



# TOWN OF CHAPEL HILL NORTH CAROLINA

## MEMORANDUM

**TO:** Town of Chapel Hill Commissions and Advisory Boards

**FROM:** Gene Poveromo, Development Manager  
Kay Pearlstein, Senior Planner

**SUBJECT:** Application for Special Use Permit, [A.C. Hotel by Marriott, 210 E. Rosemary Street](#)

### Recommended Action

- That the Community Design Commission, Transportation and Connectivity Board, and Environmental Stewardship Advisory Board forward a recommendation to the Planning Commission and the Council; and that the Planning Commission also forwards a recommendation to the Town Council.

### Explanation of Recommendation

- For this proposed development, the Council's Advisory Board Policy Manual requires that the Community Design Commission, Transportation and Connectivity Board, and Environmental Stewardship Advisory Board forward a recommendation to the Planning Commission for the Commission's consideration. The Land Use Management Ordinance requires that the Planning Commission forward a recommendation to the Council.

### Project Description

- This Special Use Permit Application proposes a 4-story hotel with 123 rooms and 111 parking spaces in an underground parking garage. Located on the northeast corner of Church Street and W. Rosemary Street, the 0.79 acre site is zoned Town Center-2 (TC-2) and is in the Northside Neighborhood Conservation District.

### Key Considerations

- *Keeping hotel traffic out of the Northside Neighborhood:* The Council expressed a concern with vehicles existing the hotel and traveling through the Northside neighborhood in order to avoid the light at Church St and W. Rosemary
- *Refuse Collection:* The staff is currently discussing with the applicant a design solution that would keep refuse collection vehicles from standing in W. Rosemary St in order to collect refuse/recycling
- *Concept Plan Review:* During the Community Design Commission and the Council review of the Concept Plan, some of the following topics were discussed: location of utilities; screening the neighborhood; use of green roofs; activating the streetscape; scale of the building; stepping back the elevation on the north side; minimizing noise from the pool.

## Background

- *February 23, 2005, Northside Neighborhood Conservation District:* The Council adopted the [Northside Neighborhood Conservation District](#)<sup>1</sup> that included Design Standards, Design Guidelines, and an area map. The hotel site is subject to the design standards and guidelines of the district.
- *September 16, 2009*<sup>2</sup>, *UNC Wesley House, Mixed Use Development:* On [September 16, 2009](#)<sup>3</sup> the Council reviewed a concept plan proposing 76,770 sq. ft., approximately 5,000 sq. ft. retail, housing for 144 students, parking for 35 cars and 66 bicycles .
- On [January 9, 2012](#)<sup>4</sup> The Council amended the *Northside Neighborhood Conservation District Regulations*. Goals includes: 1. Affordable Housing; 2. Cultural and Historic Preservation; 3. Enforcement; 4. Education and Outreach; 5. Parking; and 6. Zoning
- *August 20, 2014:*<sup>5</sup> The Community Design Commission reviewed a concept plan.
- *October 20, 2014:*<sup>6</sup> The Council reviewed a concept plan.
- *Downtown Imagined:* An on-going plan is being developed for Downtown called [Downtown Imagined](#)<sup>7</sup>
- The 2020 [Land Use Plan](#)<sup>8</sup> identifies this parcel as part of Town/Village Center and is part of Focus Area 1 of the 2020 Comprehensive Plan emphasizing infill of underutilized sites.

## Fiscal Note

- According to the applicant Fiscal Impact Analysis, the principal net general fund tax revenues to the Town of Chapel Hill resulting from the completion of this project total approximately \$192,000 per year.

## Council Goals

- Create A Place for Everyone
- Develop Good Places, New Spaces

## Attachments

- Staff Report
- Resolution A Special Use Permit Modification of Approval
- Resolution B Special Use Permit Modification of Denial
- Applicant Materials
- Applicant's response to Concept Plan Comments
- Traffic Impact Executive Summary
- Applicant's Fiscal Impact Analysis
- Area Map

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<sup>1</sup> <http://www.townofchapelhill.org/Modules/ShowDocument.aspx?documentid=728>

<sup>2</sup> [http://chapelhill.granicus.com/MediaPlayer.php?view\\_id=7&clip\\_id=607&meta\\_id=37229](http://chapelhill.granicus.com/MediaPlayer.php?view_id=7&clip_id=607&meta_id=37229)

<sup>3</sup> <http://townhall.townofchapelhill.org/agendas/2009/11/16/1c/>

<sup>4</sup> <http://www.townofchapelhill.org/index.aspx?page=1742>

<sup>5</sup> <http://www.townofchapelhill.org/index.aspx?page=15&recordid=6820>

<sup>6</sup> <http://chapelhillpublic.novusagenda.com/Bluesheet.aspx?itemid=2915&meetingid=294>

<sup>7</sup> <http://www.townofchapelhill.org/index.aspx?page=2216>

<sup>8</sup> <http://www.townofchapelhill.org/Modules/ShowDocument.aspx?documentid=1215>

## STAFF REPORT

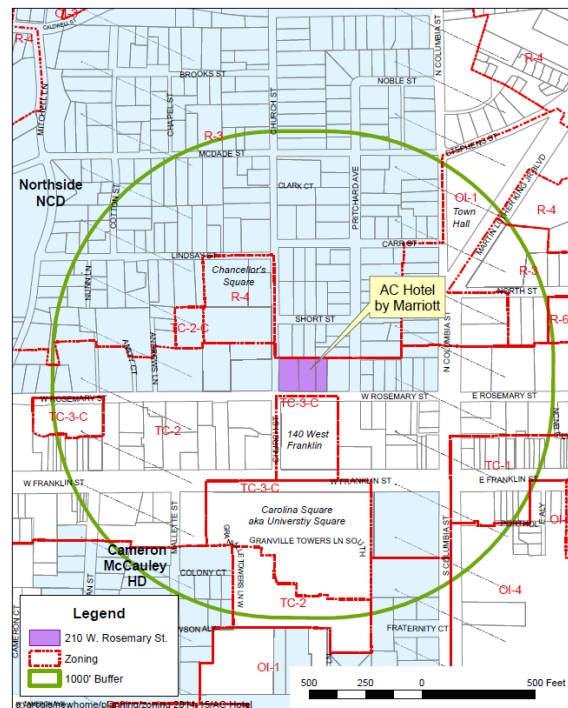
**TO:** Environmental Stewardship Advisory Board (June 9, 2015)  
Transportation and Connectivity Advisory Board (June 23, 2015)  
Community Design Commission (June 23, 2015)  
Planning Commission (July 21, 2015)

**SUBJECT:** Application for Special Use Permit – [AC Hotel by Marriott, 210 W. Rosemary Street](#)<sup>1</sup> (Project #14-076)

**DATE:** June 9, 2015

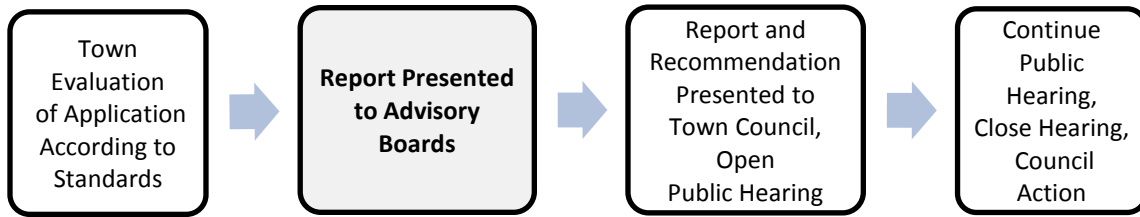
## INTRODUCTION

Attached for your consideration is a Special Use Permit Application, submitted by OTO Development, LLC, which proposes to develop a .79-acre site, comprised of 3 lots, located at 210 W. Rosemary Street. The proposed development, AC Hotel by Marriott, is on the northeast corner of the intersection of W. Rosemary Street and Church Street. The development is for a 4-story hotel and underground parking with 68,300 s.f. of floor area. The location is also in the Northside Neighborhood Conservation District (NCD) and zoned Town Center-2 (TC-2).



## PROCESS

<http://www.townofchapelhill.org/town-hall/departments-services/planning-and-sustainability/development/development-activity-report/rosemary-street-hotel><sup>1</sup>



## BACKGROUND

*February 23, 2005, Northside Neighborhood Conservation District:* On February 23, 2005, the Council adopted the [Northside Neighborhood Conservation District](#)<sup>2</sup> that included Design Standards, Design Guidelines, and an area map. The hotel site is within the boundaries of the Northside Neighborhood Conservation District (see attached map) and subject to the design standards and guidelines of the district.

*September 16, 2009, Wesley House, Mixed Use Development submitted by UNC Wesley House Foundation:* On [September 16, 2009](#)<sup>3</sup> the Council reviewed a concept plan for Wesley House, Mixed Use Development at 212, 214, and 220 W. Rosemary Street, the same three parcels proposed for the Rosemary Street Hotel. The proposed Wesley House Mixed Use development included 76,770 square feet of floor area and approximately 5,000 square feet of retail area including housing for 144 students. Parking for 35 cars and 66 bicycles was also proposed. A copy of the September 16, 2009 Council minutes and video is available on the [Town's web site](#)<sup>4</sup>.

*Fall, 2012, Northside and Pine Knolls Community Plan:* On [January 9, 2012](#)<sup>5</sup> the Town Council amended the Northside Neighborhood Conservation District Regulations. The amendment included the following goals: 1. Affordable Housing; 2. Cultural and Historic Preservation; 3. Enforcement; 4. Education and Outreach; 5. Parking; and 6. Zoning

[August 20, 2014:](#)<sup>6</sup> The Community Design Commission reviewed a concept plan. A summary of comments from the Community Design Commission review are listed below:

- Scale of the rear building adjacent to Northside needs more attention and better solution.
- Hotel users on front balconies could be a problem during hotel and adjacent restaurant events.
- Front entrance, Rosemary Street elevation, circulation on the site, and Rosemary/Church Street corner need better design.

<sup>2</sup> <http://www.townofchapelhill.org/Modules/ShowDocument.aspx?documentid=728>

<sup>3</sup> <http://townhall.townofchapelhill.org/agendas/2009/11/16/1c/>

<sup>4</sup> [http://chapelhill.granicus.com/MediaPlayer.php?view\\_id=7&clip\\_id=607&meta\\_id=37229](http://chapelhill.granicus.com/MediaPlayer.php?view_id=7&clip_id=607&meta_id=37229)

<sup>5</sup> <http://www.townofchapelhill.org/index.aspx?page=1742>

<sup>6</sup> <http://www.townofchapelhill.org/index.aspx?page=15&recordid=6820>



- Supports “green roofs;” plant additional street trees.
- Activate street frontages.
- Utility locations are important.
- Reuse/recycle existing buildings proposed for removal.

*Downtown Imagined*: An on-going plan is being developed for Downtown called [Downtown Imagined](#)<sup>7</sup> that includes the Rosemary Street Hotel site (see attached map).

The 2020 [Land Use Plan](#)<sup>8</sup> identifies this parcel as part of Town/Village Center and is part of Focus Area 1 of the 2020 Comprehensive Plan (see attached map) emphasizing infill of underutilized sites.

### **KEY CONSIDERATIONS**

During our staff review we have identified the following key considerations:

- *Keeping hotel traffic out of the Northside Neighborhood*: Keeping vehicles leaving the hotel from traveling through the Northside neighborhood in order to avoid the light at Church St and W. Rosemary
- *Refuse Collection*: Design a solution that would not require refuse collection vehicles from standing in W. Rosemary St in order to collect refuse/recycling.
- *Concept Plan Review*: The following topics were discussed: location of utilities; screening the neighborhood; use of green roofs; activating the streetscape; scale of the building; stepping back the elevation on the eastside; minimizing noise from the pool. A copy of the applicant’s response to these topics is attached.

For additional information on the Key Considerations, please refer to the following staff analysis.

### **EXISTING CONDITIONS/DEVELOPMENT PROPOSAL SUMMARY**

The Special Use Permit application is proposing the following changes with details indicated in the table below:

<b>General Site Description and Location</b>	
Address	210 W. Rosemary Street
Property Description and Location	An assemblage of three parcels combines for a total of 34,685 square feet (.79 acres) and located on the northeast corner of W. Rosemary Street and Church Street.

<sup>7</sup> <http://www.townofchapelhill.org/index.aspx?page=2216>

<sup>8</sup> <http://www.townofchapelhill.org/Modules/ShowDocument.aspx?documentid=1215>

Orange County Parcel Identifier Numbers	9788-27-6387; 9788-27-6326; and 9788-27-5353
<b>Existing Site Conditions/Proposed Development Description</b>	
Existing use and proposed Special Use Permit	Existing – Three lots at 212, 214, and 220 W. Rosemary Street occupied by Los Patrillos Restaurant, a 2-story office building, and a 1-story office building; all will be demolished. Existing rock walls are to be removed and stone recycled. Proposed – Hotel with 123 rooms and 111 parking spaces.
Floor area	A maximum of 68,330 s.f. is allowed under the Town Center-2 (TC-2) zoning district. The proposal includes a total of 68,300 s.f. of floor area.
Affordable Housing Units	Not applicable
Height	Primary height is 40 feet (30 feet if adjacent to residential zoning) and secondary height is 50 feet in the Northside NCD for TC-2 zoning district. Proposed Primary height is a maximum of 30 feet adjacent to residential on the north, and 40 feet on the other sides. The Secondary Height is proposed as 50 feet. The building steps back from 50 feet to 30 ft. on the north (adjacent to Northside NCD) at a slope of 1:1.7. The Ordinance requires no less than a 1:1 slope.
Vegetation	Twenty three trees; all to be removed. Eight new street trees are proposed on W. Rosemary Street and Church Street.
Vehicular Parking	Minimum – no minimum in TC districts; Maximum – 111 spaces Provided – 111 parking spaces
Bicycle Parking	Required - 8 spaces; Provided – 12 spaces (including 8 Class 1)
Vehicular and Pedestrian Access	Existing – 2 driveway curb cuts to W. Rosemary Street and one curb cut on Church Street. Proposed – vehicle ingress/egress to Church Street for access to underground parking and drop-off/registration under port cochere.
Land Disturbance	34,400 s.f.
Impervious Surface	23,874 s. f. (73.4%); 31,551 s.f. (97%)
Overlay Zone	Northside Neighborhood Conservation District
Resource Conservation District overlay zone	None
Jordan Riparian Buffer overlay zone	None
Steep Slopes	None
Zoning	Town Center-2 (TC-2)
<b>Requested Modification to Regulations</b>	
Modification to Regulations	None.
<b>Surroundings</b>	
General Development Pattern	Commercial/office/multi-family on W. Rosemary Street and residential on the north.
North	Residential-3; Northside NCD, single family homes
East	Town Center-2 (TC-2) and Northside NCD; restaurant (La

	Residence)
South	W. Rosemary Street; Town Center-3 (TC-3) and 140 West condominiums
West	Church Street, Town Center-2 (TC-2) and restaurant (Pantana Bobs )

### **STAFF ANALYSIS OF APPLICATION**

Town staff has reviewed this application for compliance with the themes from the [2020 Comprehensive Plan](#)<sup>9</sup>, the standards of the [Land Use Management Ordinance](#)<sup>10</sup>, and the [Design Manual](#)<sup>11</sup>, and offers the following evaluation:

**Comprehensive Plan:** The following are themes from the 2020 Comprehensive Plan, adopted June 25, 2012:

Conforms	No.	2020 Comprehensive Plan Themes
√	1	A Place for Everybody
√	2	Community Prosperity and Engagement
√	3	Getting Around
√	4	Good Places, New Spaces
√	5	Nurturing Our Community
	6	Town and Gown Collaboration

We believe that the AC Hotel by Marriott proposal complies with five of the six themes of the 2020 Comprehensive Plan. For information on how this proposed development addresses these themes and goals of the Comprehensive Plan, please refer to the attached applicant’s Statement of Justification.

**Land Use Plan:** The [2020 Land Use Plan](#)<sup>12</sup>, a component of the 2020 Comprehensive Plan, designates this site as Town/Village Center and also designated as Focus Area 1 Downtown. The draft Downtown Plan identifies this site as a redevelopment site that respects Northside with appropriate step-backs and setbacks and encourages infill.

