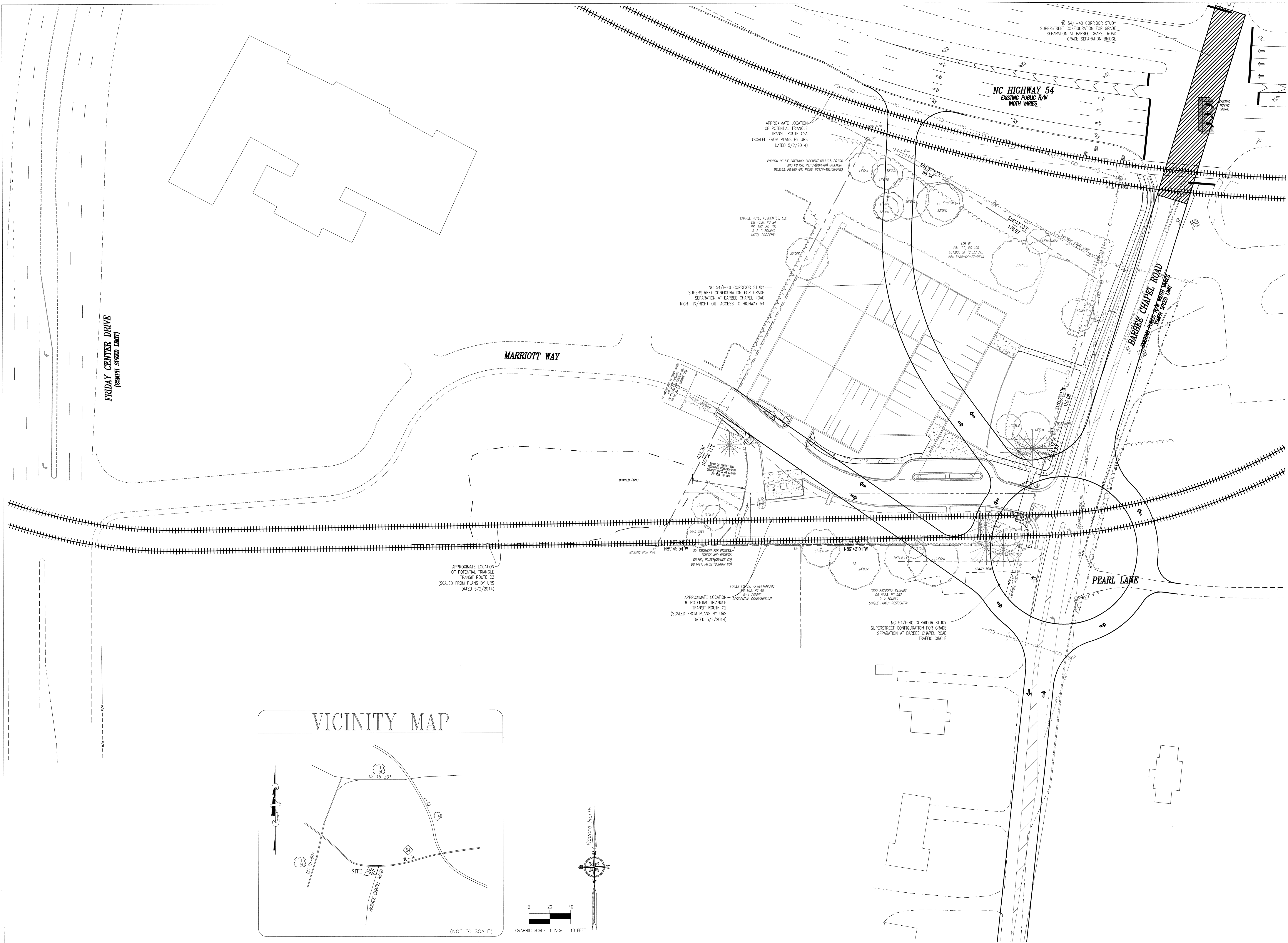
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POTENTIAL PUBLIC IMPROVEMENTS PLAN
SHEET NUMBER:

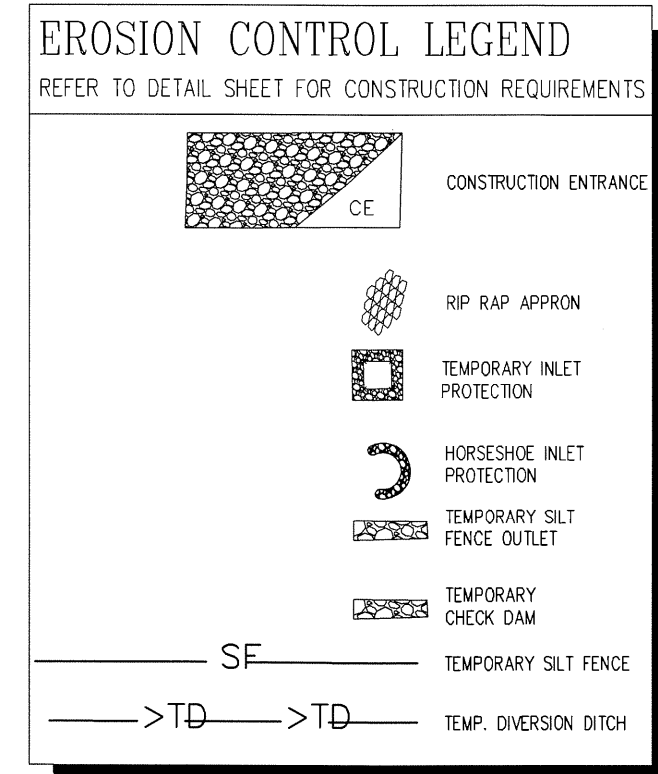
C1.0B

COMMENTS:



GRADING NOTES:

- REFER TO THE SITE PLAN FOR RELATED NOTES.
- ALL CONTOURS AND SPOT ELEVATIONS REFLECT FINISHED GRADES.
- ALL ELEVATIONS ARE IN REFERENCE TO THE BENCHMARK, AND THIS MUST BE VERIFIED BY THE GENERAL CONTRACTOR PRIOR TO GROUND BREAKING.
- THE CONTRACTOR SHALL IMMEDIATELY REPORT TO OWNER ANY DISCREPANCIES FOUND BETWEEN ACTUAL FIELD CONDITIONS AND CONSTRUCTION DOCUMENTS AND SHALL WAIT FOR INSTRUCTION PRIOR TO PROCEEDING.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR LOCATING AND PROTECTING EXISTING UTILITIES, AND SHALL REPAIR ALL DAMAGE TO EXISTING UTILITIES THAT OCCUR DURING CONSTRUCTION.
- THE CONTRACTOR SHALL BLEND NEW EARTHWORK SMOOTHLY TO TRANSITION BACK TO EXISTING GRADE.
- LIMITS OF CLEARING SHOWN ON GRADING PLAN ARE BASED UPON THE APPROXIMATE CUT AND FILL SLOPE LIMITS, OR OTHER GRADING REQUIREMENTS.
- THE PROPOSED CONTOURS SHOWN IN DRIVES AND PARKING LOTS AND SIDEWALKS ARE FINISHED ELEVATIONS INCLUDING ASPHALT. REFER TO PAVEMENT CROSS SECTION DATA TO ESTABLISH CORRECT SUB-BASE OR AGGREGATE BASE COURSE ELEVATIONS TO BE COMPLETED UNDER THIS CONTRACT.
- THE CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE SO THAT RUNOFF WILL DRAIN BY GRAVITY FLOW ACROSS NEW PAVEMENT AREAS TO NEW OR EXISTING DRAINAGE INLETS OR SHEET OVERLAND.
- ANY GRADING, BEYOND THE LIMITS OF CONSTRUCTION AS SHOWN ON THE GRADING PLAN, IS SUBJECT TO A FINE.
- GRADING MORE THAN ONE ACRE WITHOUT AN APPROVED EROSION CONTROL PLAN IS SUBJECT TO A FINE.
- STABILIZATION IS THE BEST FORM OF EROSION CONTROL. TEMPORARY SEEDING IS NECESSARY TO ACHIEVE EROSION CONTROL ON DENUDED AREAS AND ESPECIALLY WHEN THE CONSTRUCTION SEQUENCE REQUIRES IT.
- ALL GRADED SLOPES ARE TO BE SEEDED OR LANDSCAPED WITHIN 21 DAYS OF COMPLETION OF GRADING. ALL REMAINING AREAS ARE TO BE SEEDED WITHIN 15 WORKING DAYS.
- EXISTING GRADES, CONTOURS, UTILITIES AND OTHER EXISTING FEATURES FROM SURVEY BY PHILIP POST & ASSOCIATES.
- THE CONTRACTOR SHALL INCLUDE IN THE CONTRACT PRICE ANY DE-WATERING NECESSARY TO CONSTRUCT THE PROJECT AS SHOWN ON THE PLANS.
- THE CONTRACTOR IS RESPONSIBLE FOR THE DESIGN AND IMPLEMENTATION OF ALL SHEETING, SHORING, BRACING AND SPECIAL EXCAVATION MEASURES REQUIRED TO MEET OSHA, FEDERAL, STATE, AND LOCAL REGULATIONS PURSUANT TO THE INSTALLATION OF THE WORK INDICATED ON THESE DRAWINGS. THE DESIGN ENGINEER ACCEPTS NO RESPONSIBILITY FOR THE DESIGN(S) TO INSTALL SAID ITEMS.
- THE CONTRACTOR SHALL REFER TO THE ARCHITECTURAL PLANS FOR EXACT LOCATION, ELEVATION, AND DIMENSIONS OF EXIT DOORS, RAMPS, BUILDING DIMENSIONS, AND EXACT BUILDING UTILITY ENTRANCE LOCATIONS.
- ALL FILL MATERIALS, EXISTING BUILDING FOUNDATIONS, PAVEMENT AND UTILITY STRUCTURES, TOPSOIL, AND ANY OTHER DELETERIOUS MATERIALS SHALL BE COMPLETELY REMOVED FROM WITHIN THE BEARING ZONE BELOW THE STRUCTURE.
- ALL FOUNDATION EXCAVATION SHALL BE INSPECTED BY A QUALIFIED GEOTECHNICAL REPRESENTATIVE TO DETERMINE WHETHER UNSUITABLE MATERIAL MUST BE REMOVED. ALL UNSUITABLE MATERIAL SHALL BE REMOVED, BACKFILLED AND COMPACTED AS REQUIRED BY THE GEOTECHNICAL REPRESENTATIVE.
- ALL CUT OR FILL SLOPES SHALL BE 3:1 OR FLATTER UNLESS OTHERWISE NOTED OR DEPICTED.
- THE CONTRACTOR SHALL ADHERE TO ALL TERMS & CONDITIONS AS OUTLINED IN THE GENERAL N.P.D.E.S PERMIT FOR STORM WATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
- CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.
- CONTRACTOR SHALL ENSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL NATURAL AND PAVED AREAS.
- ALL UN-SURFACED AREAS DISTURBED BY GRADING OPERATION SHALL RECEIVE 4 INCHES OF TOPSOIL. CONTRACTOR SHALL APPLY STABILIZATION FABRIC TO ALL SLOPES 3:1V OR STEEPER.
- CONSTRUCTION SHALL COMPLY WITH ALL APPLICABLE GOVERNING CODES AND BE CONSTRUCTED TO SAME.

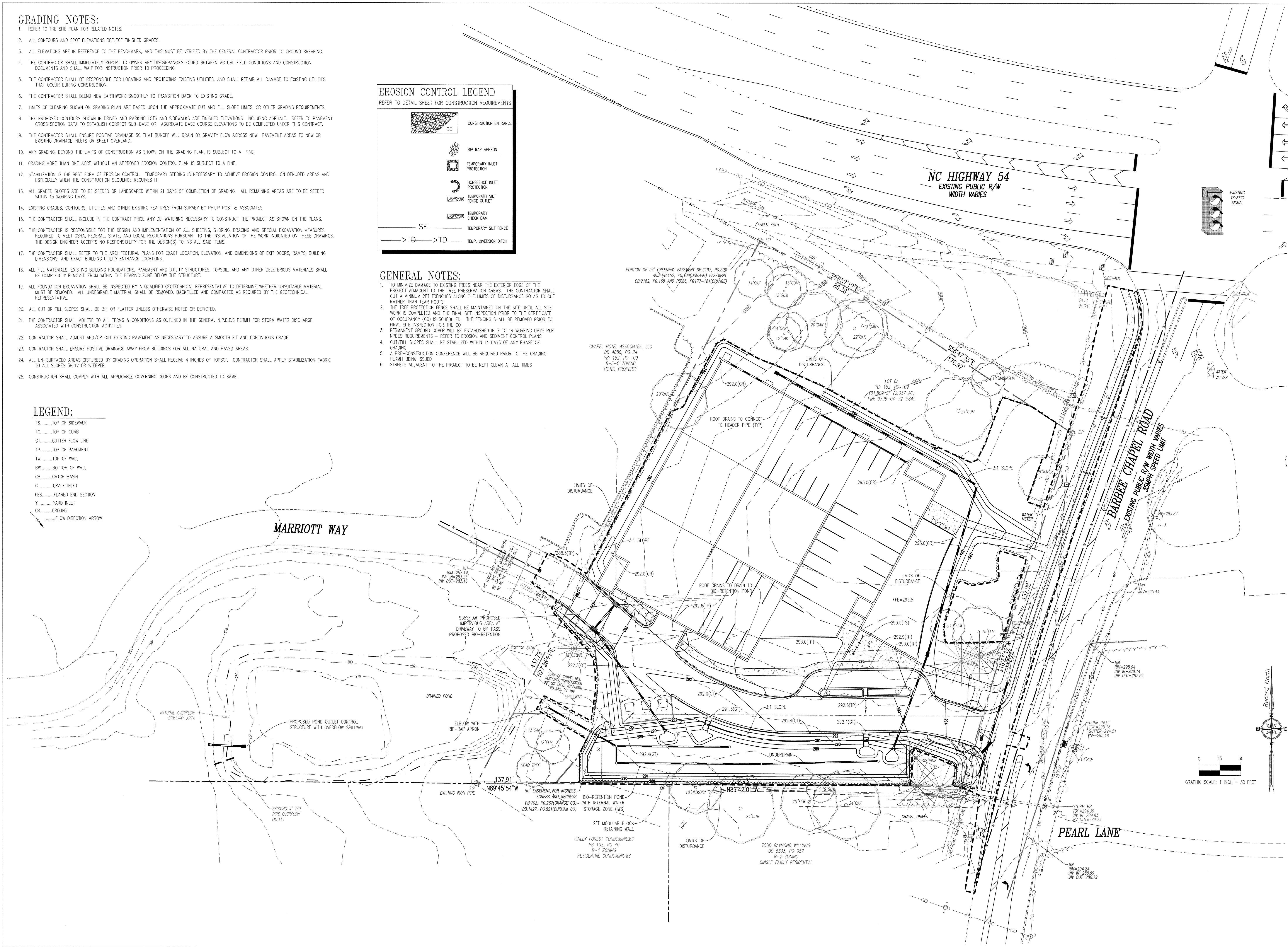


GENERAL NOTES:

- TO MINIMIZE DAMAGE TO EXISTING TREES NEAR THE EXTERIOR EDGE OF THE PROJECT ADJACENT TO THE TREE PRESERVATION AREAS, THE CONTRACTOR SHALL CUT A MINIMUM 2FT TRENCHES ALONG THE LIMITS OF DISTURBANCE SO AS TO CUT RATHER THAN TEAR ROOTS.
- THE TREE PROTECTION FENCE SHALL BE MAINTAINED ON THE SITE UNTIL ALL SITE WORK IS COMPLETED AND THE FINAL SITE INSPECTION PRIOR TO THE CERTIFICATE OF OCCUPANCY (CO) IS SCHEDULED. THE FENCING SHALL BE REMOVED PRIOR TO FINAL SITE INSPECTION FOR THE CO.
- PERMANENT GROUND COVER WILL BE ESTABLISHED IN 7 TO 14 WORKING DAYS PER NPDES REQUIREMENTS - REFER TO EROSION AND SEDIMENT CONTROL PLANS.
- CUT/FILL SLOPES SHALL BE STABILIZED WITHIN 14 DAYS OF ANY PHASE OF GRADING.
- A PRE-CONSTRUCTION CONFERENCE WILL BE REQUIRED PRIOR TO THE GRADING PERMIT BEING ISSUED.
- STREETS ADJACENT TO THE PROJECT TO BE KEPT CLEAN AT ALL TIMES.

LEGEND:

- TS.....TOP OF SIDEWALK
- TC.....TOP OF CURB
- GT.....GUTTER FLOW LINE
- TP.....TOP OF PAVEMENT
- TW.....TOP OF WALL
- BW.....BOTTOM OF WALL
- CB.....CATCH BASIN
- Q.....GRATE INLET
- FES.....FLARED END SECTION
- YL.....YARD INLET
- GR.....GROUND
-FLOW DIRECTION ARROW



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TITLE:	GRADING AND DRAINAGE PLAN
SHEET NUMBER:	C1.1

COMMENTS:

UTILITY NOTES:

1. ALL PROPOSED UTILITIES SHALL COMPLY WITH THE ORANGE WATER AND SEWER AUTHORITY STANDARDS AND SPECIFICATIONS
2. UTILITY WORK WITHIN THE RIGHT-OF-WAY WILL REQUIRE AN ENCROACHMENT AGREEMENT WITH THE NCDOT, OWSA AND THE DEVELOPER
3. THE NORTH CAROLINA FIRE CODE REQUIRES THAT VEHICLE ACCESS FOR FIRE FIGHTING SHALL BE PROVIDED TO ALL CONSTRUCTION OR DEMOLITION SITES. PROVIDE VEHICLE ACCESS TO WITHIN 100FT OF TEMPORARY OR PERMANENT FIRE DEPARTMENT CONNECTIONS AND HYDRANTS.

UTILITY LEGEND	
	NEW POWER POLE
	NEW WATER METER
	NEW GAS METER
	NEW ELECTRIC METER
	NEW WATER SERVICE (REFER TO CIVIL DRAWINGS)
	NEW GAS SERVICE
	NEW SANITARY SEWER (REFER TO CIVIL DRAWINGS)
	NEW SANITARY SEWER CLEANOUT (AT EVERY 60 FEET)
	NEW UNDERGROUND POWER (120/208/3-Ø SERVICE)
	NEW UNDERGROUND TELEPHONE SERVICE

UTILITY SERVICE NOTES:

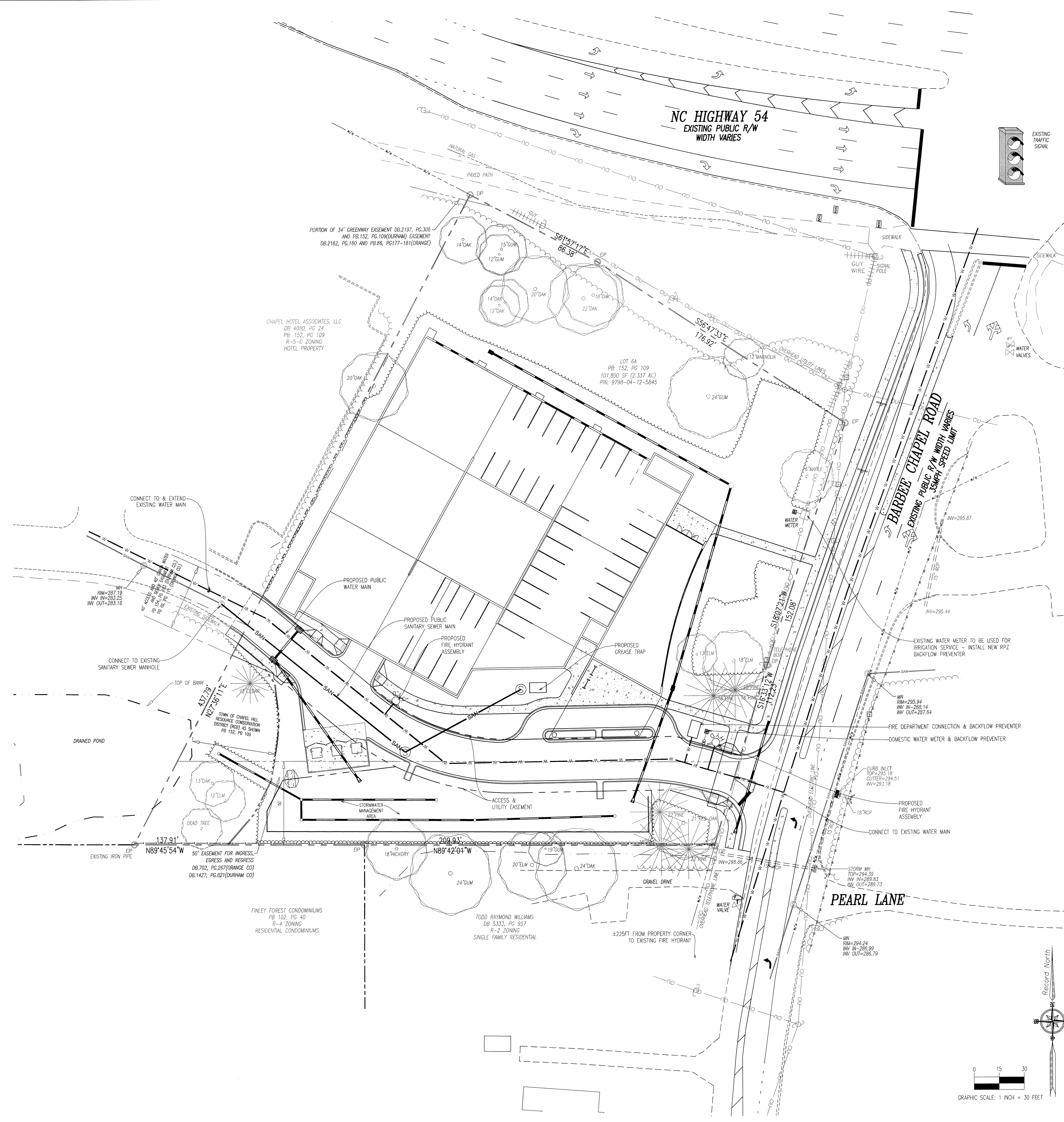
WATER SERVICE
GENERAL CONTRACTOR TO PROVIDE AND INSTALL A 1" WATER SERVICE LINE FROM PROPOSED WATER MAIN TO BUILDING PER MUNICIPAL REQUIREMENTS.
CONTACT: ORANGE WATER & SEWER AUTHORITY
TELEPHONE: 919-866-4421

ELECTRIC SERVICE
POWER CO. TO PROVIDE UNDERGROUND 120/208/3 PHASE SERVICE. GENERAL CONTRACTOR TO PROVIDE AND INSTALL TWO 4" DIA. CONDUIT W/ PULL WIRE TO UTILITY COMPANY POINT OF CONNECTION.
CONTACT: TBD
TELEPHONE: TBD

TELEPHONE SERVICE
TELEPHONE CO. TO PROVIDE NEW UNDERGROUND SERVICE. GENERAL CONTRACTOR TO PROVIDE AND INSTALL TWO 4" DIA. CONDUIT W/ PULL WIRE FROM PHONE BOARD TO UTILITY COMPANY POINT OF CONNECTION.
CONTACT: TBD
TELEPHONE: TBD

SANITARY SEWER
GENERAL CONTRACTOR TO PROVIDE AND INSTALL 4" SCHEDULE 40 PVC FROM PROPOSED SEPTIC SYSTEM TO LAST CLEAN OUT OUTSIDE OF BUILDING (MIN. 1% SLOPE). PROVIDE CLEAN OUTS EVERY 75' (TYPICAL).
CONTACT: ORANGE WATER & SEWER AUTHORITY
TELEPHONE: 919-866-4421

NATURAL GAS
GENERAL CONTRACTOR TO COORDINATE WITH NATURAL GAS UTILITY FOR SERVICE LINE TO PROPOSED BUILDING.
CONTACT: TBD
TELEPHONE: TBD



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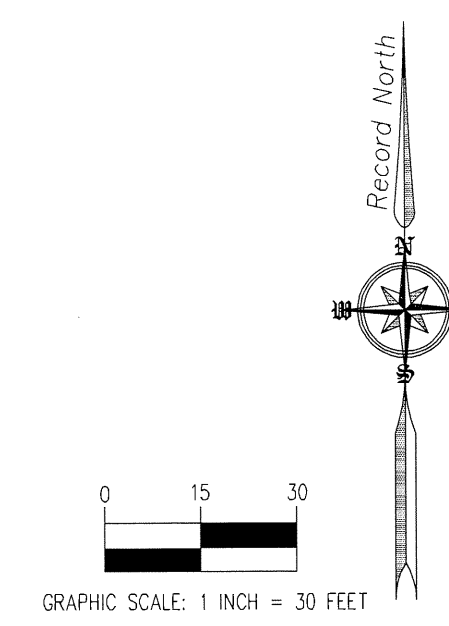
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TITLE:
UTILITY PLAN