**Zoning:** The property is zoned Town Center – 2 (TC-2) with an overlay district, Northside Neighborhood Conservation District.

***Intensity – Floor Area, Setbacks, and Building Heights:***

**Floor Area:** Maximum floor area is 68,330 sq. ft.

<sup>9</sup> <http://www.ci.chapel-hill.nc.us/town-hall/departments-services/design-chapel-hill/chapel-hill-2020>

<sup>10</sup> <http://www.ci.chapel-hill.nc.us/town-hall/departments-services/planning-and-sustainability/resources/planning-library-plans-regulations-and-guidelines/chapel-hill-land-use-management-ordinance>

<sup>11</sup> <http://www.ci.chapel-hill.nc.us/home/showdocument?id=2645>

<sup>12</sup> <http://www.townofchapelhill.org/home/showdocument?id=1215>

*Setbacks:* Zero setbacks from south (W. Rosemary Street), east (TC-2, La Residence restaurant), and west (Church Street). The northern setback is established as eleven feet (R-3, Northside) as a Transitional Control to a residential zoning district.

*Building Height:* In the Northside Neighborhood District primary building height is 30 ft.; secondary building height is 50 ft.

***Transitional Control and Intensity Modifications:*** Section 3.8.4(b)(3) requires that minimum solar setbacks adjacent to residentially zoned land be equal to the solar setback applicable in the adjacent residential district and further that the primary height adjacent to residentially zoned land not exceed thirty-five (35) feet. The hotel is shown setback twelve feet from residential properties on the north (solar setback). The solar setback in the Northside Residential-3 zoning district is 11 feet. The proposal complies with the transitional control for setback. The primary height proposed for the hotel’s north-facing façade adjacent to the residential properties is shown with a height of 30 feet. The Northside regulations require that Town Center-2 zoned properties adjacent to residential properties have a primary height no taller than 35 feet. The proposal complies with the transitional control for primary height.

**Comparison of Concept Plan and Special Use Permit Proposals**

	<b>Concept Plan</b>	<b>Special Use Permit Application</b>
Floor Area Proposed	66,925 s. f.	68,300 s, f.
Vehicular Access (Church St. only)	2 curb cuts for ingress and egress	2 curb cuts for ingress and egress
Impervious Surface	22,874 s.f.	31,551 s.f.
Number of Stories	4 stories	4 stories

**PROPOSED MODIFICATION TO REGULATIONS**

The applicant is not seeking any modifications to the regulations.

**Transportation**

***Traffic Impact and Retiming Payment:*** A Traffic Impact Study was prepared for the Town by HNTB PC (Executive Summary attached). The study assumed that the project would be completed by 2018. The study area included three major signalized intersections that have been identified as needing retiming. They are: Church Street/W. Rosemary Street, Church Street/W. Franklin Street, and Columbia Street/W. Rosemary Street. The developer shall provide a total payment of \$3,000 for retiming at these intersections. We have added a stipulation in Resolution A to this effect.

An additional recommendation of the TIA was to lengthen the W. Rosemary Street left turn lane at the Columbia Street intersection with restriping of the turning lane. We understand the Town will be undertaking the restriping of the left turn lane on W. Rosemary St. at the Columbia Street intersection this summer.

## **Access and Circulation**

***Vehicular Access:*** The proposal includes two driveways on Church Street, one for 1-way ingress and one for egress and ingress. The first driveway, one-way travel, allows for registration, drop-offs, and deliveries under a port cochere in front of the hotel entrance. The second driveway is for access and egress to the underground garage, solid waste/recycling service, and for those leaving the registration area. No access is proposed on W. Rosemary Street. We recommend that signs be erected indicating traffic movements on each of the driveways and have included way-finding signage in Resolution A.

The driveways from Church Street shall be constructed with a ramp-type concrete driveway entrance from Church Street. The Church Street sidewalk shall cross the hotel driveways with ADA detectable warning pavers. We have added this to Resolution A.

***Vehicular Parking:*** In the Town Center zoning districts, there are no minimum parking requirements for a hotel. Maximum parking for a hotel in Town Center districts is based on the number of rooms. In this case, 123 rooms are proposed. The Land Use Management Ordinance allows .9 spaces per lodging unit or 111 spaces. The proposal includes 111 parking spaces in a 2-level underground garage. We have included that the applicant provide 111 parking spaces in Resolution A.

***Transit:*** An existing bus stop on W. Rosemary Street exists between Pritchard Avenue and Church Street. There is not bus service on Church Street. The Rosemary Imagined area plan was enlarged and included as part of the Downtown Imagined plan. The plan encourages new development and infill development along W. Rosemary Street and W. Franklin Streets. Because of future increased development we have included a stipulation in Resolution A for the developer to provide a \$5,000 payment for transit improvements in the area.

***Bicycle Parking:*** A minimum of nine bicycle parking spaces are required for a hotel, 1 space per 15 lodging unit or 9 spaces in this case. The applicant is proposing a total of 12 spaces, eight in the garage and four in racks within the W. Rosemary and Church Street rights-of-way meeting the Class I and Class II requirements. We have included this in Resolution A.

***Bicycle Plan and W. Rosemary St:*** The applicant is proposing to incorporate a 7-0 wide accessible ramp from the W. Rosemary/Church Street corner as part of the streetscape improvements. The draft Bicycle Plan shows options for the block of W. Rosemary Street between Pritchard Avenue and Church Street. Because the travel lane includes 2 (11-ft) lanes of traffic, one in each direction, and an (11-ft) left turn lane onto Church Street, there is not enough width to include a 4-foot bike lane. Additional right-of-way dedication is also not feasible with 140 West on the south. The hotel on the north is moved as far back on the lot to meet the northern setback and for dedication of additional right-of-way for accessible ramps, amenity strip, and 8-foot sidewalks. The short term cross section outlined in the Bike Plan allows for sharrows on the travel lanes on W. Rosemary Street between Pritchard Avenue and Church Street. W. Rosemary Street does not have the width in this location for both handicapped accessible ramps and a bike lane. In this case, because the Bike Plan allows sharrows, we are recommending that the streetscape include accessible ramps and allow sharrows on the W.

Rosemary Street. We have included a stipulation in Resolution A for sharrows on W. Rosemary Street in front of the hotel site.

***Pedestrian Access and Streetscape Improvements:*** The applicant is proposing streetscape improvements to the W. Rosemary Street and Church Street frontages. Along W. Rosemary Street, the applicant is proposing to dedicate approximately ten feet of additional right-of-way that will include a 4-foot wide brick amenity strip adjacent to existing curb and gutter that will include four tree grates with street trees, three trash receptacles, three 15-foot street lights, and a bicycle rack. An 8-foot wide sidewalk between the brick pavement area and face of the building is proposed. A public access easement is proposed from the curb to the face of the building. We have included the dedication and streetscape improvements in Resolution A.

The applicant is proposing an 8-foot public access easement along the Church Street frontage of the property. Within the easement the applicant proposes a 4-foot brick amenity strip, 8-foot sidewalk, four tree grates with street trees, a trash receptacle, one 15-foot street light, bike rack, duct bank, and two driveway curb cuts with at-grade sidewalks across the driveways. All bike racks shall be located at light poles to avoid bikes chained to poles. A new fire hydrant is proposed that will allow the building to have a sprinkler system. We have added these improvements in Resolution A. No changes are proposed to existing curb and gutter other than the driveway curb cuts.

A new duct bank is proposed within the W. Rosemary Street right-of-way and the public pedestrian easement on Church Street. The duct bank will be located under the sidewalk and above the underground parking garage to allow public utilities to be placed underground. We have included the new duct bank in Resolution A.

**Landscape and Architecture**

***Landscape Bufferyards:*** The Land Use Management Ordinance requires the following landscape bufferyards:

<b>Location</b>	<b>Required Buffer</b>	<b>Proposed Buffer</b>
West – Church Street	No buffer	No buffer
North – Residential-3, Northside Neighborhood Conservation District and single-family home	20-foot Type ‘C’	Alternative Buffer with a mix of tall trees, under-story trees, shrubs, and fence within a 10-foot wide buffer.
East – La Residence Restaurant	No buffer	No buffer
South – W. Franklin Street	No buffer	No buffer

***Northern Landscape Buffer:*** The applicant is requesting an alternative buffer for less planting per 100 linear foot and less width than what is required by the ordinance. The ordinance requires a 20-foot type “C” buffer along the northern property line with the following plant materials per 100 feet: 4 large trees, 8 understory trees, and 30 shrubs. The applicant is proposing: a 6-foot solid wood fence at the property line, plus 2 large trees, 7 understory trees, and 20 shrubs per 100 feet of property line. The ordinance allows the Community Design Commission to approve an

Alternative Buffer if they determine the alternative buffer affords an equivalent amount of screening as the ordinance requires.

**Street Trees:** A total of eight shade trees are proposed to be planted in tree grates along the hotel frontages. In order to avoid street lights obscured by tree foliage, the applicant has located the street lights and trees to avoid this conflict. The tree pit drainage pipes will be connected to the storm drain system via a sand filter and sump. We have included a stipulation in Resolution A for eight street tree plantings to be approved by the Town Manager.

**Green Roof Plantings:** The roof tops of the 2<sup>nd</sup>, 3<sup>rd</sup>, and 4<sup>th</sup> floor are proposed to contain plant trays or modules of plants to provide stormwater management. Green screens are also proposed to provide vertical plantings on several floors. We have added a stipulation in Resolution A for the developer to provide a planting plan and installation details for the green roof and screens.

**Fire Pit:** A fire pit with a screen wall and outdoor seating is proposed under the port cochere near the building entrance. A screen wall is proposed to separate the fire pit area from the public sidewalk. Registration windows allow the area to be monitored from the inside of the hotel.

**Building Architecture:** The building is open in the middle to allow for a ground level swimming pool and to allow light into the rooms. A breakfast bar is open in the morning for the hotel guests and opens to W. Rosemary Street. A separate bar/lounge area is also proposed in the lobby. The building steps back to the Northside Neighborhood Conservation District; at the rear of the building, the height is 2 stories high, approximately 30 feet. The height complies with the Transitional Control regulations.

**Building Elevations, Lighting, and Buffer:** Prior to issuance of a Zoning Compliance Permit, the applicant shall obtain Community Design Commission approval of building elevations, northern alternative buffer, and outdoor lighting including special consideration of the location and screening of HVAC. We have included a stipulation in Resolution A to this effect.

### **Environment/Stormwater Management**

**Stormwater Management and Green Roof:** To address the stormwater requirements of the Land Use Management Ordinance, the applicant is proposing a 10,000 sq. ft. green roof system. The system is designed in accordance with the NCDENR Stormwater BMP Manual. The third and fourth stories are stepped back and expose the roof. "Green screens" or vertical growing plants, are proposed against the north-facing building wall of the third and fourth floors and vegetated trays or modules are proposed on the flat roof areas where maintenance paths will not be necessary. Additionally, roof drains are proposed to be directed to the storm drains on Church Street. Underground garage elevators are prepared to include sump pumps if needed. Any stormwater runoff in the underground garage would be collected and treated before being discharged into the sewer drains. A total of 31,532 square feet of impervious surface is proposed (100% of the site). A total of 34,412 square feet of land disturbance is proposed. We have included stipulations in Resolution A for Stormwater Management and a Green Roof.

**Energy Management Plan:** The applicant has submitted an Energy Management Plan. The Energy Management Plan details the applicant's proposal for a goal of meeting a 20 percent



more efficient than ASHRAE as referenced in the 2012 North Carolina Energy Conservation Code standard. Resolution A includes a stipulation requiring an Energy Management Plan.

1. **Refuse/Recycling Collection:** The applicant is proposing private solid waste and recycling collection. The use as proposed would not be eligible for Town solid waste collection. Solid Waste and recycling would operate in the following manner: service trucks would drive directly into the parking garage with empty, 96 gallon rolling carts to a storage area on the upper level of the underground parking garage. The storage area is proposed to be screened and approximately 324 sq. ft. in size. Empty carts would be unloaded into the storage area and full carts loaded onto a truck. Small trucks are proposed to collect the carts from within the garage for maneuverability and for height clearance. The trucks are 12 feet tall and 13 feet to 16 feet long. Trucks will then turn around and exit the site.

We have included a stipulation for the developer to designate a convenient parking space reserved for a solid waste collection vehicle and other service vehicles. If a designated space is not provided, service vehicles may be unable to park and could block entrance to most of the first parking level when positioned across from the refuse enclosure, creating a temporary one-way flow for cars driving between the parked service truck and the enclosure. This unsafe traffic maneuver could result in delays during cart replacement resulting in vehicle back-up on the ramp. This situation could cause a hazardous conflict for vehicles trying to enter/exit simultaneously, as well as for workers swapping roll carts. This space would be available to other contractors visiting the site to service the elevator, the pool, or other facility needs.

**Recreation**

As this development does not have residential component, there is no recreation space requirement.

**Fiscal Impact**

The applicant has provided a Fiscal Analysis Report. The Report states that the Town will receive revenues of \$270,000 and an overall cost to the town of \$78,000 resulting in a net revenue gain of \$192,000. Please see the applicant’s fiscal analysis for additional information.

**Additional Stipulations in Resolution A** (see Resolution A for detailed requirements)

<b><i>Accessibility Requirements</i></b>	The applicant shall provide the minimum required handicapped facilities and infrastructure required by the American with Disabilities codes and standards.
<b><i>Off-Site Construction Easements</i></b>	The applicant shall provide documentation of approval from property owners affected by off-site construction, if necessary.
<b><i>Landscape Protection</i></b>	The applicant shall obtain approval of a Landscape Plan, including a maintenance schedule, and canopy trees.
<b><i>Public Art</i></b>	The applicant is proposing to incorporate public art in their lobby. We recommend that applicant work with the Town’s Public Arts Officer.
<b><i>Erosion Control</i></b>	The applicant shall be required to provide a performance bond in

	accordance with Section 5-97.1 Bonds of the Town Code of Ordinance if more than one acre of land is disturbed.
<b><i>Lighting Plan</i></b>	The applicant shall submit site plans and other required documents to satisfy the lighting requirements of Section 5.11 of the Land Use Management Ordinance.

<b><i>Overhead Power Lines</i></b>	The applicant shall comply with section <a href="#">5.12.2 of the Land Use Management Ordinance</a> <sup>13</sup>
<b><i>Fire</i></b>	The applicant shall be required to obtain Fire Marshal approval for required fire safety features and infrastructure. This project is in the Town's Fire District and building construction materials are limited.
<b><i>Solid Waste Management Plan</i></b>	The applicant must obtain approval of a Solid Waste Management Plan from Orange County Solid Waste Department.
<b><i>Schools Adequate Public Facility Ordinance</i></b>	Not applicable.
<b><i>Construction Management Plan</i></b>	A Construction Management Plan shall be reviewed and approved by the Town Manager.
<b><i>Construction Phasing</i></b>	If applicable the applicant shall provide a Phasing Plan, detailing when public improvements and stormwater management structures shall be completed and inspected.
<b><i>Traffic and Pedestrian Control Plan</i></b>	The applicant shall provide a Work Zone Traffic Control Plan for movement of motorized and non-motorized vehicles on any public street that will be disrupted during construction.
<b><i>Repairs in the Public Rights-of-Ways</i></b>	The applicant shall repair all damage for work in the public right-of-way related to the construction of this project.
<b><i>Street Closure Plan</i></b>	The applicant shall provide a street closure plan, subject to Town Manager approval, for any work requiring street closures.

### **SPECIAL USE PERMIT FINDINGS**

The applicant's materials are included as attachments to this memorandum. All information submitted at the public hearing will be included in the record of the hearing. Based on the evidence submitted, the Council will consider whether or not it can make each of the four required findings for the approval of the Special Use Permit. The four findings are:

Finding #1: That the use or development is located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare;

Finding #2: That the use or development would comply with all required regulations and standards of the Land Use Management Ordinance;

Finding #3: That the use or development is located, designed, and proposed to be operated so as to maintain or enhance the value of contiguous property, or that the use or development is a public necessity; and

Finding #4: That the use or development conforms to the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the Comprehensive Plan.