SHEET NUMBER:
C1.2

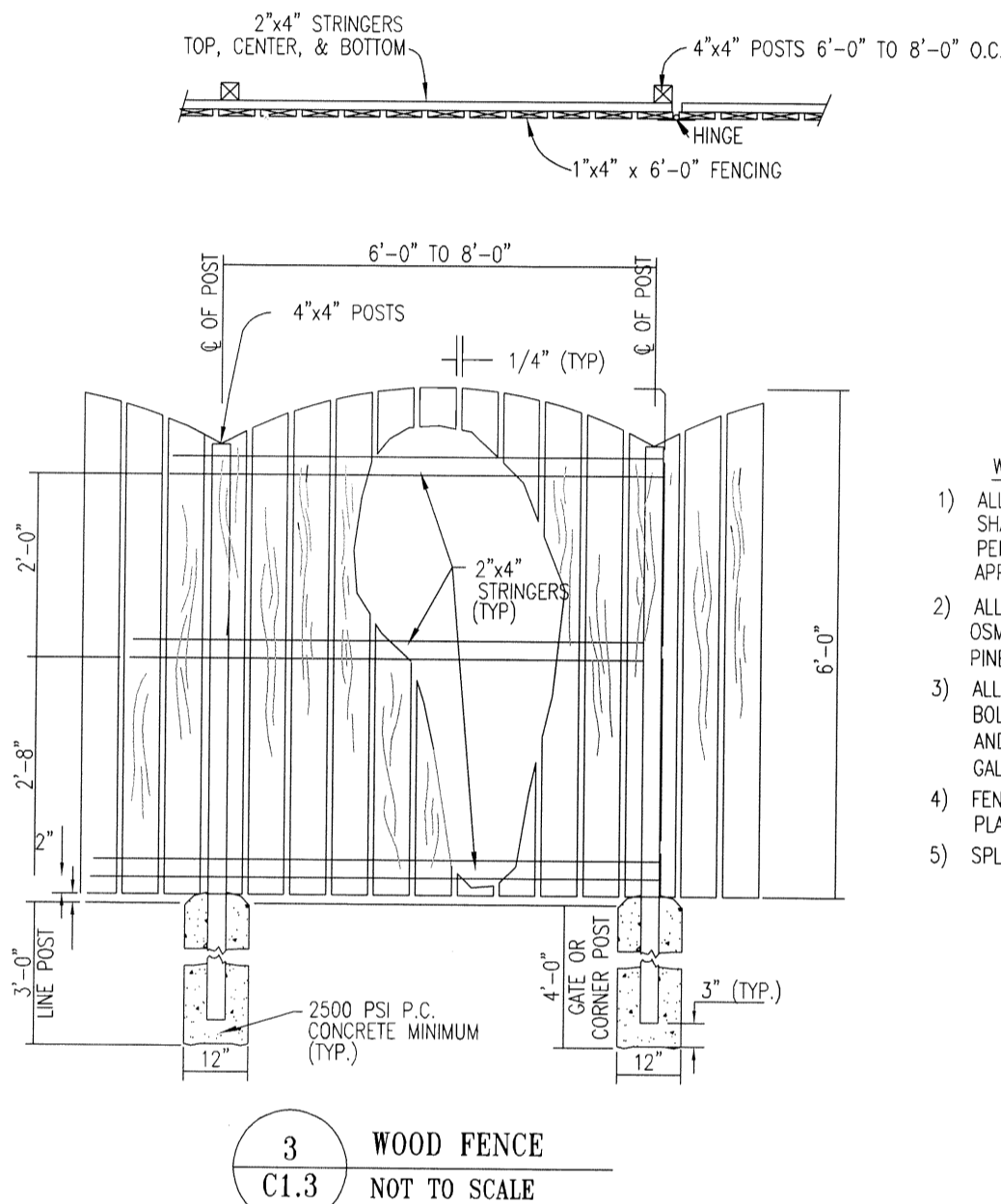
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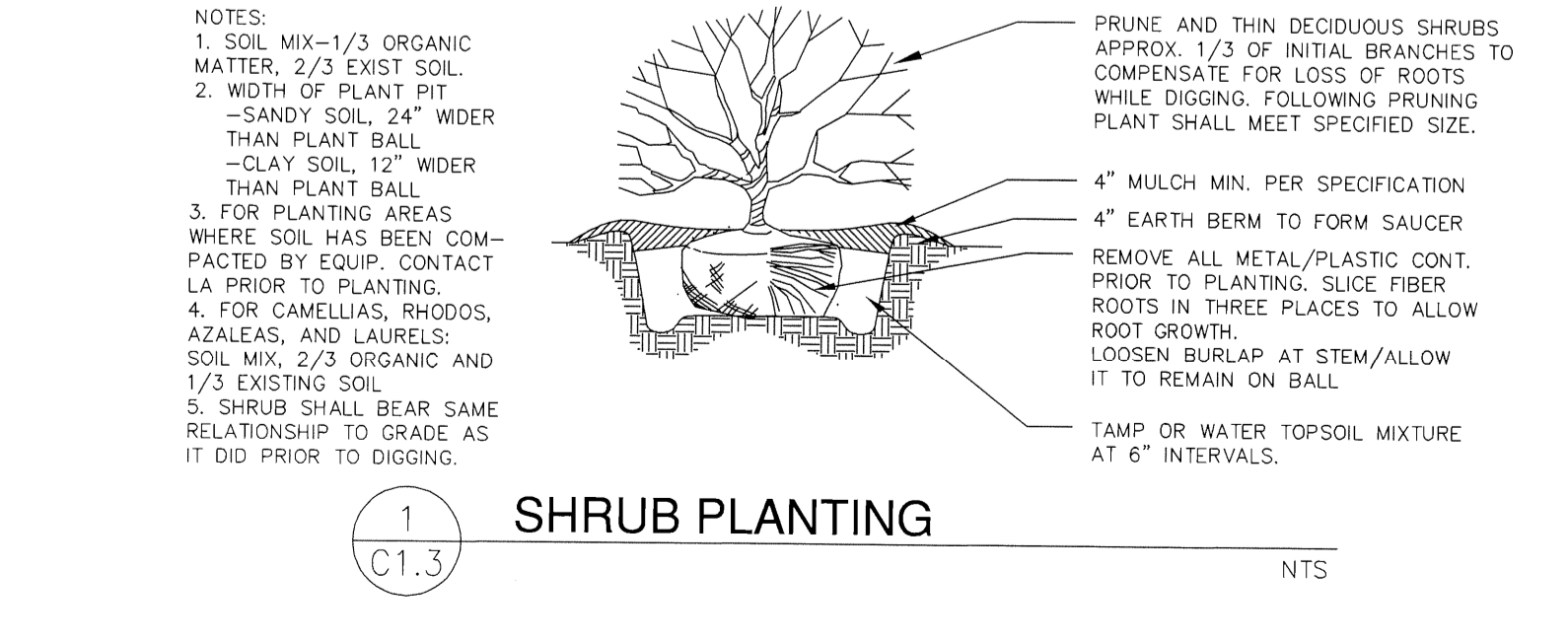
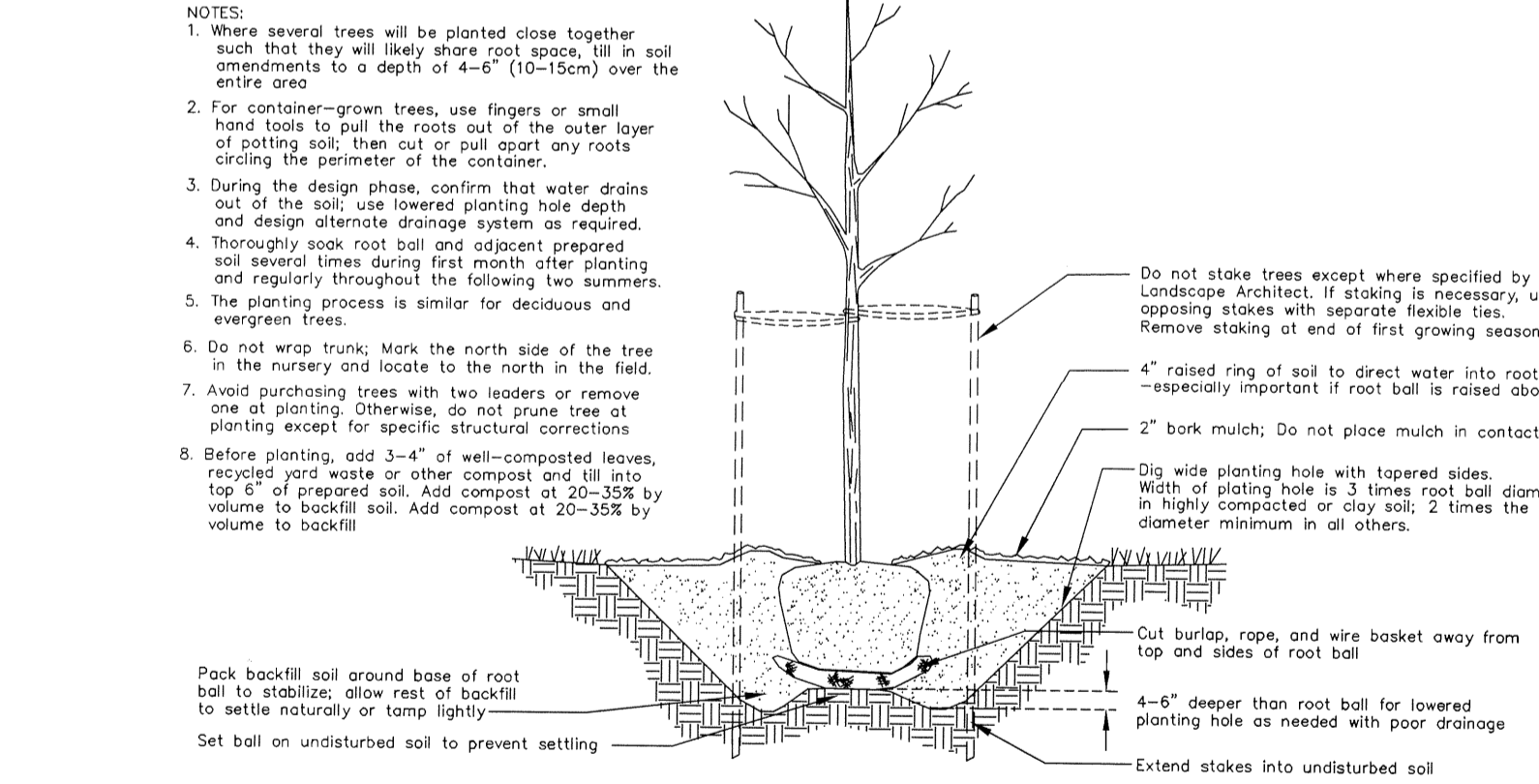
- GENERAL NOTES:**
- PROJECT IS LOCATED WITHIN THE MEADOWMONT DEVELOPMENT
 - THE APPROVED MEADOWMONT MASTER SITE PLAN ALLOCATES 19,200SF OF BUILDING AREA FOR THE PROPOSED PARCEL - A MINOR MODIFICATION TO THE MEADOWMONT MASTER SITE PLAN IS REQUESTED TO INCREASE THE PROPOSED BUILDING AREA TO 60,000SF (40,800SF INCREASE IN BUILDING AREA/PROPOSED FAR=0.6)
 - A 75FT TYPE "D" LANDSCAPE BUFFER IS REQUIRED ALONG THE NC-HIGHWAY 54 FRONTAGE PER THE APPROVED MEADOWMONT MASTER SITE PLAN
 - A 50FT TYPE "D" LANDSCAPE BUFFER IS REQUIRED ALONG THE BARBEE CHAPEL ROAD FRONTAGE PER THE APPROVED MEADOWMONT MASTER SITE PLAN
 - PARKING FOR THE PROPOSED MEDICAL OFFICE BUILDING WILL BE WITHIN A 3-LEVEL PARKING DECK UNDER THE PROPOSED OFFICE AREA
 - MEDICAL OFFICE PARKING RATE: 1 SPACE PER 2,250 SF OFFICE AREA
60,000SF/2,250 = 267 SPACES
10% ALLOWABLE PARKING REDUCTION=27 SPACES
PROPOSED NUMBER OF SPACES=240
 - 43,550SF OF IMPERVIOUS AREA HAS BEEN ALLOCATED TO THE PARCEL AS PART OF THE MEADOWMONT DEVELOPMENT (42,798 IMPERVIOUS)
 - A BUFFER MODIFICATION IS REQUESTED TO ALLOW SELECTIVE THINNING OF EXISTING TREES AND SHRUBS WITHIN THE EXISTING PERIMETER BUFFER AREAS
 - THERE IS AN EXISTING RESOURCE CONSERVATION DISTRICT (RCD) AREA ON THE PARCEL THAT HAS BEEN RECORDED IN DB 152, PG. 109 AS SHOWN
 - THE PROPERTY IS NOT LOCATED WITHIN A FLOOD HAZARD ZONE PER FEMA FIRM PANEL # 371097800K DATED 2/2/2007
 - EXISTING CONDITIONS:
-SITE IS CURRENTLY WOODED WITH AN EXISTING HOUSE & OTHER SMALL STRUCTURES THAT WILL BE DEMOLISHED AS PART OF THE DEVELOPMENT OF THE PROPERTY
-PROPERTY IS BOUNDED BY NC HWY 54 AND BARBEE CHAPEL ROAD TO THE NORTH AND EAST. THE PROPERTY TO THE WEST IS AN EXISTING MARRIOTT HOTEL. THE PROPERTY TO THE SOUTH IS AN EXISTING SINGLE FAMILY RESIDENTIAL PARCEL.
-SITE IS WITHIN THE JORDAN RIVER WATERSHED (UPPER NEW HOPE WATERSHED)
-EXISTING CROSS-ACCESS EASEMENTS GRANTS ACCESS FROM THE PROPOSED DEVELOPED ACROSS THE MARRIOTT HOTEL PROPERTY TO FORDY CENTER DRIVE.
-EXISTING SOILS ARE WHITE STONE SANDY LOAM

- LANDSCAPING CALCULATIONS:**
- NORTH BUFFER: 263LF
REQUIRED LARGE TREES 16
PROPOSED LARGE TREES 4
12 EXISTING TREES
- REQUIRED SMALL TREES 12
PROVIDED SMALL TREES 12
EXISTING VEGETATION PER TREE SAVE AREA #2
- REQUIRED SHRUBS 105
PROVIDED SHRUBS 105
EXISTING VEGETATION PER TREE SAVE AREA #2
- SOUTH BUFFER: 250LF
REQUIRED LARGE TREES 10
PROPOSED LARGE TREES 4
11 EXISTING
- REQUIRED SMALL TREES 20
PROVIDED SMALL TREES 20
EXISTING VEGETATION PER TREE SAVE AREA #2
- REQUIRED SHRUBS 75
PROVIDED SHRUBS 75
EXISTING VEGETATION PER TREE SAVE AREA #2
- WEST BUFFER: 132LF (SUBTRACTED ROAD, EASEMENT, AND TREE SAVE AREAS)
REQUIRED LARGE TREES 13
PROPOSED LARGE TREES 5
EXISTING
- REQUIRED SMALL TREES 9
PROVIDED SMALL TREES 10
- REQUIRED SHRUBS 16
PROVIDED SHRUBS 27
- EAST BUFFER: 245LF (EASEMENT AND TREE SAVE AREAS)
REQUIRED LARGE TREES 13
PROPOSED LARGE TREES 10
10 PROPOSED
3 EXISTING
- REQUIRED SMALL TREES 26
PROVIDED SMALL TREES 25
EXISTING TREE LINE PROVIDES ADDITIONAL SCREENING
- REQUIRED SHRUBS 93
PROVIDED SHRUBS 75
EXISTING VEGETATION PROVIDES ADDITIONAL SCREENING
- TREE CANOPY CALCULATION:
TOTAL GROSS LOT AREA: 101,800SF
NET LOT AREA: 78,128SF (NOT INCLUDING ACTIVE CLEARED EASEMENTS)
REQUIRED TREE CANOPY AREA: 214,385SF (304%)
EXISTING TREE CANOPY AREA: 27,568SF
TREE CANOPY DEFICIT: 470SF
REPLACEMENT TREES REQUIRED: 1 TREE PER 500SF = 1 TREE

- PLANTING NOTES:**
- ALL PLANT MATERIALS TO COMPLY WITH AMERICAN STANDARD FOR NURSERY STOCK ANSI Z60.
 - PLANT LOCATIONS TO BE APPROVED IN FIELD PRIOR TO INSTALLATION.
 - SUBSTITUTIONS OF PLANT MATERIALS SPECIFIED CAN ONLY OCCUR WITH PRIOR APPROVAL BY LANDSCAPE ARCHITECT.
 - ESTABLISH PLANT BED CONFIGURATIONS, LANDSCAPE ARCHITECT TO APPROVE BED LAYOUT IN FIELD.
 - INSTALL PLANTS AND MULCH BEDS WITH 4" OF SHREDED HARDWOOD MULCH.
 - ESTABLISH MULCH BEDS AROUND EXISTING TREES TO REMAIN.
 - PLANTS TO BE RELOCATED TO BE DUG AND REPLANTED IMMEDIATELY.
 - LAWN AREAS TO BE SEED AND STRAWED.
 - LANDSCAPE CONTRACTOR TO MAINTAIN PLANT MATERIALS FOR A ONE YEAR PERIOD FOLLOWING SUBSTANTIAL COMPLETION PER SPECIFICATIONS
 - AREAS DAMAGED FROM PLANT RELOCATION OR OTHER ACTIVITIES OF LANDSCAPE CONTRACTOR TO BE RESEED AND ESTABLISHED AT NO ADDITIONAL COST TO THE OWNER.

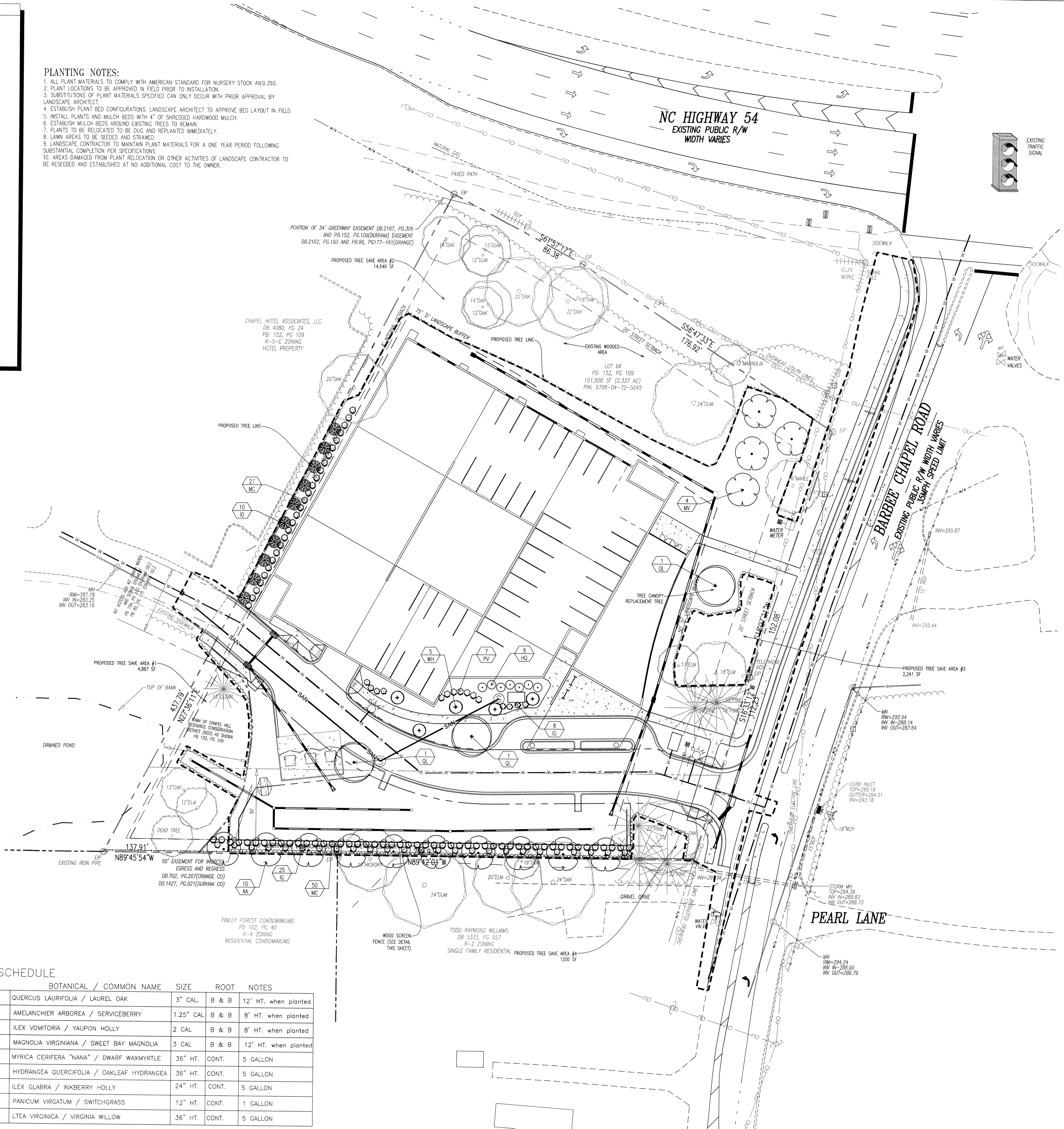


- WOOD FENCE NOTES:**
- ALL POST AND STRINGERS SHALL BE OSMOSIS TREATED OR PENTA TREATED PINE, TO BE APPROVED BY ENGINEER.
 - ALL FENCE SIDING SHALL BE OSMOSIS TREATED OR PENTA TREATED PINE, TO BE APPROVED BY ENGINEER.
 - ALL METAL FASTENERS SUCH AS BOLTS, NAILS, HINGES, HASPS, AND ETC. SHALL BE HOT DIPPED GALVANIZED.
 - FENCE DIRECTION AS NOTED ON PLANS OR AS DIRECTED BY OWNER.
 - SPLICE STRINGERS AT POSTS ONLY.



PLANT SCHEDULE

KEY	#	BOTANICAL / COMMON NAME	SIZE	ROOT	NOTES
QL	3	QUERCUS LAURIFOLIA / LAUREL OAK	3" CAL.	B & B	12" HT. when planted
AA	10	AMELANCHIER ARBOREA / SERVICEBERRY	1.25" CAL	B & B	8' HT. when planted
IO	10	ILEX VOMITORIA / YAUPOIN HOLLY	2 CAL.	B & B	8' HT. when planted
MV	4	MAGNOLIA VIRGINIANA / SWEET BAY MAGNOLIA	3 CAL.	B & B	12" HT. when planted
MC	77	MYRICA CERIFERA "NANA" / DWARF WAXMYRTLE	36" HT.	CONT.	5 GALLON
HQ	9	HYDRANGEA QUERCIFOLIA / OAKLEAF HYDRANGEA	36" HT.	CONT.	5 GALLON
IG	25	ILEX GLABRA / INKBERRY HOLLY	24" HT.	CONT.	5 GALLON
PV	7	PANICUM VIRGATUM / SWITCHGRASS	12" HT.	CONT.	1 GALLON
IV	5	LTEA VIRGINICA / VIRGINIA WILLOW	36" HT.	CONT.	5 GALLON