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<sup>13</sup>[https://www.municode.com/library/nc/chapel\\_hill/codes/code\\_of\\_ordinances?nodeId=CO\\_APXALOUSMA\\_ART5\\_DEDEST\\_5.12UT](https://www.municode.com/library/nc/chapel_hill/codes/code_of_ordinances?nodeId=CO_APXALOUSMA_ART5_DEDEST_5.12UT)

## **STAFF RECOMMENDATION**

**Staff Recommendation:** That the Community Design Commission, Transportation and Connectivity Board, and Environmental Stewardship Advisory Board forward a recommendation to the Planning Commission and the Council; and that the Planning Commission also forwards a recommendation to the Town Council.

**PROJECT FACT SHEET REQUIREMENTS**  
**Check List of Regulations and Standards**  
**Special Use Permit Application**

AC Hotel by Marriott 210 W. Rosemary Street	STAFF EVALUATION BASED ON TC-2 ZONING	
	COMPLIANCE	NONCOMPLIANCE
Use Permitted	√	
Gross Land Area ( )	√	
Minimum Lot Width	√	
Maximum Floor Area	√	
Maximum # of Dwelling Units	n/a	
Minimum Recreation Space	n/a	
Impervious Surface Limits	√	
Land Disturbance Minimized	√	
Maximum # of Vehicular Parking Spaces	√	
Minimum # of Bicycle Parking Spaces	√	
Minimum # of Loading Spaces	n/a	
Minimum Street Setbacks	n/a	
Minimum Interior Setbacks	√	
Minimum Solar Setback	√	
Maximum Primary and Secondary Height limits	√	
Minimum Landscape Bufferyards	√(with CDC approval of Alt. Buffer)	
Steep Slope Compliance	n/a	
Parking Lot Screening	n/a	
Public Water and Sewer	√	
Adequate Public Schools Facilities	n/a	

Prepared June 14, 2015

**RESOLUTION A**

**A RESOLUTION APPROVING AN APPLICATION FOR A SPECIAL USE PERMIT FOR THE AC HOTEL BY MARRIOTT (PROJECT #14-076)**

BE IT RESOLVED by the Council of the Town of Chapel Hill that it finds that a Special Use Permit application, proposed by OTO Development, LLC for the AC Hotel by Marriott, located at 210 W. Rosemary Street on property identified as Orange County Property Identifier Numbers 9788-27-6387, 9788-27-6326, and 9788-27-5353, located in the Town Center-2 (TC-2) zoning district, if developed according to the Site Plan dated January 26, 2015 and revised February 27, 2015, the conditions listed below would:

1. Be located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare;
2. Comply with all required regulations and standards of the Land Use Management Ordinance;
3. Be located, designed, and operated so as to maintain or enhance the value of contiguous property; and
4. Conform with the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the 2020 Comprehensive Plan.

BE IT FURTHER RESOLVED that the Council of the Town of Chapel Hill hereby approves the application for a Special Use Permit for AC Hotel by Marriott, 210 W. Rosemary Street, in accordance with the plans listed above and with the conditions listed below:

Stipulations Specific to the Development

1. Construction Deadline: That construction begin by \_\_\_\_\_ (2 years from the date of approval), to be completed by \_\_\_\_\_ (4 years from the date of approval).
2. Land Use Intensity: This Special Use Permit authorizes the following:

<b>Use: Business Convenience, Business General, Clinic</b>	
Gross Land Area	34,685.2 square feet
Total Floor Area Allowed	68,330 square feet
Maximum Vehicular Parking Spaces	111 spaces
Minimum Bicycle Parking Spaces	12 spaces, 8 Class I and 4 Class II
Impervious Surface	31,551 square feet

3. Detailed Plan Review and Approval: Prior to the issuance of a Zoning Compliance Permit, final detailed site plans, grading plans, utility/lighting plans, stormwater management plans

(with hydrologic calculations), landscape plans, and landscape maintenance plans shall be approved. Such plans shall conform to plans approved by the Council and demonstrate compliance with all applicable regulations and standards of the Land Use Management Ordinance and the Design Manual.

Plans, plats, and associated detailed requirements as set forth in the stipulations of this resolution and incorporated into the special use permit shall be reviewed and approved by the Town Manager, or his designee, as well as outside agencies, such as NCDOT, OWASA and Duke Energy, where indicated.

4. Performance Bond: Prior to commencing construction activity for required improvements in the public right-of-way, a performance bond shall be provided to the Town to insure that improvements in the public right-of-way, are in accordance with Town standards.

#### Access

5. Off-site Construction Easements: Prior to any land disturbance on abutting properties it will be necessary to provide documentation of approval from the affected property owner(s).
6. Accessibility Requirements: That prior to issuance of a Certificate of Occupancy, the applicant shall provide the minimum required handicapped parking spaces and design all handicapped parking spaces, ramps, and crosswalks, and associated infrastructure according to Americans with Disabilities Act standards, North Carolina Building Code, American National Standards Institute (ANSI) Code, and Town standard.
7. Performance Bond: Prior to commencing construction activity for the improvements in the public right-of-way, a performance bond shall be provided to the Town Manager to insure the construction and installation of the improvements in accordance with the standards and provisions approved by the Town as part of the project.
8. Low Vision Design Features: Pedestrian facilities that incorporate low vision design features are encouraged.

#### Transportation

9. Public Right-of-Way Dedication Plat: That prior to the issuance of a Certificate of Occupancy, the applicant shall submit a recorded right-of-way dedication plat for all required W. Rosemary Street public roadway improvements associated with the development. That the plat shall be reviewed and approved by the Town Manager prior to recordation.
10. Pedestrian Easement Plat: That prior to the issuance of a Certificate of Occupancy, the applicant shall submit a recorded pedestrian easement plat for all required streetscape improvements adjacent to the Church Street right-of-way associated with the development including the underground duct bank. That the plat shall be reviewed and approved by the Town Manger prior to recordation.



11. Sidewalks: Prior to issuance of a Certificate of Occupancy, the developer shall provide an eight foot wide concrete sidewalk constructed to Town standard along the W. Rosemary Street and Church Street frontages.
12. Church Street Driveways: Prior to issuance of a Zoning Compliance Permit, it will be necessary for the Town Manger to review and approve details for concrete ramp-type driveways to access the site and not street-type driveway and that ADA approved detectable warning pavers be installed in the portion of the sidewalk driveway crossing and that a detail of the curb ramp and the intersection be provided.
13. Traffic Signal Timing: Prior to issuance of a Zoning Compliance Permit the developer shall provide a payment of \$3,000 for signal retiming for the following four intersections: Church Street/W. Rosemary Street; Church Street/W. Franklin Street; and N. Columbia Street/W. Rosemary Street.
14. W. Rosemary Street Sharrows: Prior to issuance of a Certificate of Occupancy, the developer shall provide sharrow markings on W. Rosemary Street westbound lane along the property frontage.
15. W. Rosemary Street Streetscape Improvements: Prior to issuance of a Certificate of Occupancy, the developer shall provide four tree grates, three light poles, trash receptacles, and bike racks to the streetscape in front of the hotel along the W. Rosemary Street frontage.
16. Church Street Improvements: Prior to issuance of a Certificate of Occupancy, the developer shall provide four tree grates, one light pole, trash receptacles, new fire hydrant, and bike rack to the streetscape in front of the hotel along the Church Street frontage.
17. Repairs in Public Right-of-Way: Prior to issuance of a Certificate of Occupancy, it will be necessary to repair all damage for work in the public right-of-way related to the construction of this project, which may include pavement milling and overlay. The design must be reviewed and approved by the Town Manager prior to a Zoning Compliance Permit.
18. Street Closure Plan: That prior to the issuance of a Zoning Compliance Permit, the developer shall provide a street closure plan, subject to Town Manager approval, for any work requiring street, sidewalk, or lane closures.
19. Work Zone Traffic Control Plan: Prior to issuance of a Zoning Compliance Permit, it will be necessary to provide a Work Zone Traffic Control Plan and a Construction Management Plan for approval by the Town Manager. The Work Zone Traffic Control Plan shall comply with the Manual on Uniform Traffic Control Devices. The Construction Management Plan shall provide staging, construction worker parking, construction vehicle routes, and hours of construction.
20. Wayfinding Signage: Prior to issuance of a Certificate of Occupancy, the developer shall provide wayfinding signage in the underground garage and indicating direction of travel.

Prior to issuance of a Zoning Compliance Permit, the wayfinding signage shall be reviewed and approved by the Town Manager.

21. Transportation Management Plan: A Transportation Management Plan for the development shall be approved prior to issuance of the Zoning Compliance Permit with annual updates required. The required components of the Transportation Management Plan shall include:
  - a) Designation of a Transportation Coordinator to communicate, attend workshops and conferences, and promote alternate modes of transportation.
  - b) Submission of an Employees due biannually.
  - c) Submission of an updated annual Transportation Management Plan Report.
  
22. Bus Stop Payment: Prior to issuance of a Zoning Compliance Permit the developer shall provide a payment of \$5,000 for maintenance of existing transit stops in the area.
  
23. Bicycle Parking: That prior to the issuance of a Zoning Compliance Permit, the developer shall provide dimensioned details for a bicycle parking space for each unit that comply with Town parking standards. The bicycle parking design must comply with the spring 2010 Association of Pedestrian and Bicycle Professionals Guidelines, and the Class I and Class II bicycle parking standards required by the Town Design Manual.
  
24. Parking Lot Standards: Prior to issuance of a Certificate of Occupancy, the developer shall construct the underground parking garage and drive aisles to Town standard or National Parking Associate standards for pavement design and dimensions.
  
25. Fire Flow: Prior to issuance of a Zoning Compliance Permit, the developer shall submit a fire flow report prepared and sealed by a professional engineer licensed in North Carolina. The report must show the available fire flow will meet Town standard.

Landscape and Architecture

26. Landscape Bufferyards: That the developer shall provide the following bufferyards:

<b>Location</b>	<b>Required Buffer</b>
West – Church Street, Town Center-2 (TC-2)	No buffer
North – Residential-3; Northside Neighborhood Conservation District (NCD), single-family dwellings	Alternative Buffer with reduced widths to 10 feet, fewer plantings, and fence.
East – Town Center-2 and Northside NCD; restaurant	No buffer
South – W. Rosemary Street	No buffer

27. Landscape Protection: Prior to issuance of a Zoning Compliance Permit, a detailed Landscape Protection Plan shall be approved. The plan shall include which trees will be removed and which will be preserved, critical root zones of all rare and specimen trees, and clearly indicate names and species.
28. Tree Protection Fencing Prior to Construction: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a note on the Final Plans indicating that tree protection fencing will be installed prior to land-disturbing activity on the site. Tree protection fencing shall be provided around construction limits and indicated construction parking and materials staging/storage areas, and Town standard landscaping protection notes, subject to Town Manager approval.
29. Landscape Planting Plan: Prior to issuance of a Zoning Compliance Permit, the developer shall provide a detailed Landscape Planting Plan with a detailed planting list, subject to Town Manager approval.
30. Green Roof Plantings: Prior to issuance of a Zoning Compliance Permit, the applicant provide details of the green roof installation including plan list and how the areas will be accessed for ongoing maintenance.
31. Transformer Screening: That the transformer located adjacent to Church Street be screened with landscaping allowing room for maintenance and access as required by Duke Power and be approved by the town Manager prior to issuance of a Zoning Compliance Permit.
32. Lighting Plan Approval: Prior to issuance of a Zoning Compliance Permit, a Lighting Plan shall be submitted with approval by the Town and Duke Energy.
33. Community Design Commission Approval: That the applicant obtain Community Design Commission approval of building elevations and lighting, including the location and screening of all HVAC/Air Handling Units for this project, prior to issuance of a Zoning Compliance Permit. That prior to issuance of a Zoning Compliance Permit the Commission shall approve a lighting plan for this site and shall take additional care during review to ensure that the proposed lighting plan will minimize 1) upward light pollution and 2) offsite spillage of light.
34. Alternative Buffer: That prior to the issuance of Zoning Compliance Permit, the Community Design Commission approve an alternate buffer. The approval shall determine that the alternate buffer along the northern property line will provide the same degree of visual and noise obstruction as the required buffer.

#### Environment

35. Energy Management Plan: That prior to issuance of a Zoning Compliance Permit, the applicant shall provide an Energy Management Plan (EMP) for Town approval. The plan's goal includes to incorporate a "20 percent more energy efficient" feature to outperform the American Society of Heating, Refrigeration, and Air Conditioning Engineers (ASHRAE)

energy efficiency standard in place at the time of approval. The property owner shall provide, by the Certificate of Occupancy date, a letter sealed by a licensed professional engineer showing the anticipated energy performance of the buildings, as designed and built, satisfying the goal of “20 percent more energy efficient” expectation.

### Stormwater Management

36. Stormwater Management Plan: That this project must comply with the Section 5.4 Stormwater Management of the Land Use Management Ordinance.
37. Green Roof: That prior to issuance of a Zoning Compliance Permit, the Green Roof be compliant with NCDENR BMP manual and approval by the town Manager for treatment for 85% TSS removal from new impervious surfaces
38. Jordan Watershed Stormwater Management for New Development: That if the total disturbed area exceeds 0.5 acres, this project must comply with Section 5.19 Jordan Watershed Stormwater Management for New Development of the Land Use Management Ordinance to provide the required reductions in nitrogen and phosphorus loads for new development and redevelopment projects.
39. Silt Control: That the applicant takes appropriate measures to prevent and remove the deposit of wet or dry silt on adjacent paved roadways.
40. Jordan Surety: That if the total disturbed area exceeds 0.5 acres, prior to issuance of a Certificate of Occupancy, the owner shall post a maintenance bond or other surety instrument satisfactory to the Town Manager, in an amount equal to one hundred twenty-five (125) percent of the construction cost of each stormwater management facility to assure maintenance, repair, or reconstruction necessary for adequate performance of the stormwater management facility, or establish a stormwater maintenance (sinking fund) budget and escrow account in accordance with the requirements of Section 5.19 of the Land Use Management Ordinance.
41. Erosion Control: The applicant shall provide a copy of the approved erosion and sediment control permit from Orange County Erosion Control Division prior to receiving a Zoning Compliance Permit. During the construction phase, additional erosion and sediment controls may be required if the proposed measures do not contain the sediment. Sediment leaving the property is a violation of the Town’s Erosion and Sediment Control Ordinance.
42. Erosion Control Inspections: That, in addition to the requirement during construction for inspection after every rainfall, the applicant shall inspect the erosion and sediment control devices daily, make any necessary repairs or adjustments to the devices, and maintain inspection logs documenting the daily inspections and any necessary repairs.

43. Curb Inlets: The applicant shall provide pre-cast curb inlet hoods and covers stating, "Dump No Waste! Drains to Jordan Lake", in accordance with the specifications of the Town Standard Detail SD-5A, for all new curb inlets for private, Town and State rights-of-way.
44. As-Built Plans: That prior to the issuance of a Certificate of Occupancy, the applicant shall provide certified as-built plans for building footprints, parking lots, street improvements, storm drainage systems and stormwater management structures, and all other impervious surfaces, and a tally of the constructed impervious area. The as-built plans should be in DXF binary format using State plane coordinates and NAVD 88.
45. Phasing Plan: That prior to issuance of a Zoning Compliance Permit, the applicant shall obtain approval of a Phasing Plan that provides details of which improvements are to be constructed during each phase. The phasing plan shall detail public improvements and stormwater management structures will be completed in each phase prior to requesting a Certificate of Occupancy. Construction for any phase may not begin until all public improvements in previous phases are complete, with a note to this effect on the final plans and plats.
46. On-Site/Adjacent Stormwater Features: That the final plans locate and identify existing site conditions including all on-site and adjacent stormwater drainage features on the plans prior to issuance of a Zoning Compliance Permit. The final plans must provide proper inlet protection for the stormwater drainage inlets on or adjacent to the site to ensure the stormwater drainage system will not be obstructed with construction debris.
47. P.E. Certification: That prior to the issuance of a Certificate of Occupancy for any phase, the applicant shall provide a certification, signed and sealed by a North Carolina-licensed Professional Engineer, that the stormwater management facilities are constructed in accordance with the approved plans and specifications.
48. Repair/Replacement of Damaged Stormwater Infrastructure: Existing stormwater infrastructure that is damaged as a result the project demolition or construction, must be repaired or replaced, as specified by the Stormwater Management Engineer, prior to requesting a Certificate of Occupancy.
49. Contamination Assessment: That prior to construction, dewatering or approval of foundation drains being connected to the stormwater system, groundwater sampling must be performed and the results submitted to the Town. Ongoing analysis during construction may be necessary and analysis of the foundation drain discharged may also be necessary. The analysis shall be reviewed and approved by the Town Manager.

#### Water, Sewer, and Other Utilities

50. Duct Bank: Prior to issuance of a Certificate of Occupancy, the applicant shall install a duct bank along the hotel frontage on W. Rosemary Street and Church Street. The duct bank shall be located under the public sidewalk and above the underground garage.

51. Additional Empty Conduits: The applicant shall install additional empty conduits within the duct bank to provide for future underground utility installations.
52. Utility/Lighting Plan Approval: That the final utility/lighting plan shall be approved by Orange Water and Sewer Authority, Duke Energy Company, and other local utility service providers, and the Town Manager before issuance of a Zoning Compliance Permit. The property owner shall be responsible for assuring that these utilities can continue to serve the development. In addition, detailed construction drawings shall be submitted to OWASA for review/approval prior to issuance of a Zoning Compliance Permit.
53. Lighting Plan: That prior to issuance of a Zoning Compliance Permit, the applicant shall submit site plans and other required documents to satisfy the lighting requirements of Section 5.11 of the Land Use Management Ordinance including submission of a lighting plan, providing for adequate lighting on public sidewalks, including driveway crossings, and beneath awnings, demonstrating compliance with Town standards, sealed by a Professional Engineer, for Town Manager approval.
54. Relocation of Overhead Utilities Underground: Prior to issuance of a Certificate of Occupancy, it will be necessary to provide for the underground installation of all public utilities as specified by Section 5.12.2 in the Land Use Management Ordinance.
55. Water/Sewer Line Construction: That all public water and sewer plans be approved by OWASA and constructed according to their standards. Where sewer lines are located beneath drive aisles and parking areas construction methods approved by OWASA shall be employed, to ensure that sewer lines will not be damaged by heavy service vehicles. That prior to issuance of a Zoning Compliance Permit, final plans shall be approved by OWASA and the Town Manager.
56. OWASA Approval: That prior to issuance of a Zoning Compliance Permit, easement plats and documentation as required by OWASA and the Town Manager, shall be recorded if necessary.

#### Fire Safety

57. Fire Sprinklers: That the applicant shall install sprinklers under the North Carolina Fire Code prior to a Certificate of Occupancy. Prior to issuance of a Zoning Compliance Permit, the plans shall show all proposed fire department connections to such systems.
58. New Fire Hydrant: Prior to issuance of a Certificate of Occupancy, the applicant shall install a new fire hydrant on Church Street in front of the hotel.
59. Hydrants Active: That the applicant shall provide active fire hydrant coverage, acceptable to the Fire Department, for any areas where combustible construction materials will be stored or installed, prior to having such materials delivered to the site. All required fire hydrants must be installed, active, and accessible for the Fire Department use prior to the arrival of

combustible materials on site. That fire protection systems shall be installed according to Town Ordinance, the NC Fire Code, and NFPA 13.

60. Fire Hydrant and FDC Locations: That the Final Plans shall indicate the locations of existing and proposed fire hydrants and Fire Department Connections (FDC). Fire Department Connections shall be located on the street side of the building within 100 feet of a hydrant. Hydrant spacing shall comply with the Town Design Manual. Design shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
61. Firefighting Access during Construction: That as required by NC Fire Code (Section 1410.1 Required Access), vehicle access for firefighting shall be provided to all construction or demolition sites including vehicle access to within 100 feet of temporary or permanent fire department connections and hydrants. Vehicle access shall be provided by either temporary or permanent roads capable of supporting vehicle loading under all weather conditions.
62. Fire Flow Report: That the Final Plan application shall include a fire flow report sealed by an Engineer registered in the State of North Carolina. An OWASA flow test must be provided with the report. Fire flow shall meet the 20 psi or exceed the requirements set forth in the Town Design Manual. The Fire Flow Report shall be reviewed and approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
63. Heavy-Duty Paving: That prior to issuance of a Certificate of Occupancy the applicant shall provide heavy duty paving designed and built to withstand fire apparatus weighing at least 75,000 pounds.
64. Fire Lane: Prior to issuance of a Certificate of Occupancy, the fire lane shall be marked and signed in accordance with Town standards, with the associated plans approved by the Town Manager prior to issuance of a Zoning Compliance Permit.

#### Solid Waste Management and Recycling

65. Solid Waste Management Plan: That prior to issuance of a Zoning Compliance Permit a detailed Solid Waste Management Plan, including a recycling plan and a plan for managing and minimizing construction debris, shall be approved by the Town Manager. The plan shall include dimensioned, scaled details of any proposed refuse/recycling collection areas, associated screening, and protective bollards, if applicable. Each bulk waste container shall be labeled as to type of material to be collected. Prior to issuance of a Zoning Compliance Permit, the developer shall provide documentation of an agreement for solid waste and recycling collection by a private provider.
66. Roll Cart Staging and Storage: No storage of staging of roll carts may occur within any public right-of-way at any time.
67. Solid Waste Compactor: That the developer include a location for storage and service of a solid waste compactor in the event that one is needed in the future.



68. Roll Cart Storage and Access: That the developer provide a plan showing access, staging, and storage locations for solid waste collection service and who will be responsible for implementation of the plan and that the plan be approved by the Town Manager prior to issuance of a Zoning Compliance Permit.
69. Reservation for Service Vehicle Parking: That the developer designates a parking space reserved for solid waste collection vehicles and other service vehicles close to the solid waste cart storage area.
70. Construction Waste: Prior to the issuance of a Zoning Compliance Permit, a detailed Construction Waste Management Plan shall be approved by the town Manager. The plan shall include dimensioned, scaled details of any proposed refuse/recycle collections areas. Clean wood waste, scrap metal and corrugated cardboard, all present in construction waste, must be recycled. All haulers of construction waste must be properly licensed. The developer shall provide the name of the permitted waste disposal facility to which any land clearing or demolition waste will be delivered.

#### State and Federal Approvals

71. State or Federal Approvals: That any required State or federal permits or encroachment agreements (e.g., 401 water quality certification, 404 permit) shall be approved and copies of the approved permits and agreements be submitted to the Town of Chapel Hill prior to the issuance of a Zoning Compliance Permit.

#### Miscellaneous

72. Temporary Construction Access Agreements: Prior to issuance of a Zoning Compliance Permit, the applicant shall provide construction agreements with adjacent property owners, where necessary, subject to Town Manager approval. If the abutting property is to be used as part of construction access, provide documentation of permission from the owner of said property.
73. Construction Management Plan: That a Construction Management Plan be approved by the Town Manager prior to issuance of a Zoning Compliance Permit. The construction management plan shall: 1) indicate how construction vehicle traffic will be managed, 2) identify parking areas for on-site construction workers including plans to prohibit parking in residential neighborhoods, 3) indicate construction staging and material storage areas, 4) identify construction trailers and other associated temporary construction management structures, and 5) indicate how the project construction will comply with the Town's Noise Ordinance.
74. Traffic and Pedestrian Control Plan: That the applicant shall provide a Work Zone Traffic Control Plan for movement of motorized and non-motorized vehicles on any public street that will be disrupted during construction. The plan must include a pedestrian management plan indicating how pedestrian movements will be safely maintained. The existing crosswalk at W. Rosemary Street and Pritchard Avenue shall be included in the Pedestrian Control

Plan. The plan must be reviewed and approved by the Town Manager prior to the issuance of a Zoning Compliance Permit. At least 5 working days prior to any proposed lane or street closure the applicant must apply to the Town Manager for a lane or street closure permit.

75. Construction Sign Required: That the applicant shall post a construction sign at the development site that lists the property owner's representative and telephone number, the contractor's representative and telephone number, and a telephone number for regulatory information at the time of issuance of a Building Permit, prior to the commencement of any land disturbing activities. The construction sign may have a maximum of 32 square feet of display area and maximum height of 8 feet. (§5.14.3(g) of Land Use Management Ordinance). The sign shall be non-illuminated, and shall consist of light letters on a dark background. That prior to the issuance of a Zoning Compliance Permit, a detail of the sign shall be reviewed and approved by the Town Manager.
76. Open Burning: That the open burning of trees, limbs, stumps, and construction debris associated with this development is prohibited.
77. Detailed Plans: That prior to the issuance of a Zoning Compliance Permit, final detailed site plans, grading plans, utility/lighting plans, stormwater management plans (with hydrologic calculations), landscape plans, and landscape maintenance plans shall be approved by the Town Manager. Such plans shall conform to plans approved by this application and demonstrate compliance with all applicable regulations and the design standards of the Land Use Management Ordinance and the Design Manual.
78. As-Built Plans: That prior to issuance of a Certificate of Occupancy, the applicant shall provide certified as-built plans for building footprints, parking lots, street improvements, storm drainage systems and stormwater management structures, and all other impervious surfaces. The as-built plans should be in DXF binary format using State plane coordinates and NAVD 88. The applicant shall also contact the Town's Engineering and Design Services Division for address assignment of each unit.
79. Vested Right: This Special Use Permit constitutes a site specific development plan establishing a vested right as provided by N.C.G.S. Section 160A-385.1 and Appendix A of the Chapel Hill Land Use Management Ordinance.
80. Continued Validity: That continued validity and effectiveness of this approval shall be expressly conditioned on the continued compliance with the plans and conditions listed above.
81. Non-Severability: That if any of the above conditions is held to be invalid, approval in its entirety shall be void.

BE IT FURTHER RESOLVED that the Council hereby approves the application for a Special Use Permit for AC Hotel at Marriott at 210 W. Rosemary Street.

This the \_\_\_\_\_ day of \_\_\_\_\_ 2015.



**RESOLUTION B**  
(Denying the Special Use Permit Application)

**A RESOLUTION DENYING AN APPLICATION FOR SPECIAL USE PERMIT FOR THE AC HOTEL BY MARRIOTT (PROJECT #14-076)**

BE IT RESOLVED by the Council of the Town of Chapel Hill that it finds that a Special Use Permit application, proposed by proposed by OTO Development, LLC for the AC Hotel by Marriott, located at 210 W. Rosemary Street on property identified as Orange County Property Identifier Numbers 9788-27-6387, 9788-27-6326, and 9788-27-5353, located in the Town Center-2 (TC-2) zoning district, if developed according to the Site Plan dated January 26, 2015 and revised February 27, 2015, the conditions listed below would not:

1. Be located, designed, and proposed to be operated so as to maintain or promote the public health, safety, and general welfare;
2. Comply with all required regulations and standards of the Land Use Management Ordinance;
3. Be located, designed, and operated so as to maintain or enhance the value of contiguous property; and
4. Conform with the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the 2020 Comprehensive Plan.

BE IT FURTHER RESOLVED that the Town Council hereby denies the application for a Special Use Permit for the AC Hotel by Marriott, 210 W. Rosemary Street.

This the \_\_\_\_ day of \_\_\_\_\_.

# SPECIAL USE PERMIT APPLICATION



**TOWN OF CHAPEL HILL**  
Planning Department  
405 Martin Luther King Jr. Blvd  
Chapel Hill, NC 27514  
phone (919) 968-2728 fax (919) 969-2014  
www.townofchapelhill.org

Parcel Identifier Number (PIN): 97882776387, 9788276326, 9788275353

Date: 01/26/15

## Section A: Project Information

Project Name: AC by Marriott

Property Address: 214 West Rosemay Street Zip Code: 27516

Use Groups (A, B, and/or C): Group B Existing Zoning District: TC-2

Project Description: 4 story hotel with 123 guest rooms, 2 levels of below grade parking with 112 parks, outdoor swimming pool, bar, and 1100 sq-ft meeting space.

## Section B: Applicant, Owner and/or Contract Purchaser Information

### Applicant Information (to whom correspondence will be mailed)

Name: OTO Development, LLC

Address: 100 Dunbar Street, Suite 402

City: Spartanburg State: South Carolina Zip Code: 29306

Phone: (864) 327-4046 Email: dmitchell@otodevelopment.com

The undersigned applicant hereby certifies that, to the best of his knowledge and belief, all information supplied with this application is true and accurate.

Signature:  Date: 1/23/2015

### Owner/Contract Purchaser Information:

Owner  Contract Purchaser

Name: Chapel Hill Hotels, LLC

Address: 100 Dunbar Street, Suite 402

City: Spartanburg State: South Carolina Zip Code: 29306

Phone: (864) 327-4046 Email: dmitchell@otodevelopment.com

The undersigned applicant hereby certifies that, to the best of his knowledge and belief, all information supplied with this application is true and accurate.

Signature:  Date: 1/23/2015



**PROJECT FACT SHEET**  
 TOWN OF CHAPEL HILL  
 Planning Department

**Section A: Project Information**

Application type: Special Use Permit Application Date: 01/26/15

Project Name: AC by Marriott

**Use Type:** (check/list all that apply)

Office/Institutional  Residential  Mixed-Use  Other: Hotel

**Overlay District:** (check all those that apply)

Historic District  Neighborhood Conservation District  Airport Hazard Zone

Northside

**Section B: Land Area**

Net Land Area (NLA): Area within zoning lot boundaries		NLA=	31,532	sq. ft.
Choose one, or both, of the following (a or b), not to exceed 10% of NLA	a) Credited Street Area (total adjacent frontage) x ½ width of public right-of-way	CSA=	8,985.25	sq. ft.
	b) Credited Permanent Open Space (total adjacent frontage) x ½ public or dedicated open space	COS=		sq. ft.
TOTAL: NLA + CSA and/or COS = Gross Land Area (not to exceed NLA + 10%)		GLA=	34,685.2	sq. ft. (+10% of NLA)

**Section C: Special Protection Areas, Land Disturbance, and Impervious Area**

**Special Protection Areas:** (check all those that apply)

Jordan Buffer  Resource Conservation District  100 Year Floodplain  Watershed Protection District

Land Disturbance	Total (sq ft)
Area of Land Disturbance (Includes: Footprint of proposed activity plus work area envelope, staging area for materials, access/equipment paths, all grading, including off-site clearing)	34,100
Area of Land Disturbance within RCD	N/A
Area of Land Disturbance within Jordan Buffer	N/A

Impervious Areas	Existing (sq ft)	Demolition (sq ft)	Proposed (sq ft)	Total (sq ft)
Impervious Surface Area (ISA)	23,015	23,015	31,532	31,532
Impervious Surface Ratio: Percent Impervious Surface Area of Gross Land Area (ISA/GLA) %	72.9%	72.9%	100%	100%
If located in Watershed Protection District, % of impervious surface on 7/1/1993	72.9%			



# PROJECT FACT SHEET

TOWN OF CHAPEL HILL

Planning Department

## Section D: Dimensions

Dimensional Unit (sq ft)	Existing (sq ft)	Demolition (sq ft)	Proposed (sq ft)	Total (sq ft)
Number of Buildings	3	3	1	1
Number of Floors	2	2	4	4
Recreational Space	0	0	2,800	2,800

### Residential Space

Dimensional Unit (sq ft)	Existing (sq ft)	Demolition (sq ft)	Proposed (sq ft)	Total (sq ft)
Floor Area (all floors – heated and unheated)	8,840	8,840	0	0
Total Square Footage of All Units				
Total Square Footage of Affordable Units				
Total Residential Density				
Number of Dwelling Units	3	3	0	0
Number of Affordable Dwelling Units				
Number of Single Bedroom Units				
Number of Two Bedroom Units				
Number of Three Bedroom Units				

### Non-Residential Space (Gross Floor Area in Square Feet)

Use Type	Existing	Proposed	Uses	Existing	Proposed
Commercial					
Restaurant			# of Seats		
Government					
Institutional					
Medical					
Office					
Hotel	0	68300	# of Rooms	0	123
Industrial					
Place of Worship			# of Seats		
Other					