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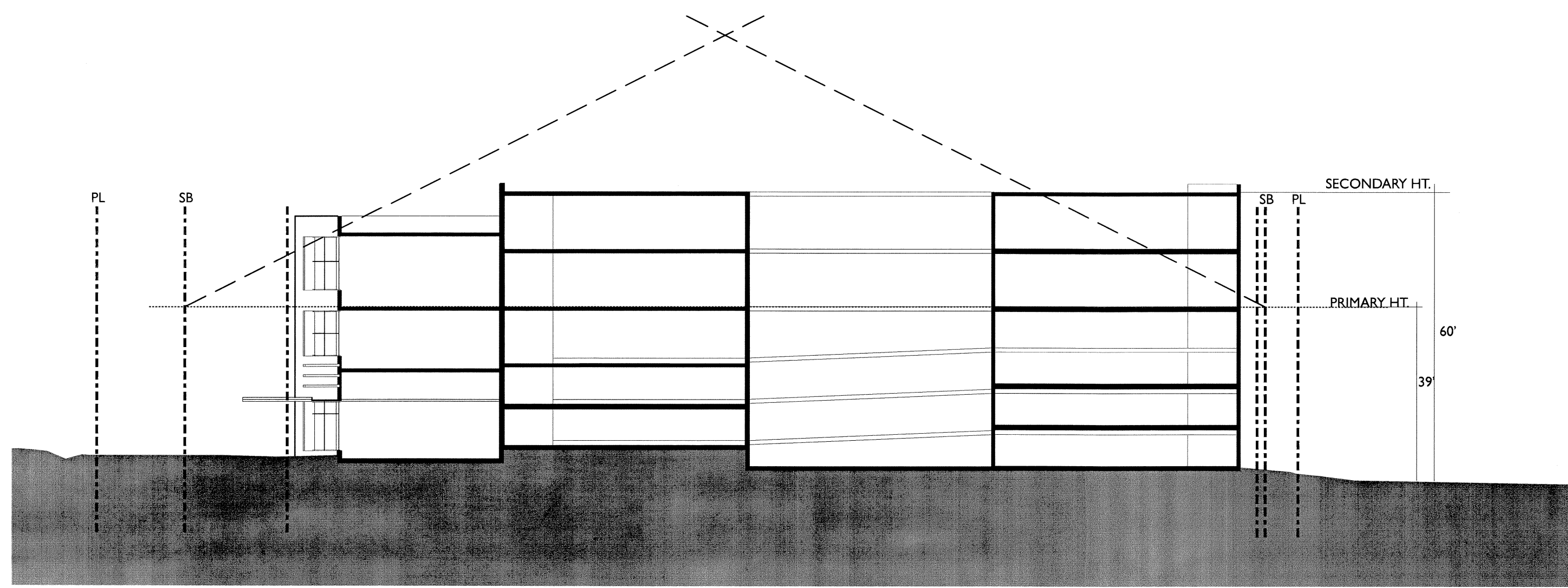
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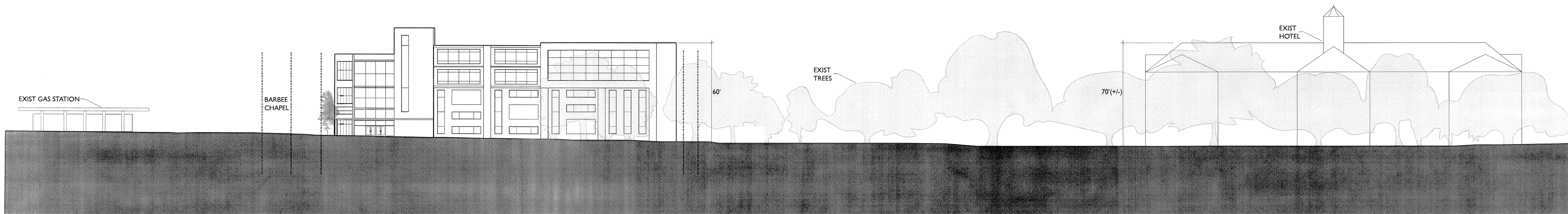
TITLE:
LANDSCAPE PLAN

SHEET NUMBER:
C1.3

COMMENTS:



2 BUILDING ENVELOPE SECTION



1 SITE SECTION

THESE DRAWINGS AND THE ACCOMPANYING SPECIFICATIONS ARE INSTRUMENTS OF SERVICE AND AS SUCH SHALL REMAIN THE PROPERTY OF THE ARCHITECT. THEY HAVE BEEN PREPARED FOR A SPECIFIC PROJECT AND SHALL NOT BE USED IN CONNECTION WITH ANY OTHER PROJECTS WITHOUT PRIOR WRITTEN PERMISSION OF THE ARCHITECT.
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SHEET NAME:
SECTIONS

PHASE:
SCHEMATIC DESIGN

REVISIONS:

ISSUE DATE: 10-15-2014
PROJECT #: 14021
DRAWN BY: JDH

SHEET NUMBER

A-1



Coulter Jewell Thames, PA

MAIN OFFICE
111 WEST MAIN STREET
DURHAM, NC 27701
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Planning for the Future

February 25, 2015

Town of Chapel Hill
Planning Department
405 Martin Luther King Jr. Blvd.
Chapel Hill, NC 27514

Reference: **Minor Modification to Existing Meadowmont Master Land Use Plan
Tri-City Medical Building**
5002 Barbee Chapel Road
Chapel Hill, NC 27517
Pin: 9798-04-72-5824

Please find below a summary of the proceedings from the September 15, 2014 Town Council meeting. In particular this summary includes the comments that were made during the earlier Community Design Commission meeting and also those comments directly from Council members the night of the meeting.

Community Design Commission (CDC) Comments:

Neil Robinette with Tri City, Inc. presented the following summary of the CDC comments early in the meeting.

CDC noted the Importance of maintaining the existing buffer along NC HWY 54. Current project plans demonstrate a 20 foot front setback and a 75 foot landscape buffer directly adjacent to NC HWY 54. Project plans have been modified to limit disturbance within the landscape buffer area.

CDC suggested keeping the proposed design within the existing scale and landscape form of the current built environment surrounding the project. Developer believes as the design is refined more and more the proposed building does fit nicely into the built environment especially as it relates to the existing Marriott which the developer feels the proposed project will further complement.

CDC would like the developer to further engage anticipated transportation modes including rail transit, bicycle, pedestrian and the public environment. Developer feels the proposed concept proactively address's the public space, greenway path and future rail transit system.

CDC would like the developer to offer a signature building while working to camouflage the parking garage. Developer believes the proposed design is consistent with this request as the parking garage is being concealed nicely and is aesthetically pleasing.

CDC was concerned about the Floor Area Ratio (FAR) and if a variance would be needed. Developer noted the site is entitled for a 30% FAR. The proposed development would be more in the range of a 60% FAR which is greater. Developer noted the 30% FAR is a limited use of the property. Developer feels the 60% FAR would fit better into the Towns long term goals and objectives as to provide a more dense development on the site in an appropriate manner.

Council Comments from September 15, 2014:

After Neil Robinette with Tri City, Inc. completed his presentation Council members voiced the following comments.

Council member inquired about the medical research institute use and what kind of clients the developer foresaw using the space. Developer noted they are pursuing several users currently for the site, the most notable being UNC. Developer mentioned having a working relationship with several UNC doctors and that the development group has performed past work for UNC and would like to further the relationship. Developer noted one opportunity is a medical research and education facility for UNC.

Council member inquired if there would be patients coming to the new facility to see their doctor or if workers would be showing up in the am working a full day and leaving at night. Developer noted the use is an institution, meaning the facility would include clinics with patients and doctors on site coming and going throughout the day.

Council member believes the additional traffic all entering and exiting on Barbee Chapel Rd. will only worsen an existing problematic intersection. It was noted the light rail may soften the effects sometime in the future. However in the meantime allowing all the additional trips to the intersection would be a big jump. Council member did agree that it is an appropriate use for the site and that they were just concerned about the traffic.

Council member noted his appreciation for the way the parking is being masked and believes the project as a whole is great. Council member notes traffic on NC HWY 54 backs up almost to interstate 40 at 8:00am and that most people will not be going to this building but those trying to make a left turn here will just make the traffic worse. Council member states the applicant will need to submit a TIA that is very persuasive. Further noting traffic is a huge issue here and the light rail is great but may never happen.

Council member requested a site rendering from eye level at the Barbee Chapel / NC HWY 54 intersection looking towards the proposed site to tell better how prominent the proposed building would really be from NC HWY 54. Developer noted the proposed building facade will pretty much be in line with the adjacent hotel and not within the buffer. The developer also noted the scale of the building and setbacks will more closely represent that of the adjacent hotel than that of the East 54 development.

Mayor inquired what the bicycle/ pedestrian connectivity would be from NC HWY 54 to the proposed building. Developer noted the building will have a plaza where pedestrians and bikes both can stop and use the facilities inside. **Mayor noted the project must ensure the corridor for the future rail is fully respected with the proposed development. Mayor also requested an accurate rendering of the proposed building from the light/intersection. Mayor noted that he believes the Marriot hotel has done disservice to the corridor with being so screened. Mayor questioned why a signature building would be built at an intersection and then screened. Mayor agrees screening can be argued both ways but believes there should be a glimpse of view and something to look at as people are coming into town.**

Ending the meeting the mayor asked for a resolution on transmitting the concept plans. All members of Council including the Mayor were in favor and the above comments were referred.

TRI-CITY MEDICAL BUILDING

TRAFFIC IMPACT STUDY

EXECUTIVE SUMMARY



Prepared for:

The Town of Chapel Hill
Public Works Department
Traffic Engineering Division

Prepared by:

HNTB North Carolina, PC

*343 East Six Forks Road
Suite 200
Raleigh, NC 27609*

NCBELS License #: C-1554

January 2015

HNTB

TRI-CITY MEDICAL BUILDING

TRAFFIC IMPACT STUDY

EXECUTIVE SUMMARY



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January 2015



1-20-15



EXECUTIVE SUMMARY

Project Overview

A medical office development, known as the Tri-City Medical Building, located along NC Highway 54 (Raleigh Road) near its intersection with Barbee Chapel Road is being proposed in Chapel Hill. The project proposes to construct approximately 60,000 square feet of office space on an existing parcel in the southwest quadrant of the NC 54 and Barbee Chapel Road intersection. **Figure ES-1** shows the general location of the site. The project is anticipated to be fully complete by 2016. This report analyzes the complete build-out scenario for the year 2017 (one year after anticipated completion), the no-build scenario for 2017, as well as 2014 existing year traffic conditions.

The proposed site concept plan shows a single full movement access driveway along Barbee Chapel Road that would tie into the existing terminus of Marriott Way to the west. No other vehicular access connections are proposed. **Figure ES-2** displays the preliminary concept plan of the Tri-City Medical Building and nearby land uses and roadways. The project is expected to provide 240 parking spaces in an on-site parking garage. This report analyzes and presents the transportation impacts that the Tri-City Medical Building will have on the following intersections in the project study area:

- NC 54 & Meadowmont Lane / Friday Center Drive
- NC 54 & E. Barbee Chapel Road / Barbee Chapel Road
- Friday Center Drive & Marriott Way
- Barbee Chapel Road & Stancell Drive
- Barbee Chapel Road & Proposed Site Access

The impacts of the proposed site at the study area intersections will be evaluated during the AM, noon, and PM peak hours of an average weekday. The following study is based on background traffic for the existing year, 2014, the year following the estimated site build out year of 2016, as well as the estimated site-generated traffic produced by the medical office development.

Existing Conditions

Study Area

The site is located in eastern Chapel Hill along NC 54 (Raleigh Road) in the southwest quadrant of its intersection with Barbee Chapel Road, immediately adjacent to the Courtyard by Marriott hotel. The study area contains two signalized intersections along NC 54 along with two unsignalized intersections. NC 54 is a major arterial facility providing connectivity between downtown Chapel Hill/UNC Main Campus, Durham and the I-40 corridors. Remaining study area network roadways are either collector streets or local neighborhood access streets.

Site Traffic Generation

With the addition of new peak hour trips during the weekday AM, noon, and PM peak hours, there are potential site traffic impacts to the study area intersections. **Table ES-1** shows the site trip generation details, with generation rates and methodologies taken from the *Institute of Transportation Engineers (ITE) Trip Generation Manual, Version 9*.



Table ES-1
Weekday Vehicle Trip Generation Summary - Tri-City Medical Building

Trip Generation Statistic	Daily			AM Peak Hour			Noon Peak Hour*			PM Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
Raw Total	1,119	1,119	2,238	113	30	143	83	81	164	52	132	184
Transit/Ped/Bike Reduction	-56	-56	-112	-6	-2	-8	-4	-5	-9	-3	-7	-10
Vehicular Driveway Trips	1,063	1,063	2,126	107	28	135	79	76	155	49	125	174
Pass-By Trips	0	0	0	0	0	0	0	0	0	0	0	0
New Vehicular Trips Added to Network	1,063	1,063	2,126	107	28	135	79	76	155	49	125	174

* - No ITE Data Available, Assumed Average of AM/PM Peak Hour Data

Background Traffic

Background traffic growth for the 2017 analysis year is expected to come from two sources - ambient regional traffic growth and specific development-related traffic growth. Based on existing information, one Town-approved development project (Glen Lennox Redevelopment) near the project study area is planned to be contribute to background traffic growth by the 2017 analysis year. All remaining estimated traffic growth in the area is assumed to occur due to overall region-wide ambient growth. To account for this, an ambient area-wide traffic growth percentage of 1.4 percent per year was applied to existing traffic volumes based on information from the historic daily traffic growth patterns in the project study (NCDOT and Town of Chapel Hill daily traffic information) and consistent with recent traffic impact studies completed near the project study area.