Dimensional Requirements		Required by Ordinance	Existing	Proposed
Setbacks (minimum)	Street	0		11
	Interior (neighboring property lines)	8		12
	Solar (northern property line)	0		12
Height (maximum)	Primary	30'/40'		30
	Secondary	50'		50
Streets	Frontages	12		313
	Widths	15		150

\* 30' on North, 40' on all other sides





# PROJECT FACT SHEET

TOWN OF CHAPEL HILL

Planning Department

## Section F: Adjoining or Connecting Streets and Sidewalks

(Note: For approval of proposed street names, contact the Engineering Department)

Street Name	Right-of-way Width	Pavement Width	Number of Lanes	Existing Sidewalk*	Existing curb/gutter
West Rosemary Street	Variable	36.7'	3	<input checked="" type="checkbox"/> Yes	<input checked="" type="checkbox"/> Yes
Church Street	30'	25'	2	<input checked="" type="checkbox"/> Yes	<input checked="" type="checkbox"/> Yes

List Proposed Points of Access (Ex: Number, Street Name):

\*If existing sidewalks do not exist and the applicant is adding sidewalks, please provide the following information:

Sidewalk Information			
Street Names	Dimensions	Surface	Handicapped Ramps
Church Street	30'	Concrete	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> No <input type="checkbox"/> N/A
			<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A

## Section G: Parking Information

Parking Spaces	Minimum	Maximum	Proposed
Regular Spaces	N/A	111 (0.9 per lodging unit)	112*
Handicap Spaces			5
Total Spaces			115
Loading Spaces			
Bicycle Spaces	8		10
Surface Type	Below grade parking		

\* Includes 24 compact

## Section H: Landscape Buffers

Location (North, South, Street, Etc.)	Minimum Width	Proposed Width	Alternate Buffer	Modify Buffer
North	20'-Type C	12'	<input checked="" type="checkbox"/> Yes	<input type="checkbox"/> Yes
East	0	10'	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes
West Rosemary Street	N/A	N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes
Church Street	N/A	N/A	<input type="checkbox"/> Yes	<input type="checkbox"/> Yes



# PROJECT FACT SHEET

TOWN OF CHAPEL HILL

Planning Department

## Section I: Land Use Intensity

Existing Zoning District: TC-2

Proposed Zoning Change (if any):

Note: Refer to Table 3.8-1 (Dimensional Matrix) in the Land Use Management Ordinance for help completing this table.

Zoning – Area – Ratio			Impervious Surface Thresholds			Minimum and Maximum Limitations	
Zoning District(s)	Floor Area Ratio (FAR)	Recreation Space Ratio (RSR)	Low Density Residential (0.24)	High Density Residential (0.50)	Non-Residential (0.70)	Maximum Floor Area (MFA) = FAR x GLA	Minimum Recreation Space (MSR) = RSR x GLA
TC-2	1.97	0.120				68,329.8	4,162.2
<b>TOTAL</b>						68,330	4,162
RCD Streamside		0.01					
RCD Managed		0.019					
RCD Upland							

## Section J: Utility Service

Check all that apply

<b>Water</b>	<input checked="" type="checkbox"/> OWASA	<input type="checkbox"/> Individual Well	<input type="checkbox"/> Community Well	<input type="checkbox"/> Other
<b>Sewer</b>	<input checked="" type="checkbox"/> OWASA	<input type="checkbox"/> Individual Septic Tank	<input type="checkbox"/> Community Package Plant	<input type="checkbox"/> Other
<b>Electrical</b>	<input checked="" type="checkbox"/> Underground	<input type="checkbox"/> Above Ground		
<b>Telephone</b>	<input checked="" type="checkbox"/> Underground	<input type="checkbox"/> Above Ground		
<b>Solid Waste</b>	<input type="checkbox"/> Town	<input checked="" type="checkbox"/> Private		



**SPECIAL USE PERMIT APPLICATION  
SUBMITTAL REQUIREMENTS  
TOWN OF CHAPEL HILL  
Planning Department**

The following must accompany your application. Failure to do so will result in your application being considered incomplete. For assistance with this application, please contact the Chapel Hill Planning Department (Planning) at (919)968-2728 or at [planning@townofchapelhill.org](mailto:planning@townofchapelhill.org).

X	<b>Application fee</b> (including <a href="#">Engineering Review fee</a> ) ( <a href="#">refer to fee schedule</a> )	Amount Paid \$	28,277
X	<b>Pre-application meeting</b> – with appropriate staff		
X	<b>Digital Files</b> - provide digital files of all plans and documents		
X	<b>Recorded Plat or Deed of Property</b>		
X	<b>Project Fact Sheet</b>		
*	<b>Traffic Impact Statement</b> – completed by Town’s consultant (or exemption) *Under Separate Cover		
X	<b>Description of Public Art Proposal</b>	(See Email)	
X	<b>Statement of Justification</b>		
X	<b>Response to Community Design Commission and Town Council Concept Plan comments</b>		
N/A	<b>Affordable Housing Proposal, if applicable</b>		
N/A	<b>Provide existing Special Use Permit, if Modification</b>		
X	<b>Mailing list of owners of property within 1,000 feet perimeter of subject property</b> ( <a href="#">see GIS notification tool</a> )		
X	<b>Mailing fee for above mailing list (mailing fee is double due to 2 mailings)</b>	Amount Paid \$	708.40
X	<b>Written Narrative describing the proposal</b>		
N/A	<b>Resource Conservation District, Floodplain, &amp; Jordan Buffers Determination</b> - necessary for all submittals		
N/A	<b>Jurisdictional Wetland Determination</b> – if applicable	* Requested. See Attached	
N/A	<b>Resource Conservation District Encroachment Exemption or Variance (determined by Planning)</b>		
N/A	<b>Jordan Buffer Authorization Certificate or Mitigation Plan Approval (determined by Planning)</b>		
X	<b>Reduced Site Plan Set (reduced to 8.5"x11")</b>		

**Stormwater Impact Statement (1 copy to be submitted)**

- a) Written narrative describing existing & proposed conditions, anticipated stormwater impacts and management structures and strategies to mitigate impacts
- b) Description of land uses and area (in square footage)
- c) Existing and proposed Impervious surface area in square feet for all subareas and project area
- d) Ground cover and uses information
- e) Soil information (classification, infiltration rates, depth to groundwater and bedrock)
- f) Time of concentration calculations and assumptions
- g) Topography (2-foot contours)
- h) Pertinent on-site and off-site drainage conditions
- i) Upstream and/or downstream volumes
- j) Discharges and velocities
- k) Backwater elevations and effects on existing drainage conveyance facilities
- l) Location of jurisdictional wetlands and regulatory FEMA Special Flood Hazard Areas



**SPECIAL USE PERMIT APPLICATION  
SUBMITTAL REQUIREMENTS  
TOWN OF CHAPEL HILL  
Planning Department**

- m) Water quality volume calculations
- n) Drainage areas and sub-areas delineated
- o) Peak discharge calculations and rates (1, 2, and 25-year storms)
- p) Hydrographs for pre- & post-development without mitigation, post-development with mitigation
- q) Volume calculations and documentation of retention for 2-year storm
- r) 85% TSS removal for post-development stormwater run-off
- s) Nutrient loading calculations
- t) BMP sizing calculations
- u) Pipe sizing calculations and schedule (include HGL & EGL calculations and profiles)

**Plan Sets (10 copies to be submitted no larger than 24"x36")**

Plans should be legible and clearly drawn. All plan sets sheets should include the following:

- Project Name
- Legend
- Labels
- North Arrow (North oriented toward top of page)
- Property Boundaries with bearing and distances
- Scale (Engineering), denoted graphically and numerically
- Setbacks
- Streams, RCD Boundary, Jordan Riparian Buffer Boundary, Floodplain, and Wetlands Boundary, where applicable
- Revision dates and professional seals and signatures, as applicable

**Cover Sheet**

- a) Include Project Name, Project fact information, PIN, Design team

**Area Map**

- a) Project name, applicant, contact information, location, PIN, & legend
- b) Dedicated open space, parks, greenways
- c) Overlay Districts, if applicable
- d) Property lines, zoning district boundaries, land uses, project names of site and surrounding properties, significant buildings, corporate limit lines
- e) Existing roads (public & private), rights-of-way, sidewalks, driveways, vehicular parking areas, bicycle parking, handicapped parking, street names.
- f) 1,000' notification boundary

**Existing Conditions Plan**

- a) Slopes, soils, environmental constraints, existing vegetation, and any existing land features
- b) Location of all existing structures and uses
- c) Existing property line and right-of-way lines



**SPECIAL USE PERMIT APPLICATION  
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TOWN OF CHAPEL HILL  
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- d) Existing utilities & easements including location & sizes of water, sewer, electrical, & drainage lines
- e) Nearest fire hydrants
- f) Nearest bus shelters and transit facilities
- g) Existing topography at minimum 2-foot intervals and finished grade
- h) Natural drainage features & water bodies, floodways, floodplain, RCD, Jordan Buffers & Watershed boundaries

### Detailed Site Plan

- a) Existing and proposed building locations
- b) Description & analysis of adjacent land uses, roads, topography, soils, drainage patterns, environmental constraints, features, existing vegetation, vistas (on & off-site)
- c) Location, arrangement, & dimension of vehicular parking, width of aisles and bays, angle of parking, number of spaces, handicapped parking, bicycle parking . Typical pavement sections & surface type
- d) Location of existing and proposed fire hydrants
- e) Location and dimension of all vehicle entrances, exits, and drives
- f) Dimensioned street cross-sections and rights-of-way widths
- g) Pavement and curb & gutter construction details
- h) Dimensioned sidewalk and tree lawn cross-sections
- i) Proposed transit improvements including bus pull-off and/or bus shelter
- j) Required landscape buffers (or proposed alternate/modified buffers)
- k) Required recreation area/space (including written statement of recreation plans)
- l) Refuse collection facilities (existing and proposed) or shared dumpster agreement
- m) Construction parking, staging, storage area, and construction trailer location
- n) Sight distance triangles at intersections
- o) Proposed location of street lights and underground utility lines and/or conduit lines to be installed
- p) Easements
- q) Clearing and construction limits
- r) Traffic Calming Plan – detailed construction designs of devices proposed & associated sign & marking plan

### Stormwater Management Plan

- a) Topography (2-foot contours)
- b) Existing drainage conditions
- c) RCD and Jordan Riparian Buffer delineation and boundary (perennial & intermittent streams, note ephemeral streams on site)
- d) Proposed drainage and stormwater conditions
- e) Drainage conveyance system (piping)
- f) Roof drains
- g) Easements
- h) BMP plans, dimensions, details, and cross-sections
- i) Planting and stabilization plans and specifications



### Landscape Protection Plan

- a) Rare, specimen, and significant tree survey within 50 feet of construction area
- b) Rare and specimen tree critical root zones
- c) Rare and specimen trees proposed to be removed
- d) Certified arborist tree evaluation, if applicable
- e) Significant tree stand survey
- f) Clearing limit line
- g) Proposed tree protection /silt fence location
- h) Pre-construction/demolition conference note
- j) Landscape protection supervisor note
- k) Existing and proposed tree canopy calculations, if applicable

### Planting Plan

- a) Dimensioned and labeled perimeter landscape bufferyard
- b) Off-site buffer
- c) Landscape buffer and parking lot planting plan (including planting strip between parking and building, entryway planting, and 35% shading requirement)

### Steep Slope Plan

- a) Classify and quantify slopes 0-10%, 10-15%, 15-25% and 25% and greater
  - b) Show and quantify areas of disturbance in each slope category
  - c) Provide/show specialized site design and construction techniques
- N/A Entire Site is  
Less than 10%

### Grading and Erosion Control Plan

- a) Topography (2-foot contours)
- b) Limits of Disturbance
- c) Pertinent off-site drainage features
- d) Existing and proposed impervious surface tallies

### Streetscape Plan, if applicable

- a) Public right-of-way existing conditions plan
- b) Streetscape demolition plan
- c) Streetscape proposed improvement plan
- d) Streetscape proposed utility plan and details
- e) Streetscape proposed pavement/sidewalk details
- f) Streetscape proposed furnishing details
- g) Streetscape proposed lighting details



### **Solid Waste Plan**

- a) Preliminary Solid Waste Management Plan
- b) Existing and proposed dumpster pads
- c) Proposed dumpster pad layout design
- d) Proposed heavy duty pavement locations and pavement construction detail

### **Construction Management Plan**

- a) Construction trailer location
- b) Location of construction personnel parking and construction equipment parking
- c) Location and size of staging and materials storage area
- d) Description of emergency vehicle access to and around project site during construction
- e) Delivery truck routes shown or noted on plan sheets

### **Energy Management Plan**

- a) Description of how project will be 20% more energy efficient than ASHRAE Standards
- b) Description of utilization of sustainable forms of energy (Solar, Wind, Hydroelectric, and Biofuels)
- c) Participation in NC GreenPower program
- d) Description of how project will ensure indoor air quality, adequate access to natural lighting, and allow for proposed utilization of sustainable energy
- e) Description of how project will maintain commitment to energy efficiency and reduced carbon footprint over time
- f) Description of how the project's Transportation Management Plan will support efforts to reduce energy consumption as it affects the community

### **Exterior Elevations**

- a) An outline of each elevation of the building, including the finished grade line along the foundation (height of building measured from mean natural grade).

January 26, 2015

## **SUP Narrative 214 West Rosemary Street**

**AC Hotel by Marriott**  
**W. Rosemary & Church Streets**  
Chapel Hill, North Carolina

### **Introduction**

The AC Hotel by Marriott is a proposed single structure, 4-story hotel development with 123 rooms and two levels of below ground parking at the northeast corner of W. Rosemary Street and Church Street. The hotel's lobby area will include guest registration, meeting space, restaurant, and a bar. Outdoor spaces include a pool, adjacent terraces, and a fire pit lounge.

The AC Hotel is categorized by Marriott as member of the Lifestyle Collection. The AC Lifestyle is urban and modern. Hotel guests as well as the local community will sense the hotel is part of the community. The lobby spaces are relaxed, designed for a group of guests to gather before heading out to dinner. The outdoor fire pit is an example of where guests will gather before heading out.

The hotel's relaxed modern interior blends art and architecture. Advances in curatorial lighting are employed to emphasize the art and architectural elements. Technology is used discretely. The media library, similar to break out rooms, provides an intuitive multimedia system for small groups to meet, present, and share information.

The style and location of the AC Hotel blends with the Rosemary/Downtown Imagined master plan. The hotel's convenient location to Rosemary and Franklin Streets, and UNC's campus makes walking and alternative transportation accessible. The hotel is accessible to the community, and the community is accessible to the hotel guest.

Multi-modal transportation is a part of the hotel's sustainability effort. Guests will be able to park their cars during their stay and walk, or utilize available transit options, to reach their destinations. The hotel's design will look to minimize its environmental footprint. A Green roof system will help manage storm water run-off and solar heat gain. Domestic water consumption will be lessened with low flow showerheads and low flow toilets. The sanitary waste is reduced as well. Lighting, which is a significant power consumer, will be reduced with the latest in LED technology.

To develop the AC Hotel, the developer requires a minor alteration to the landscape buffer on the northern property line. A preliminary design of the alternative buffer is included in the Special Use Permit plans.

The AC Hotel is located in a transitional area between the urban town core of West Franklin and West Rosemary Streets and the Northside Neighborhood Conservation District. The property is currently zoned TC-2 and will not have to be rezoned for this project. The building design, stepping down from a 4-story façade along West Rosemary Street to a single story façade on the northern property line respects that transition from the urban to residential scale.





January 26, 2015

## **STATEMENT OF JUSTIFICATION**

**AC Hotel by Marriott  
W. Rosemary & Church Streets**  
Chapel Hill, North Carolina

### **Summary**

The AC Hotel by Marriott is a proposed 4-story hotel development with 123 rooms, and two levels of below ground parking proposed for the north east corner of W. Rosemary Street and Church Street. The hotel will be in one building, constructed on an assembly of three parcels of land. The 0.72 acre site is currently used as a restaurant, real estate business and rental housing.

The project aims to provide a unique, upscale, moderately priced hotel option in the downtown Chapel Hill core area. The hotel will include amenities such as a restaurant, bar, meeting spaces and an outdoor swimming pool. The restaurant and bar will only serve light fare so as not to compete with, and to encourage the use of, the existing downtown establishments.

The project is well served by existing mass transit, is located not far from West Franklin Street within the central business district of the Town. The walkable proximity to existing restaurants, retail, UNC campus and public services will reduce our guest's dependency on vehicular travel during their stay. It is anticipated that once the guests arrive, they will park their vehicle and walk or, utilize alternate modal means of transportation during their stay.

The site is zoned TC-2. The TC districts are, as stated in LUMO section 3.3.1, "*intended to provide for the development of the commercial, service and social center of Chapel Hill....*" Rezoning of this property is not required.

The concept and design of the AC Hotel by Marriott is in keeping with the 2020 Comprehensive Plan and the Rosemary/Downtown Imagined initiative and will take a significant step toward the Town of Chapel Hill's vision for the Downtown Future Focus Area as an amenity to the heart of downtown.

One modifications to a buffer requirement in the Land Use Management Ordinance (LUMO) is being requested for the development of the hotel. This modification is identified below, in the findings of fact, and evidence provided to support the request.

**The John R. McAdams  
Company, Inc.**

**Raleigh / Durham, NC**  
2905 Meridian Parkway  
Durham, North Carolina 27713  
(919) 361-5000

**Charlotte, NC**  
11301 Carmel Commons Blvd,  
Suite 111  
Charlotte North Carolina 28226  
(704) 527-0800

McAdamsCo.com

*Designing Tomorrow's Infrastructure & Communities*

Statement of Justification  
AC Hotel by Marriott  
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### **Special Use Permit – Required Findings of Fact**

***Finding #1*** – “*That the use or development is located, designed and proposed to be operated so as to maintain or promote the public health, safety, and general welfare.*”

#### **Project**

The AC Hotel by Marriott will provide visitor accommodations for business people, visiting professors, visiting families, and tourists with a high quality, yet moderately priced alternative to the current accommodations in the downtown core. Its proximity any number of restaurants, retail options, the UNC campus and other downtown services will support the local businesses. Furthermore, all of these options are within such close distances that walking will be promoted over vehicular use.

#### **Traffic**

Vehicular access into the site will be from Church Street into the visitor drop off area and to two levels of sub-surface structured under-building parking garage. Pedestrian access will be from both Church Street and W. Rosemary Street.

The Town’s Traffic Engineering consultant, HNTB, is currently generating a Traffic Impact Analysis. The transportation improvements identified by the study will be addressed by the project.

#### **Parking**

Parking is a concern in downtown. Therefore, the hotel will provide 112 automotive and 10 bicycle parking spaces. The off-street parking will be provided in two-levels of below grade structured parking. There will be limited impact from the hotel on downtown parking, as the guests are anticipated to arrive, park their vehicle, and then walk or use public transportation to reach their destinations.

#### **Recreation Opportunities**

Located in the TC-2 district, the hotel is required to provide a 0.12 ratio of active recreational space to gross land area (GLA). This requirement will be met through a combination of internal amenities such as a fitness facility and swimming pool and a payment-in-lieu. These amenities along with the walkable proximity to essential

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AC Hotel by Marriott  
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retail and public services will ideally reduce the dependency on vehicular travel for residents, reducing the long-term carbon footprint of the project.

#### Utilities and Town Services

- Water and sewer service will be provided by OWASA.
- Electricity will be provided by Duke Energy.
- Fire protection and emergency services will be provided by the Town.
- Solid waste and recycling will be provided by Waste Industries.

#### Stormwater Management

Stormwater management facilities will be provided to meet the requirements of the Town of Chapel Hill's ordinance. The slight increase in impervious surface area and nitrogen and phosphorous removal are being addressed through the use of green roofs. Areas on the second, third and fourth floor roofs have been identified for the potential installation of green roof systems. Final locations will be determined during the development of construction drawings.

#### Fiscal Impact

Property taxes for the project are anticipated to provide over \$200,000 in incremental property taxes.

Transient Occupancy Tax – The Town of Chapel Hill and Orange County are each project to receive over \$160,000 in annual, recurring transient occupancy tax revenues.

#### Employment

The hotel will provide job opportunities for local residents including an anticipated 100 temporary construction jobs and 20-25 permanent positions.

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**Finding #2** – *“That the use or development complies with all required regulations and standards of the Land Use Management Ordinance.”*

The AC Hotel by Marriott is designed to meet the standards set forth by the Town of Chapel Hill’s Land Use Management Ordinance.

The site is in a TC-2 zoning district and part of the Northside Neighborhood Conservation District. The proposed development is a transition zone between the more urban, town center and the Northside residential neighborhood. For this reason, the northern edge of the building holds to a one-story, 30-foot building height. The second, third and fourth floors step back at approximately at 1:1.7 slope, which is also greater than the required 1:1 step back ratio further limiting the impact of the four story building on the adjacent residential properties. The “U” shape form of the building also helps create privacy for the hotel guests as well as keeping all guest windows an appropriate distance from the residential properties. Finally, plantings on the roofs or facades of the building stepbacks will further soften the impact of the building on the adjacent residential properties.

Landscape Buffer / Building Setback

Requirements:

The northern boundary of the project is adjacent to R-3 designated properties. The LUMO requirement for a hotel use (Use Group B) adjacent to residential (Use Group A) is a 20’-Type C landscape buffer. Yet, the building setback for the TC-2 zoning district is zero.

Proposed:

In order to achieve the desired floor area, the building has been placed at 12-foot off the northern property line. This compromise between the zero setback and the 20-foot buffer will allow the project to achieve the desired ground floor building footprint while still providing a 12-foot alternative buffer which meets the opacity requirements of the LUMO.

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Justification:

The decorative wall will meet the requirements of Section 5.6.2 (d) of the LUMO which offers a variety of options to achieve the same or higher level of screening:

*“Screening required within buffers is intended to provide separation of spaces without necessarily eliminating visual contact between the spaces, and may consist of existing vegetation, planted vegetation, a landscaped earth berm, a decorative wall, a wood fence, or a combination of the above. Any of the options specified in the design manual for the required buffer type shall satisfy the buffer requirements of this section.”*

And Section 5.6.8:

*“In lieu of compliance with the above buffer yard and screening requirements, a developer may submit to the community design commission for its approval a detailed plan with specifications for landscaping and screening which will afford a degree of buffering and screening equivalent to or exceeding that provided.”*

A preliminary plan for the alternative buffer design is included in the documents. Additionally, the final design will be submitted to the CDC for review during the Zoning Compliance Permit approval process.

**Finding #3** – *“That the use or development is located, designed and proposed to be operated so as to maintain or enhance the value of contiguous property, or that the use or development is a public necessity.”*

The properties contiguous to the proposed hotel include non-residential or mixed use properties on all sides except the single family residential (rental) homes to the north of the project. In addition to the economic contribution to the Town, the AC Hotel by Marriott will increase the value of adjacent properties in the following ways:

- By bringing more people downtown, offering more business for the adjacent restaurants and shops

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- By following the vision of the established Comprehensive Plan, which was developed under the specified goals of protecting existing residences and preserving property values
- By providing an additional, moderately priced, hotel option.
- By reducing the incremental burden on the existing downtown traffic and parking supply created when visitors stay on the outskirts of Town, or in surrounding communities, and drive into the downtown area.
- By building momentum for the Rosemary/Downtown Imagined process through the re-development of an underutilized commercial core site.

***Finding #4*** – “*That the use or development conforms with the general plans for the physical development of the Town as embodied in the Land Use Management Ordinance and in the Comprehensive Plan.*”

The hotel is in conformance with the Town’s Land Use Management Ordinance, the 2020 Comprehensive plan – Downtown Focus Area, Rosemary Imagined and Downtown Imagined initiatives and the Northside Neighborhood Conservation District.

### **Compliance with the Comprehensive Plan**

#### **The Big Ideas**

1. *Implement a bikeable, walkable, green communities plan by 2020.*

The downtown location of the AC Hotel by Marriott will limit the need for vehicle use during guest stays, encourage walking to downtown destinations, provides dedicated bike storage, incorporates green roofs and incorporates LEED building methods.

2. *Create an entrepreneurial enterprise hub in the Rosemary Street corridor.*

The AC Hotel by Marriott will be a support facility for enterprises in the Rosemary Street corridor providing visitor accommodations and potential meeting spaces.

3. *Create entertainment/dining/arts hubs to capitalize on Chapel Hill’s strengths as a recreational destination.*

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The hotel will serve as a unique amenity for the Town of Chapel Hill, the hotel will draw visitors to the downtown and provide built in demand for the existing restaurant and retail businesses. Furthermore, the hotel will complement the existing uses and not compete against them.

4. *Increase the ratio of workforce housing by 2020 to develop a plan for student housing community.*

The hotel will not displace any existing residential uses.

5. *Establish a structure to support community and neighborhood engagement in a proactive manner.*

The project will engage in active community outreach throughout the entitlement approval and construction process including keeping the Northside neighborhood informed and involved and promoting local hiring through job fairs.

#### Six Overarching Themes

- *A place for everyone*

The project will create an additional, more affordable lodging option for visitors desiring to stay within the downtown core.

- *Community Prosperity & Engagement*

The project will increase the commercial tax base (property and transient occupancy taxes)

- *Getting Around*

The project is located within walking distance to the existing downtown core and transit options.

- *Good Places, New Spaces*

The project is an infill, redevelopment of an underutilized property.

- *Nurturing Our Community*

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Green roofs, LEED building practices, stormwater management, public art.

Chapel Hill has defined clear goals for the future, including a vibrant, diverse, pedestrian-friendly and accessible downtown with infill redevelopment opportunities. The AC Hotel by Marriott site, with its TC-2 zoning designation in place, has consistently been identified as a “potential development site” in the Rosemary Imagined, Downtown Imagined and the Downtown Development Framework & Action Plan.

The following statements, or themes, appear consistently, through-out the Town’s work associated with the downtown.

#### Rosemary / Downtown Imagined

- The district would attract and retain the intellectual capital of the University to provide high-quality jobs and bring additional vitality to the Rosemary Street and downtown areas.
- Encourage the redevelopment of the properties identified as potential redevelopment sites on the concept plan.
- Encourage infill development.
- Place a landscape buffer, such as trees bushes, or attractive fencing behind...the commercial properties on Rosemary Street.
- When new structures are built along Rosemary Street, incorporate setbacks and stepbacks that respect the adjacent residences.

#### 2020 Comprehensive Plan – Downtown Focus Area

- Connections to downtown, improved circulation and new amenities to support a vibrant economy downtown...
- Infill with transit and redevelopment opportunities
- Infill underutilized sites.
- Importance of parking.

#### Northside Neighborhood Conservation District

- Help preserve and protect the unique and distinctive older in-town residential neighborhoods or commercial districts...

The underlined portions of the statements point to the essence of each statement. The AC Hotel by Marriott as proposed provides an affirmative response for the desires expressed in each of these statements. It will be taking advantage of an identified development opportunity site with an infill





Statement of Justification  
AC Hotel by Marriott  
January 26, 2015  
Page 9 of 9

redevelopment project. The design respects the adjacent Northside neighborhood by providing the required landscape buffer and a building design that is less than the allowable primary building height and steps back at a greater relief slope than required. It will be an amenity for the downtown that will bring visitors in close, walkable, proximity to the downtown core contributing to the vitality of the downtown and Rosemary Street areas.



**AC by Marriott**

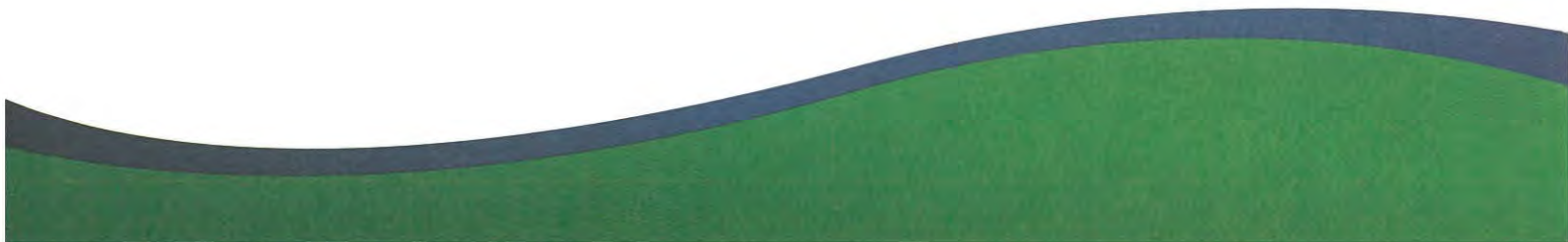
W Rosemary & Church St.  
Chapel Hill, NC  
January 23, 2015

To Whom It May Concern,

We are in receipt of the plans provided to us by Overcash Demmit for the AC by Marriott project in Chapel Hill, North Carolina detailing the proposed trash and recycling receptacle location and service requirements (plan marked A1.1). After review of the plans, Waste Industries is prepared to provide the required service for this location.

Regards,

Jason McMillan  
Sales Representative  
Waste Industries USA, Inc.





## New Market Waste Solutions

May 11, 2015

To Whom It May Concern:

New Market Waste Solutions has found a company to service the new hotel in Chapel Hill.

McJunk, Inc. is able to service the AC hotel by Marriott being proposed by OTO Development at 214 West Rosemary Street Chapel Hill, NC 27516. Michael McMorrow, one of the owners of McJunk has approved the services.

They plan to pick-up on a daily basis (Mon-Sat). There will most likely be a few roll carts for trash and recycling roll carts as well. (Please see attachments for details)

The truck size is an 18 cubic yard truck. Dimensions are 7 ½ feet wide by 12 ½ feet long by 12' Tall. The daily pickup would occur as follows – the McJunk truck would be able to back up into the garage where the Roll carts are stored and swap out the Roll Carts each day. By swapping out the carts, we will eliminate the possibility of spilling trash and causing a mess.

We also have a plan "B" and have identified a hauler who has a rear load truck and will be able to pull into the covered area as well and service the roll carts.

If you have any questions please don't hesitate to contact me.

Sincerely,

Robert M. Ryan

President

Cc: Dennis Mitchell  
Michael McMorrow

Attachments



January 26, 2015

## **LOCAL ART STATEMENT OF JUSTIFICATION**

**AC Hotel by Marriott**  
**W. Rosemary & Church Streets**  
Chapel Hill, North Carolina

### **Summary**

The AC Hotel by Marriott is a proposed 4-story hotel development with 123 rooms in the north east corner of W. Rosemary Street and Church Street. The project aims to provide a unique, upscale, hotel option in downtown Chapel Hill. The AC Hotel intends to complement the existing culture of the community and engage the guests to experience the culture

The Interior Designer will be challenged to incorporate local art in to the ground floor common area. Incorporating local art will offer a gateway for guests to explore Chapel Hill.

Sincerely,

Dennis Mitchell  
Development Manager  
OTO Development, LLC



January 26, 2015

## **ENERGY MANAGEMENT PLAN**

**AC Hotel by Marriott**  
**W. Rosemary & Church Streets**  
Chapel Hill, North Carolina

The AC Hotel will pursue USGBC LEED certification. The Energy & Atmosphere credit with energy modeling will be a point actively pursued. On past hotel projects, the Developer has achieved 10% improvements on ASHRAE energy efficiency standards. Heat pump technology used to heat and cool hotel guestrooms is the limiting factor preventing further gains. An energy management system will be installed in each guestroom. The energy management system is a combination of a motion sensor and door contact to determine when a room is occupied or unoccupied. Unoccupied rooms are allowed to drift further from the set point saving energy. Energy efficient lighting will be used throughout the hotel. Lighting is a combination of LED and compact florescent though LED is quickly replacing compact florescent bulbs of all types.

Sustainable energy will not be utilized on site. The hotel's roof will encompass approximately 10,000 sq ft of green roof for storm water management and solar heat gain mitigation. Mechanical equipment and maintenance access will consume the balance of roof space.