Impact Analysis

Peak Hour Intersection Level of Service (LOS)

Study results indicate existing traffic operations at all study area intersections are acceptable during all three weekday peak hours analyzed. 2017 build-out year+1 background traffic growth causes delays to increase, but no study intersection is projected to fall to a deficient LOS. The addition of peak hour site-generated trips to the projected 2017 background traffic volumes, along with the effect of redistributing a portion of study area traffic due to the proposed Marriott Way/Site Driveway connection between Friday Center Drive and Barbee Chapel Road, do not cause study area intersections to experience deficient traffic operations in any peak hour – and in fact may improve overall traffic congestion for certain movements. An important assumption made for the 2017 With Site analyses was to restrict access to right-turns only at the proposed site driveway connection with Barbee Chapel Road. This is consistent with recommendation from previous traffic impacts studies for this location and will potentially prevent any additional “cut-through” issues between the Park-and-Ride facilities off Friday Center Drive and the NC 54 corridor east of the project study area.

A summary of the traffic operations for each intersection, related to vehicular delays (intersection average as a whole if signalized, critical movement if stop-controlled) and the corresponding Level-of-Service (LOS) is shown in **Table ES-2**.



**Table ES-2. Tri-City Medical Building
 LOS and Delay (Seconds/Vehicle) Summary**

Intersections	Peak Hour	2014 Existing		2017 No-Build		2017 Build		2017 Mitigated	
		LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
NC 54 & Meadowmont Lane / Friday Center Drive	AM	C	26.3	C	27.5	C	28.6	N/A	N/A
	NOON	C	22.2	C	22.6	C	23.5	N/A	N/A
	PM	D	41.7	D	49.3	D	44.5	N/A	N/A
NC 54 & Barbee Chapel Road / E. Barbee Chapel Road	AM	D	41.4	D	52.1	D	47.0	N/A	N/A
	NOON	B	18.2	B	18.8	B	19.0	N/A	N/A
	PM	C	27.6	C	32.9	D	36.1	N/A	N/A
Friday Center Drive & Marriott Way / UNC Park-and-Ride	AM	B*	12.7*	B*	12.9*	B*	14.3*	B*	14.3*
	NOON	B*	11.3*	B*	11.6*	B*	12.6*	B*	12.6*
	PM	C*	17.5*	C*	19.0*	D*	27.4*	D*	27.4*
Barbee Chapel Road & Stancell Drive	AM	B*	12.3*	B*	12.6*	B*	12.1*	B*	11.0*
	NOON	A*	9.2*	A*	9.2*	A*	9.2*	A*	9.1*
	PM	B*	13.1*	B*	13.5*	B*	12.8*	B*	9.3*
Barbee Chapel Road & Marriot Way / Proposed Site Driveway	AM	N/A	N/A	N/A	N/A	A*	9.5*	N/A	N/A
	NOON	N/A	N/A	N/A	N/A	A*	9.9*	N/A	N/A
	PM	N/A	N/A	N/A	N/A	D*	31.6*	N/A	N/A
Marriott Way & Proposed Site Parking Deck Access	AM	N/A	N/A	N/A	N/A	A*	9.3*	N/A	N/A
	NOON	N/A	N/A	N/A	N/A	A*	8.9*	N/A	N/A
	PM	N/A	N/A	N/A	N/A	A*	9.2*	N/A	N/A

N/A – Not Applicable or No Improvements Necessary

BOLD/ITALICS – Critical Movement or Overall Intersection Requires Mitigation Per Town TIS Guidelines

* - Worst-Case LOS/Delay for Unsignalized/Stop-Controlled Critical Movement

Access Analysis

Vehicular site access is to be accommodated by a single access driveway connecting to Barbee Chapel Road about 325 feet to the south of its signalized intersection with NC 54. Design details related to driveway throat length are shown on the concept plan and assume an approximate 275 foot driveway throat at this driveway (measured to the actual access point at the internal parking garage). Site plans also show a short one-way drop off area approximately 100 feet to the west of the driveway connection with Barbee Chapel Road. The access driveway will function as a local street, as it will directly connect to the terminus of Marriott Way to the west and provide a complete local roadway connection to Friday Center Drive.

Driveway distance along Barbee Chapel Road from the signalized intersection at NC 54 is approximately 325 feet, and is acceptable, based on recommendations of 100 foot minimum corner clearance as set forth in the 2003 *NC DOT Policy on Street and Driveway Access to North Carolina Highways* and the 100 foot minimum along collector streets specified in the 2005 *Town of Chapel Hill Design Manual*. The location of the internal garage access driveway and the connection to Barbee Chapel Road would also comply with these minimum criteria. The



spacing between the proposed driveway/access street and the nearest adjacent existing driveways along Barbee Chapel Road (Pearl Lane and an existing residential driveway are approximately 80 feet south of the proposed driveway) is less than the recommended 100 foot spacing along collector roadways found in Table 4-A-1 in the Town Design Manual.

Crash Analysis

Data from the NCDOT Traffic Safety Unit was requested for the recent five-year period for all existing major study area intersections and the Barbee Chapel Road corridor. Results indicate that the Barbee Chapel Road corridor in the project study area experiences a lower crash rate than similar facilities across North Carolina. Only three crashes were reported along the corridor in the five year period. Intersection crash summaries show that the NC 54 intersection with Meadowmont Lane has the highest number of crashes (38) of any study area intersection. Crash rates for study area intersections are, in general, lower than similar intersections in Chapel Hill.

Other Transportation-Related Analyses

Other transportation-related analyses relevant to the 2001 Town of Chapel Hill Guidelines for the preparation of Traffic Impact Studies were completed as appropriate. The following topics listed in **Table ES-3** are germane to the scope of this study.

Table ES-3. Other Transportation-Related Analyses

Analysis	Comment
Long-Range Planning Level Daily Volume-Capacity Analysis	The proposed site will add approximately 2,000 daily trips to the study area network. The NC 54 corridor currently carries over 40,000 daily trips, with a daily capacity of over 75,000 west of Barbee Chapel Road. East of Barbee Chapel Road, the daily capacity drops to approximately 36,000. Long term proposed roadway upgrades (beyond the time frame of this study) will increase daily capacity along NC 54. Daily volumes on Barbee Chapel Road are 9,500, with a daily roadway capacity of 17,500.
Turn Lane Storage Requirements	Storage bay lengths at study area intersections were analyzed using Synchro and HCS 95 th percentile (max) queue length estimates for the 2017 Build Scenario. The NC 54/Barbee Chapel Road intersection northbound approach has existing and projected queues that exceed its current storage bays in the AM Peak hour that may need additional geometric improvement in the future. Provision of a connection to the Friday Center area via Marriott Way and the proposed site driveway from Barbee Chapel Road may aid in the reduction of existing/future northbound queues by removing some of the traffic volume at NC 54 that contributes to the queuing issues.
Appropriateness of Acceleration/Deceleration Lanes	The site concept plan shows no specifics related to acceleration/deceleration lanes. Based on the recommended access laneage details at the site driveway connection with Barbee Chapel Road, a northbound left-turn deceleration lane, with a minimum of 100 feet of vehicle storage and appropriate taper is recommended to remove these vehicles from the Barbee Chapel Road traffic stream. Additionally, no southbound exclusive left-turn deceleration lane is provided at the existing intersection of Friday Center Drive and Marriott Way. For safety and operational reasons, this should be constructed with 100 feet of storage and appropriate taper. No other specific acceleration/deceleration lane issues were analyzed in the project study area.



Analysis	Comment
Pedestrian and Bicycle Analysis	Existing pedestrian and bicycle access and connectivity is excellent along the NC 54 corridor adjacent to the site, with the exception of limited points of pedestrian crossing of NC 54 immediately in the vicinity of the site. Sidewalk exists along most study area facilities, with the exception of Barbee Chapel Road. A paved off-road shared pedestrian/bicycle path exists on both sides of NC 54. Delineated bicycle lanes along Meadowmont Lane and E. Barbee Chapel Road are present in the project study area, as well.
Public Transportation Analysis	Public transportation service to the study area, and to the proposed site, is excellent with multiple bus stops and multiple local and regional bus routes on Friday Center Drive and NC 54 proximate to the site.

Mitigation Measures/Recommendations

Planned Improvements

There are no Town of Chapel Hill or North Carolina Department of Transportation improvement projects for study area roadway facilities within the analysis year time frame of 2014-2017. The NC 54 Corridor and Land Use Study (Renaissance Planning Group, 2011) featured roadway improvement recommendation along the NC 54 corridor to include several short-term and mid-term upgrades, including the following:

- Create free flow westbound travel lane on NC 54 between Friday Center Drive and Barbee Chapel Road
- Complete Marriott Way connection between Friday Center Drive and Barbee Chapel Road
- Convert Meadowmont Lane/Friday Center Drive and Barbee Chapel Road intersection with NC 54 to a superstreet corridor
- Construct additional through travel lanes on NC 54 east of Barbee Chapel Road

None of these planned improvements were considered built by the 2017 analysis year in this study. They were considered, along with any other necessary mitigation possibilities in the 2017 with site analyses, as appropriate.

The recent NCDOT STIP contains a corridor upgrade project U-5774 for NC 54 between US 15-501 in Chapel Hill and the I-40 corridor. The project is slated to be broken into several segments, one of which features the conversion of the existing NC 54 / Barbee Chapel Road intersection to a grade separated interchange. This segment, along with the others identified in the STIP, is tentatively slated for design, right-of-way acquisition, and construction starting in 2023. Due to the timeframe of these improvements being beyond the build-out horizon year for the Tri-City Medical Building, they were not specifically analyzed in this study.

Background Committed Improvements

There are no specific geometric or operational improvements to study area roadway intersections or facilities related to background private development projects that are expected to be completed between 2014 and 2017.



Applicant Committed Improvements

Based on the preliminary site concept plans and supporting development information provided, there are the following specific internal and external transportation-related improvements proposed adjacent to the Tri-City Medical Building:

- Completion of Marriott Way between Barbee Chapel Road and the current stub-out of Marriott Way on Courtyard by Marriott parcel.
- Provision of a drop-off area (single one-way lane) at the front of the proposed building
- Full access driveway connection to internal structured parking within the proposed building footprint
- Provision of continuous sidewalk along site frontage on Barbee Chapel Road and continuation of existing sidewalk along the south side of Marriott Way to Barbee Chapel Road.

Necessary Improvements

Based on traffic capacity analyses for the 2017 design year, and analyses of existing study area turning bay storage lengths and site access, the following improvements are recommended as being necessary for adequate transportation network operations (see **Figures ES-3 and ES-4**).

- 1) Due to existing queuing issues and congestion along northbound Barbee Chapel Road near the site frontage, it is recommended that the proposed access driveway/Marriott Way extension be limited to eastbound right-turns only. This will prevent congestion within the Tri-City Medical Building site and eliminate safety concerns of left-turning vehicles attempting to potentially find gaps in high volume peak hour traffic on Barbee Chapel Road. In addition, this access restriction may reduce the potential for cut-through traffic from the Friday Center Park-and-Ride Lots that might seek to access NC 54 via Marriott Way/Barbee Chapel Road.

This improvement is recommended due to the impact of the Tri-City Medical Building and the proposed connection with Marriott Way.

- 2) Northbound Barbee Chapel Road in the vicinity of the proposed site driveway should be restriped to allow left-turns into the site and as a connection to the Friday Center Drive area. This allowance should reduce traffic volumes (especially in the AM peak hour) at the Barbee Chapel Road / NC 54 intersection and improve operations/reduce queues for the northbound approach. A left-turn bay of at least 100 feet of storage with appropriate taper should be created along Barbee Chapel Road, along with a concrete median island from the proposed driveway access to just north of the Pearl Lane intersection (see **Figure ES-4**). This will aid in preventing left-turns eastbound from the driveway, while still allowing full access for residents in the Pearl Lane area.