A contribution to NC Greenpower will be made to help offset the carbon footprint.

Indoor air quality will follow the USGBC LEED model. During construction ductwork will be sealed to prevent dust from settling in the duct. High efficiency filters will be used in the primary HVAC units. The Marriott brand requirements exceed ASHRAE and USGBC standards by requiring 100% outside makeup air to all guestrooms and corridors while achieving positive pressurization. Natural day lighting will utilize. Each guestroom has an exterior window and the ends of corridors will have windows. The ground floor lobby is designed around a large expanse of window walls. Windows are high efficiency with a low E coating.

The developer who is also the operator is committed to energy efficiency. An energy efficient building reduces the carbon foot print and reduces energy cost. A LEED point the developer typically pursues is continual monitoring of energy consumption and reporting to USGBC. By using this point as part of LEED certification, the developer is forcing the hotel to manage energy consumption.

An important point of the AC Hotel is its location as part of Franklin/Downtown Imagined. The hotel is accessible to UNC's campus and downtown. To encourage hotel guests to leave cars parked in the hotel garage, an interactive information board will be placed in the lobby. The board provides information on local attractions such as dining and direction for walking to a desired restaurant.

Sincerely,

Dennis Mitchell  
Development Manager  
OTO Development, LLC

**WEST ROSEMARY STREET HOTEL**

**TRAFFIC IMPACT STUDY**

***EXECUTIVE SUMMARY***



Prepared for:

The Town of Chapel Hill  
Public Works Department  
Traffic Engineering Division

***Prepared by:***

***HNTB North Carolina, PC***

*343 East Six Forks Road  
Suite 200  
Raleigh, NC 27609*

*NCBELS License #: C-1554*

March 2015

**HNTB**

# WEST ROSEMARY STREET HOTEL

## TRAFFIC IMPACT STUDY

### EXECUTIVE SUMMARY



#### Prepared for:

The Town of Chapel Hill  
Public Works Department  
Traffic Engineering Division

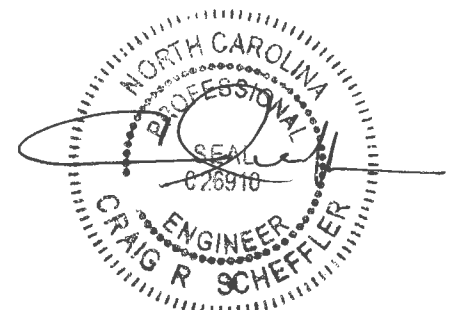
#### Prepared by:

**HNTB North Carolina, PC**

343 East Six Forks Road  
Suite 200  
Raleigh, NC 27609

NCBELS License #: C-1554

March 2015



3-9-15



## **EXECUTIVE SUMMARY - DRAFT**

### **Project Overview**

A commercial development known as the West Rosemary Street Hotel, located at the intersection of W. Rosemary Street and N. Church Street, is being proposed in Chapel Hill. The project proposes to construct a new four story 123-room hotel on three existing parcels along the north side of W. Rosemary Street. **Figure ES-1** shows the general location of the site. The project is anticipated to be fully complete by 2017. This report analyzes the complete build-out scenario for the year 2018 (one year after anticipated completion), the no-build scenario for 2018, as well as 2015 existing year traffic conditions.

The proposed site concept plan shows a single full movement access driveway along N. Church Street that would access an underground parking facility for hotel patrons, along with an adjacent drop-off driveway and a service driveway at the back of the property. No other vehicular access connections are proposed. **Figure ES-2** displays the preliminary concept plan of the West Rosemary Street Hotel and nearby land uses and roadways. The project is expected to provide 110 parking spaces in an on-site underground parking garage. This report analyzes and presents the transportation impacts that the West Rosemary Street Hotel will have on the following intersections in the project study area:

- NC 86 (N. Columbia Street) and Rosemary Street
- NC 86 (Columbia Street) and Franklin Street
- N. Church Street and W. Rosemary Street
- N. Church Street and W. Franklin Street / University Square Driveway
- N. Church Street and Proposed Site Driveway (parking garage access)

The impacts of the proposed site at the study area intersections will be evaluated during the AM, noon, and PM peak hours of an average weekday. The following study is based on background traffic for the existing year, 2015, the year following the estimated site build out year of 2017, as well as the estimated site-generated traffic produced by the proposed hotel, adjusted for the elimination of current site traffic produced by the existing land uses on the three parcels scheduled for redevelopment.

### **Existing Conditions**

#### **Study Area**

The site is located in downtown Chapel Hill along W. Rosemary Street in the northwest quadrant of its intersection with N. Church Street. The study area contains four signalized intersections along W. Rosemary Street and W. Franklin Street as they intersect N. Church Street and NC 86 (N. Columbia Street). NC 86 and Franklin Street are major arterial facilities providing connectivity between the UNC Main Campus/downtown area, Carrboro, and the region. Remaining study area network roadways are either collector streets or local neighborhood access streets.

#### **Site Traffic Generation**

With the addition of new “net” peak hour trips during the weekday AM, noon, and PM peak hours, there are potential site traffic impacts to the study area intersections. **Table ES-1** shows the site trip generation details, with generation rates and methodologies taken from the *Institute*





of Transportation Engineers (ITE) Trip Generation Manual, Version 9 and adjusted, as appropriate, for the estimated existing traffic volumes from the existing commercial development that the hotel will replace, as well as the effects of transit/pedestrians/bicycles.

**Table ES-1**  
**Weekday Vehicle Trip Generation Summary - West Rosemary Street Hotel**

Trip Generation Statistic	Daily			AM Peak Hour			Noon Peak Hour*			PM Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
New Site Raw Totals	503	503	1,006	38	27	65	38	32	70	38	36	74
Existing Development Traffic Volumes	262	262	524	7	2	9	25	22	47	23	21	44
<b>Net Site Traffic Added to the Study Area</b>	<b>241</b>	<b>241</b>	<b>482</b>	<b>31</b>	<b>25</b>	<b>56</b>	<b>13</b>	<b>10</b>	<b>23</b>	<b>15</b>	<b>15</b>	<b>30</b>

\* - No ITE Data Available, Assumed Average of AM/PM Peak Hour Data

**Background Traffic**

Background traffic growth for the 2018 analysis year is expected to come from two sources - ambient regional traffic growth and specific development-related traffic growth. Based on existing information, three Town-approved development projects (The Lux, 123 West Franklin, Carolina North – Phase 1) near the project study area is planned to be contribute to background traffic growth by the 2018 analysis year. All remaining estimated traffic growth in the area is assumed to occur due to overall region-wide ambient growth. To account for this, an ambient area-wide traffic growth percentage of 1.0 percent per year was applied to existing traffic volumes based on information from the historic daily traffic growth patterns in the project study (NCDOT and Town of Chapel Hill daily traffic information) and consistent with recent traffic impact studies completed near the project study area.

**Impact Analysis**

**Peak Hour Intersection Level-of-Service (LOS)**

Study results indicate existing traffic operations at all study area intersections are acceptable during the AM and noon weekday peak hours, but drop to a LOS E for two study area intersections in the PM peak hour. 2018 build-out year+1 background traffic growth impacts are mitigated by recommended signal retimings (from other recent development traffic impact studies) throughout the project study area, and it is anticipated that overall intersection performance should improve at the two intersections that exhibit existing PM peak hour operational issues. The addition of peak hour site-generated trips to the projected 2018 background traffic volumes, do not cause study area intersections to experience deficient traffic operations in any peak hour. A summary of the traffic operations for each intersection, related to vehicular delays (intersection average as a whole if signalized, critical movement if stop-controlled) and the corresponding Level-of-Service (LOS) is shown in **Table ES-2**.



**Table ES-2. West Rosemary Street Hotel  
 LOS and Delay (Seconds/Vehicle) Summary**

Intersections	Peak Hour	2015 Existing		2018 No-Build		2018 Build		2018 Mitigated	
		LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
W. Rosemary Street & N. Church Street	AM	A	8.1	A	8.6	B	10.2	N/A	N/A
	NOON	B	13.5	B	13.0	B	13.4	N/A	N/A
	PM	B	15.5	B	15.7	B	17.0	N/A	N/A
W. Rosemary Street & NC 86 (N. Columbia Street)	AM	C	31.1	C	25.7	C	25.8	N/A	N/A
	NOON	D	37.3	C	26.6	C	26.7	N/A	N/A
	PM	<b>E</b>	<b>63.0</b>	C	33.6	C	33.7	N/A	N/A
Franklin Street & NC 86 (Columbia Street)	AM	D	41.3	D	39.6	D	39.8	N/A	N/A
	NOON	D	50.1	D	47.5	D	47.5	N/A	N/A
	PM	<b>E</b>	<b>78.2</b>	D	53.6	D	53.9	N/A	N/A
W. Franklin Street & N. Church Street / 123 W. Franklin Site Dr	AM	A	5.4	A	8.1	A	8.9	N/A	N/A
	NOON	A	9.1	B	10.5	B	11.2	N/A	N/A
	PM	A	9.7	B	10.6	B	10.7	N/A	N/A
N. Church Street & Proposed Site Driveway	AM	N/A	N/A	N/A	N/A	A*	9.2*	N/A	N/A
	NOON	N/A	N/A	N/A	N/A	A*	9.5*	N/A	N/A
	PM	N/A	N/A	N/A	N/A	B*	10.3*	N/A	N/A

N/A – Not Applicable or No Improvements Necessary

**BOLD/ITALICS** – Critical Movement or Overall Intersection Requires Mitigation Per Town TIS Guidelines

\* - Worst-Case LOS/Delay for Unsignalized/Stop-Controlled Critical Movement

**Access Analysis**

Vehicular site access is to be accommodated via a proposed two-way site driveway connection to N. Church Street that will directly access the proposed hotel’s structured parking facilities. Driveway throat length as shown on the proposed site development plans (90 feet) is adequate for projected 2018 traffic conditions. Based on capacity analysis results, there is little chance of substantial queuing occurring within the two-way aisle. There is an additional driveway connection for inbound drop-offs that is proposed, along with a service access driveway at the back of the site parcel – both connecting to N. Church Street.

Driveway distances from the signalized intersection at W. Rosemary Street and N. Church Street are acceptable (110 feet), based on recommendations of 100 foot minimum corner clearance as set forth in the 2003 *NC DOT Policy on Street and Driveway Access to North Carolina Highways* and the 2005 *Town of Chapel Hill Design Manual*.

Access for pedestrians and bicyclists is currently excellent, with continuous sidewalk on both sides of all study area roadways and crosswalk/pedestrian signalization at all signalized study area intersections. Bicycle access is well planned to and from the site (which will have dedicated bicycle storage spaces in the parking area). Wider travel lanes on several study area roadways, including W. Rosemary Street adjacent to the proposed site, are also in existence.



**Crash Analysis**

Data from the NCDOT Traffic Safety Unit was requested for the recent five-year period for all existing major study area intersections and the W. Rosemary Street corridor. Results show that the Rosemary Street corridor in the vicinity of the site experienced 35 crashes in the last five years, though most were located near the NC 86 (N. Columbia Street) intersection. Of the four project study area intersections, the Franklin Street/Columbia Street intersection experiences the most crashes and highest crash rates.

**Other Transportation-Related Analyses**

Other transportation-related analyses relevant to the 2001 Town of Chapel Hill Guidelines for the preparation of Traffic Impact Studies were completed as appropriate. The following topics listed in **Table ES-3** are germane to the scope of this study.

**Table ES-3. Other Transportation-Related Analyses**

Analysis	Comment
Long-Range Planning Level Daily Volume-Capacity Analysis	Since the proposed site will add less than 500 new “net” daily trips to the study area network, no long-range planning-level analyses of daily traffic impacts were conducted for this study.
Signal Phasing Analysis	Signal phasing changes (potentially changing the phase order for left-turn signal phases to lead or lag depending on the time of day) may enhance traffic operation for 2018 future conditions at the signalized intersections along N. Columbia Street.
Progression Analysis	All current signalized intersections in the project study area feature weekday peak hour coordinated operations. Adjustments to existing signal timings to improve progressed arterial movements along both Franklin Street and N. Columbia Street may enhance traffic progression in 2018 future scenarios.
Turn Lane Storage Requirements	Storage bay lengths at study area intersections were analyzed using Synchro and HCS 95 <sup>th</sup> percentile (max) queue length estimates for the 2018 Build Scenario. In most cases, existing storage for turn lanes is adequate in the project study area, and can be managed with signal timing adjustments. The eastbound left-turn at the NC 86 (N. Columbia Street) intersection with Rosemary street is currently only 100 feet – but 2018 PM peak hour conditions suggest it may require up to 250 feet. Restriping of this approach to provide that distance should be feasible with the existing roadway cross-section along Rosemary Street.
Appropriateness of Acceleration/Deceleration Lanes	Given the existing location and configuration of site driveway, and the lane geometrics, traffic patterns and posted speeds on N. Church Street, no special acceleration or deceleration lanes are required due to the proposed W. Rosemary Street Hotel development.
Pedestrian and Bicycle Analysis	Existing pedestrian access and connectivity is excellent through the study area. No specific bicycle amenities exist in the study area, although the existing width of Rosemary and Franklin Streets is conducive to bicycling activities.
Public Transportation Analysis	Public transportation service to the site is excellent, with an on-street bus stops located less than 200 feet to the east and west of the site and multiple routes serving the study area.



## **Mitigation Measures/Recommendations**

### **Planned Improvements**

There are no Town of Chapel Hill or North Carolina Department of Transportation improvement projects for study area roadway facilities within the analysis year time frame of 2015-2018.

### **Background Committed Improvements**

The 123 West Franklin project is expected to provide geometric and signal timing operational improvements to the W. Franklin Street/N. Church Street intersection (see **Figure ES-3**). Several other recent traffic impact studies for developments in or near the downtown area have recommended corridor signal retiming and/or geometric improvements for other study area intersections. For the purposes of this study, only the optimized signal timing improvements are expected to be completed in the 2015-2018 time frame.

### **Applicant Committed Improvements**

Based on the preliminary site concept plans and supporting development information provided, there are no external transportation-related improvements proposed adjacent to the West Rosemary Street Hotel – other than the removal of two existing access driveways along the W. Rosemary Street frontage.

### **Necessary Improvements**

Based on the 2018 design year peak hour intersection capacity analyses, none of the study area intersections are expected to be over capacity (overall LOS E or F) in any of the three weekday peak hours studied in this report. Two intersections, W. Rosemary St/N. Columbia St and Franklin St/Columbia St, are currently operating at a LOS E in the 2015 PM peak hour, due to high existing traffic volumes and some approaches and existing coordinated signal timings.

With the recommended retiming of the study area intersections as a potential improvement listed in other recent traffic impact studies for nearby study area developments (the Lux, 123 West Franklin, Carolina North Phase 1), overall delays likely to be experienced in the 2018 design year are projected to be less than the current 2015 peak hour analysis results.




- Retiming of the four study area traffic signals by the 2018 build-out year to optimize traffic progression, and to investigate the usage of lead/lag left-turn phasing where appropriate, should be implemented whether or not the West Rosemary Street Hotel is constructed and was considered to be implemented for the 2018 analysis year to compare whether or not additional mitigation due to West Rosemary Street Hotel site traffic was needed.