This improvement is recommended due to the impact of the Tri-City Medical Building and the proposed connection with Marriott Way.

- 3) To improve safety in the area along Barbee Chapel Road immediately south of the NC 54 intersection, a concrete median should be installed from its existing location to just north of the existing service station driveway (see **Figure ES-4**). This will restrict Stancell Drive to become a RIRO intersection. Full access will still be provided for the service station, and those current left-turn movements from southbound Barbee Chapel



Road to Stancell Drive will need to access Stancell Drive from the Little John Road intersection located to the east of Barbee Chapel Road – though field observation noted that a portion of these actually immediately turn into the service station.

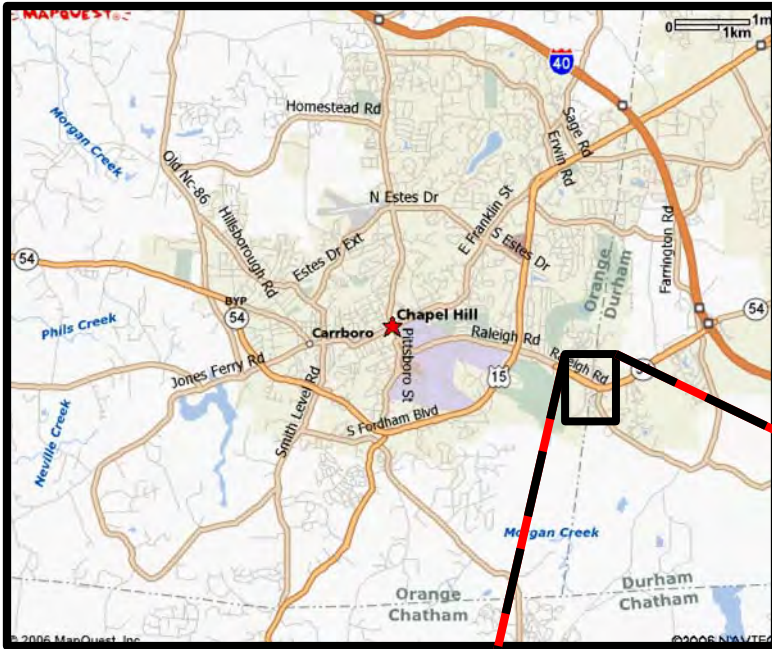
The proposed concrete median and access restrictions for Stancell Drive are recommended whether or not the Tri-City Medical Building project is constructed.

- 4) Retime the NC 54 intersections with Barbee Chapel Road and Meadowmont Lane/Friday Center Drive to optimize overall capacity after the creation of the Marriott Way/proposed site driveway connection between Friday Center Drive and Barbee Chapel Road. Study results between the No-Build and Build scenarios indicate that, even with existing signal timings held constant, there is little negative effect on traffic operations at these two intersections, even with additional traffic related to the Tri-City Medical Building. However, both intersections experience appreciable congestion and queues during weekday peak hours. Traffic volumes at several approaches at these two intersections may be reduced due to local traffic shifting onto the Marriott Way extension, which is an opportunity to reoptimize signal operations and reduce critical movement delays and queues (particularly for northbound Barbee Chapel Road). Retiming these two intersections may improve operations at the for 2017 build-out+1 year conditions beyond what has been analyzed for this study.





The proposed signal timing improvements are recommended due to site traffic impacts from the Tri-City Medical Building and associated anticipated changes in traffic patterns due to the Marriott Way/proposed site driveway connection with Barbee Chapel Road and Friday Center Drive.

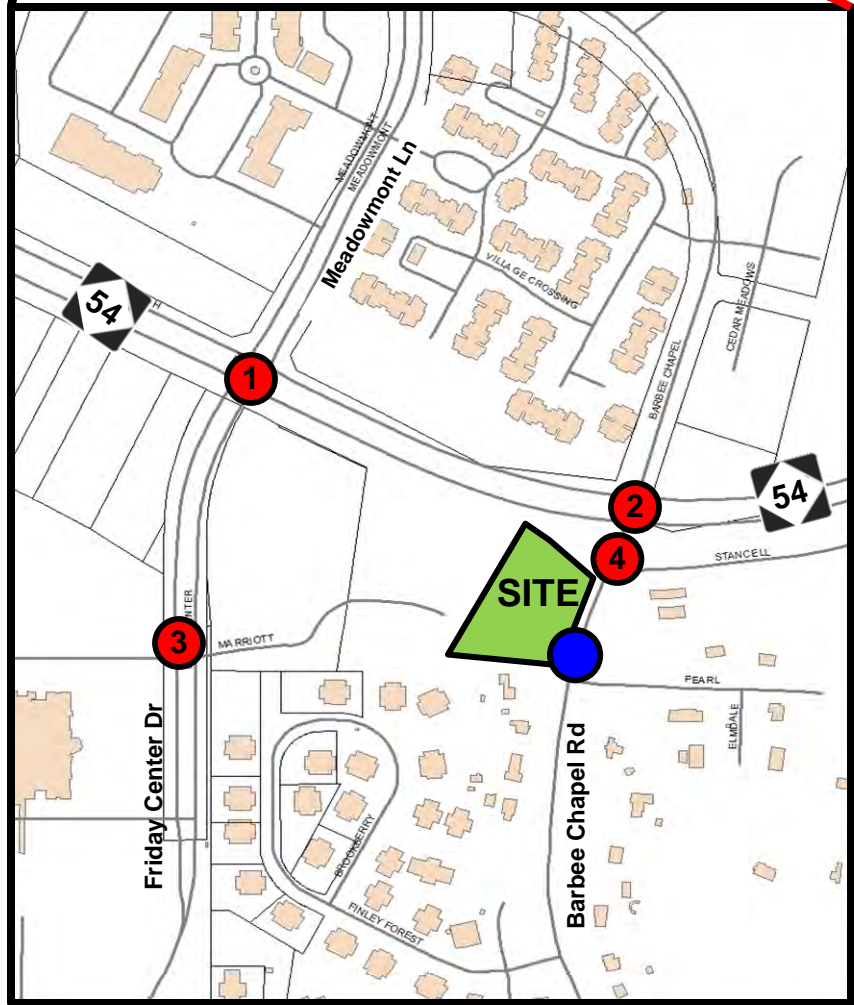
- 5) A safety-related recommendation for the existing intersection of Friday Center Drive and Marriott Way/UNC Park-and-Ride Driveway is to construct a southbound left-turn lane with 125 feet of storage and appropriate taper in the existing median on Friday Center Drive. This would remove left-turning traffic from the southbound traffic stream on Friday Center Drive.

This improvement is recommended whether or not the Tri-City Medical Building is constructed.



LEGEND

-  = Existing Building Footprint
-  = Existing Study Area Intersection
-  = Proposed Site Driveway
-  = Proposed Site Parcel



**Tri-City Medical Building
Traffic Impact Study**

DATE: January 2015

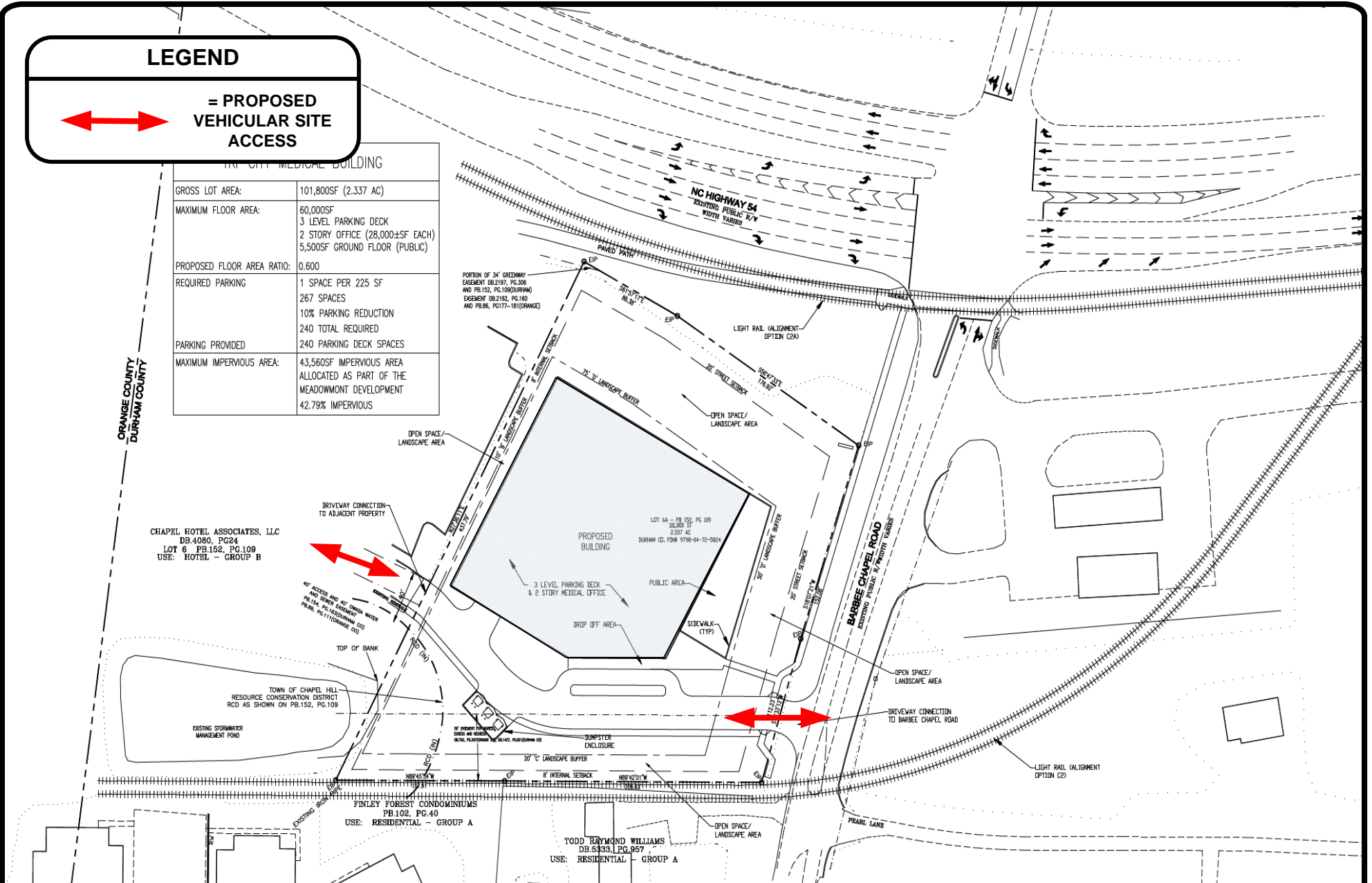
PROJECT STUDY AREA MAP

FIGURE ES-1

LEGEND

 = PROPOSED VEHICULAR SITE ACCESS

TRI-CITY MEDICAL BUILDING	
GROSS LOT AREA:	101,800SF (2.337 AC)
MAXIMUM FLOOR AREA:	60,000SF 3 LEVEL PARKING DECK 2 STORY OFFICE (28,000±SF EACH) 5,500SF GROUND FLOOR (PUBLIC)
PROPOSED FLOOR AREA RATIO:	0.600
REQUIRED PARKING	1 SPACE PER 225 SF 267 SPACES 10% PARKING REDUCTION 240 TOTAL REQUIRED
PARKING PROVIDED	240 PARKING DECK SPACES
MAXIMUM IMPERVIOUS AREA:	43,560SF IMPERVIOUS AREA ALLOCATED AS PART OF THE MEADOWMONT DEVELOPMENT 42.79% IMPERVIOUS



NOT TO SCALE

Tri-City Medical Building Traffic Impact Study

SITE CONCEPT PLAN

DATE: January 2015

FIGURE ES-2



NOT TO SCALE

LEGEND



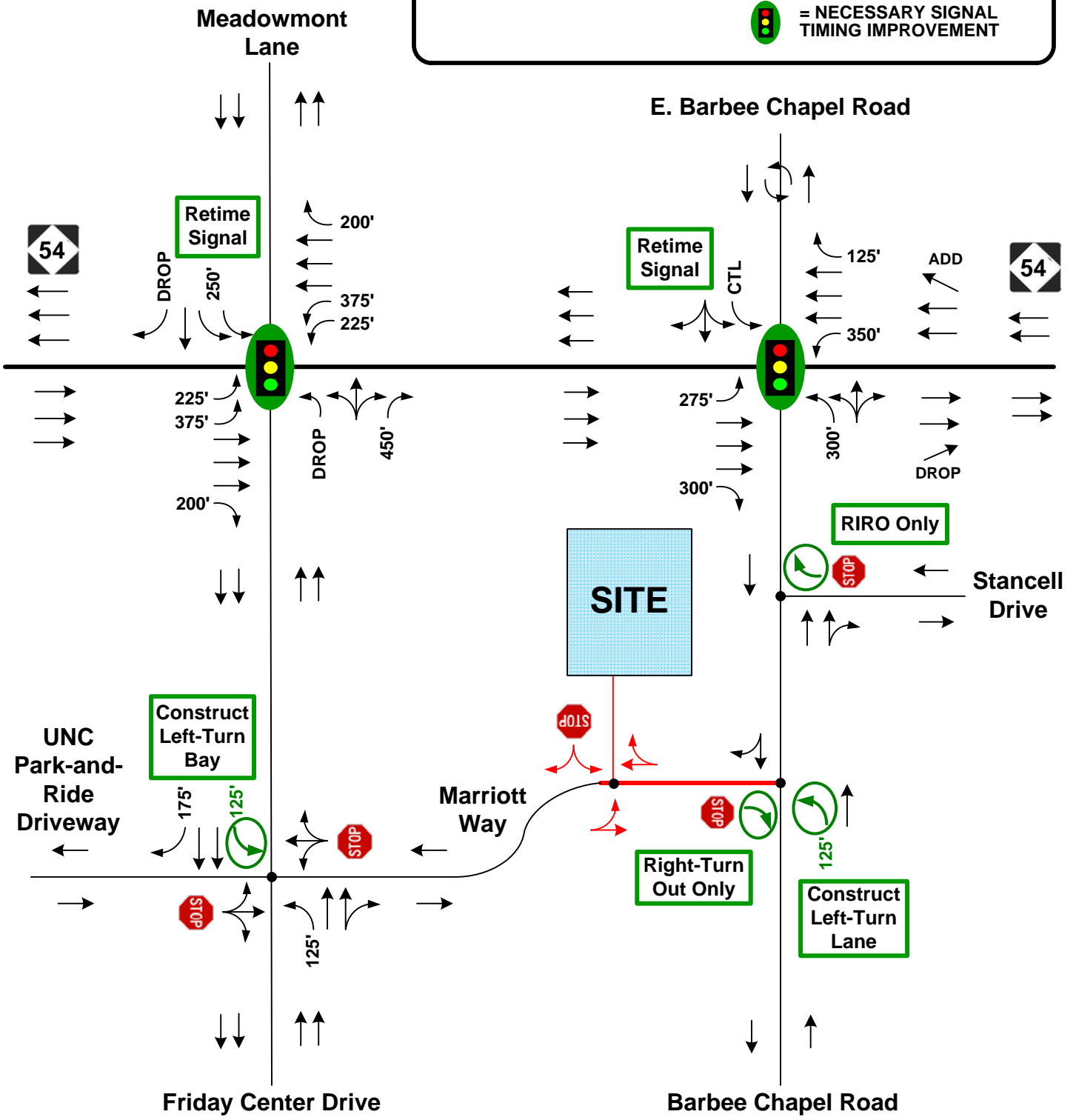
= APPLICANT COMMITTED IMPROVEMENT



= NECESSARY GEOMETRIC IMPROVEMENT



= NECESSARY SIGNAL TIMING IMPROVEMENT



Tri-City Medical Building Traffic Impact Study

DATE: January 2015

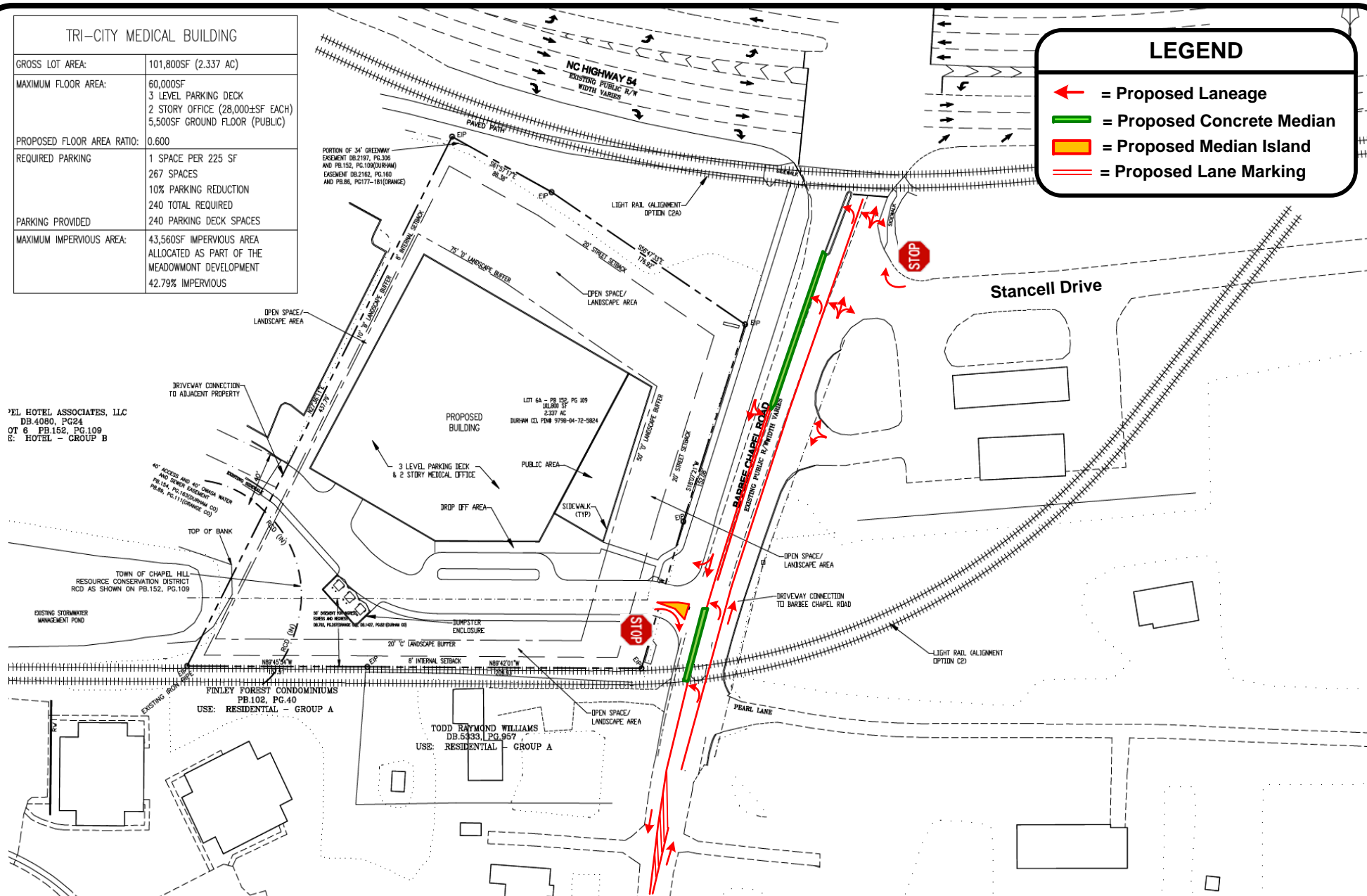
COMMITTED AND RECOMMENDED IMPROVEMENTS

FIGURE ES-3

TRI-CITY MEDICAL BUILDING	
GROSS LOT AREA:	101,800SF (2.337 AC)
MAXIMUM FLOOR AREA:	60,000SF 3 LEVEL PARKING DECK 2 STORY OFFICE (28,000±SF EACH) 5,500SF GROUND FLOOR (PUBLIC)
PROPOSED FLOOR AREA RATIO:	0.600
REQUIRED PARKING	1 SPACE PER 225 SF 267 SPACES 10% PARKING REDUCTION 240 TOTAL REQUIRED 240 PARKING DECK SPACES
PARKING PROVIDED	240
MAXIMUM IMPERVIOUS AREA:	43,560SF IMPERVIOUS AREA ALLOCATED AS PART OF THE MEADOWMONT DEVELOPMENT 42.79% IMPERVIOUS

LEGEND

- ← = Proposed Laneage
- = Proposed Concrete Median
- = Proposed Median Island
- = = Proposed Lane Marking



NOT
TO
SCALE

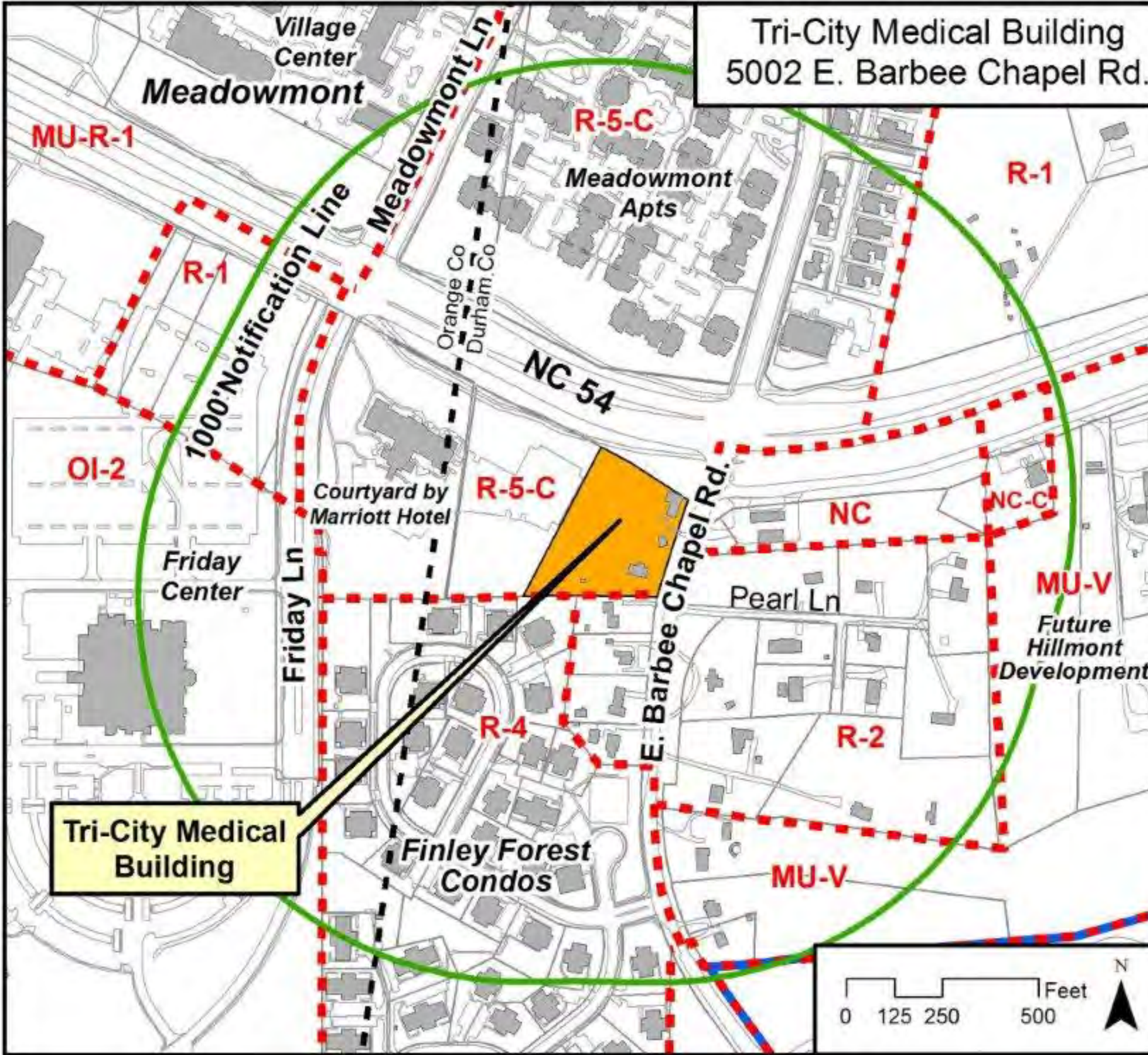
Tri-City Medical Building
Traffic Impact Study

RECOMMENDED IMPROVEMENTS DETAIL

DATE: January 2015

FIGURE ES-4

Tri-City Medical Building
5002 E. Barbee Chapel Rd.



Tri-City Medical Building