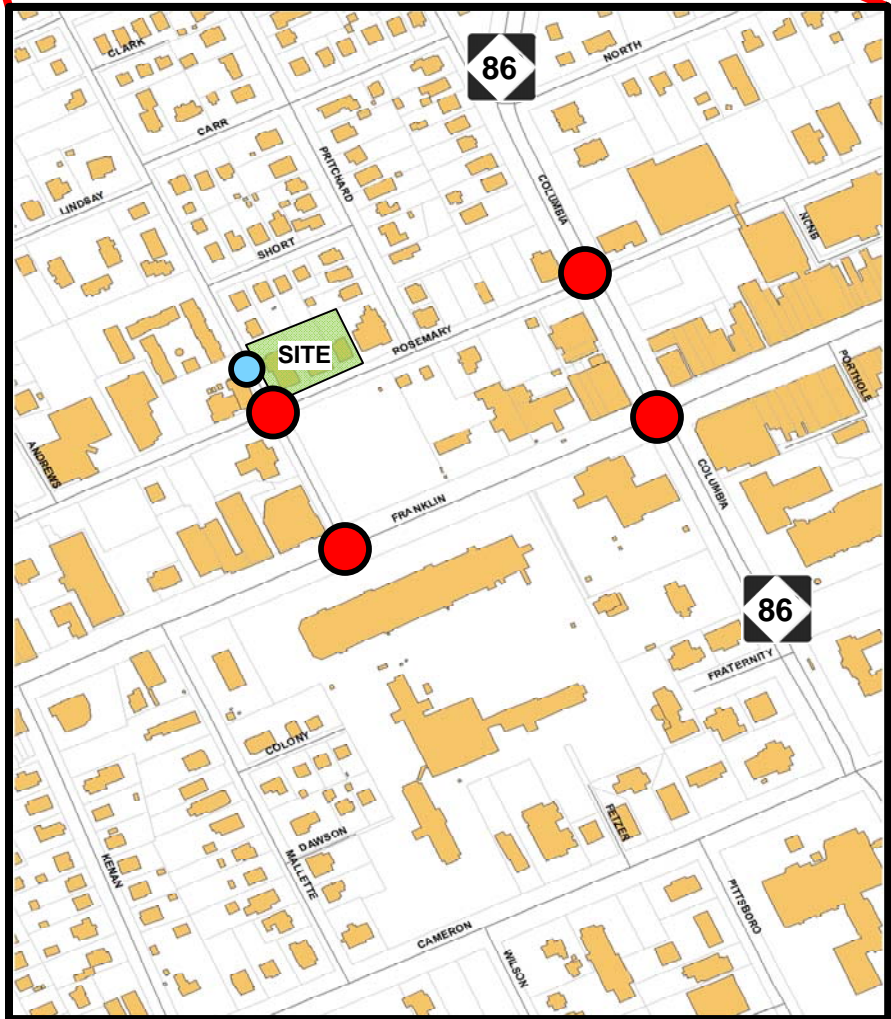
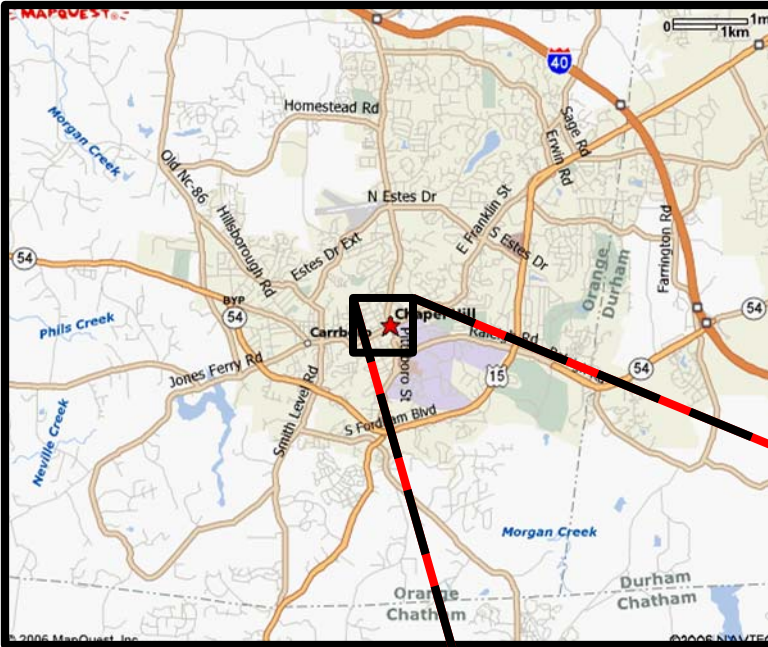
One recommended improvement, unrelated to intersection capacity analyses, is to lengthen the eastbound left-turn bay, via pavement marking upgrades, at the intersection of Rosemary Street and N. Columbia Street. This improvement would be necessary with or without site-related traffic from the West Rosemary Street Hotel development. Estimated 95<sup>th</sup> percentile queue lengths for this movement are up to 250 feet (PM peak hour) in the 2018 – With Site Traffic scenario. Field observation and review of existing aerial imagery of the W. Rosemary Street corridor indicate that adequate pavement width exists beyond the existing 100 foot eastbound left-turn bay to adjust pavement markings without geometric widening. This improvement would improve the overall efficiency of the intersection and is shown in **Figure ES-3**.

- This improvement is recommended whether or not the West Rosemary Street Hotel project is constructed.



**LEGEND**

-  = Existing Study Area Intersections
-  = Proposed Site Driveway
-  = Existing Building Footprints

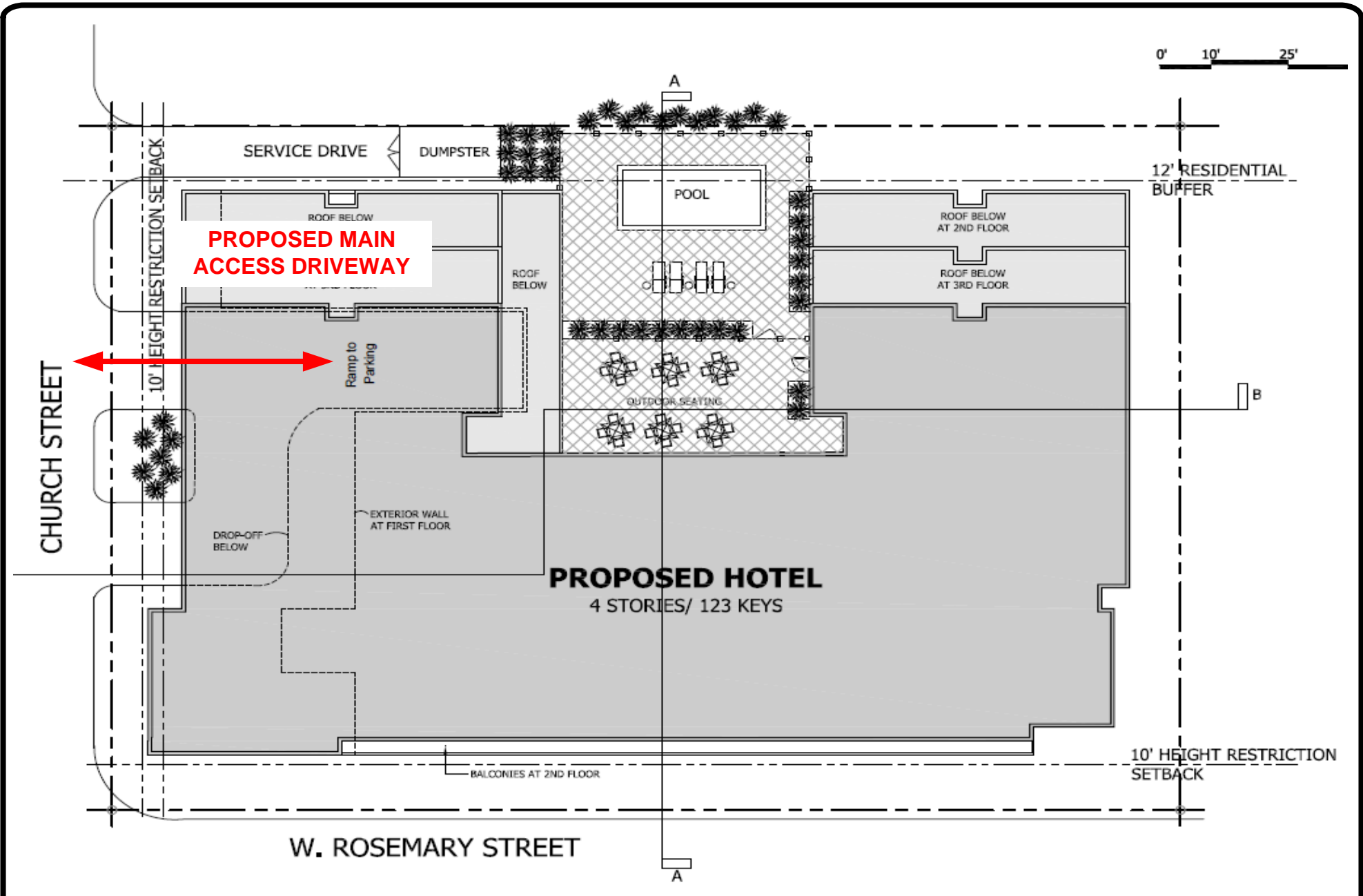


**West Rosemary Street Hotel  
Traffic Impact Study**

SITE LOCATION MAP

DATE: March 2015

**FIGURE ES-1**



**HNTB**



**West Rosemary Street Hotel  
Traffic Impact Study**

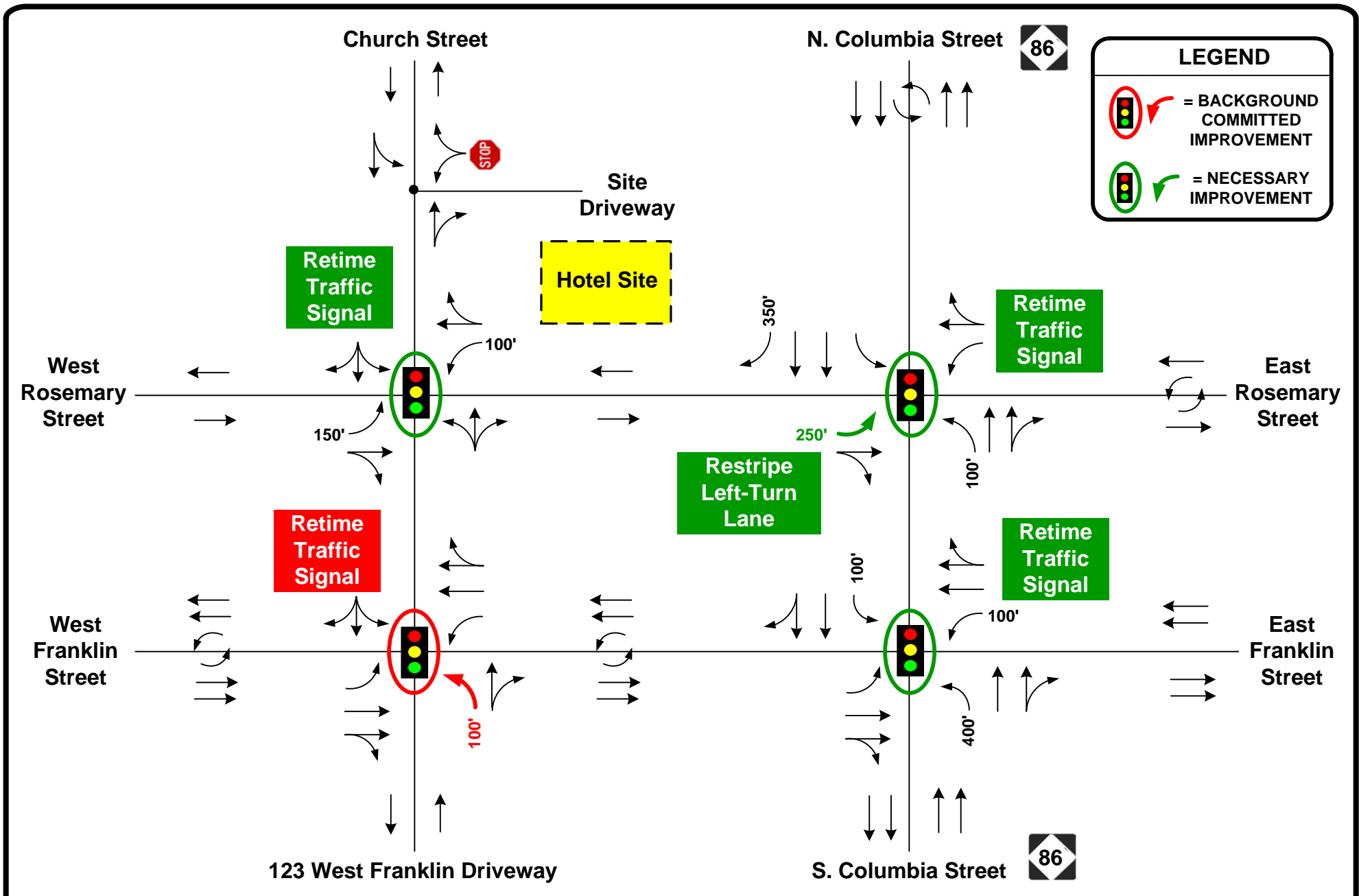
PRELIMINARY SITE PLAN



**NOT  
TO  
SCALE**

DATE: March 2015

**FIGURE ES-2**



**HNTB**



**West Rosemary Street Hotel  
Traffic Impact Study**

RECOMMENDED IMPROVEMENTS



NOT  
TO  
SCALE

DATE: March 2015

**FIGURE ES-3**



**PUBLIC WORKS DEPARTMENT  
STORMWATER MANAGEMENT DIVISION**

405 Martin Luther King, Jr. Blvd.  
Chapel Hill, NC 27514-5705  
Telephone (919) 969-7246  
Fax (919) 969-7276  
www.townofchapelhill.org

## STREAM DETERMINATION RECORDS REVIEW

Property Information	
Parcel ID Number (PIN)	Address / Location Description
9788-27-5353	220 and 222 West Rosemary Street
9788-27-6326	214 West Rosemary Street
9788-27-6387	210 and 212 West Rosemary Street

After reviewing Town GIS information, USGS 1:24,000 Topographic maps, and County Soil Survey maps, I have determined no new stream determination will be required for the property(ies) listed above for the following reason(s):

- No unclassified streams or waterbodies, streams or waterbodies identified as requiring a new classification or determination, or unidentified flowlines (possible streams) are shown within 150 feet of the property in question on the Town's GIS, the USGS 1:24,000 Topographic map, or the County Soil Survey map for the area.
- A Resource Conservation District boundary was set on a recorded final plat for the property in question, and there are no streams or waterbodies shown on the USGS 1:24,000 Topographic map or County Soil Survey within 150 feet of the property.
- A stream determination has been done for this property, a property uphill or upstream, or a nearby property as of January 1, 2004 or later, and that stream determination applies to this property. A copy of the documentation for the relevant site visit(s) is attached.

Relevant PIN(s):

**A map showing water features, their Town flow classifications, presence of Jordan Riparian Buffers, and their approximate locations is attached. Origins or breakpoints that have been flagged in the field are marked on the map.**

Other conditions exist which may affect the location of the Resource Conservation District or Jordan Stream Buffer:

- FEMA floodzone is mapped in the area. Precise location of the Base Flood Elevation and associated Resource Conservation District must be determined by a field survey commissioned by the owner or a representative.
- Segments of perennial or intermittent stream are piped in the area. These segments do not have an associated Jordan Stream Buffer.
- Possible Jurisdictional Wetlands have been identified in the area. A formal review by a professional certified in Jurisdictional Wetland Delineation is recommended.

*Patricia D'Arconte*

Digitally signed by Patricia D'Arconte  
Date: 2015.01.27 11:31:57 -05'00'



















01/27/2015

Town Staff signature

date



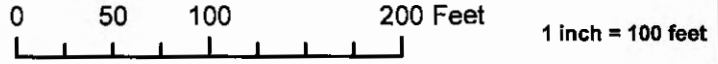
# Stream Determination Area Map

- |   |                     |   |                           |
|---|---------------------|---|---------------------------|
|  | Unclassified Stream |  | Non-regulated Waterbody   |
|  | Ephemeral Stream    |  | Non-perennial Waterbody   |
|  | Intermittent Stream |  | Wide Perennial Stream     |
|  | Perennial Stream    |  | Perennial Waterbody       |
|  | Culverts            |  | Approximate Jordan Buffer |
|  | 2-foot Contours     |  | Ephemeral Breakpoint      |
|  | 10-foot Contours    |  | Intermittent Breakpoint   |
|  | Buildings           |  | Perennial Breakpoint      |
|  | Parcels             |   |                           |
|  | Site visited        |   |                           |

**Address:** 210, 212, 214, 220, 222  
West Rosemary Street

**Parcel ID:** 9788-27-6387, 9788-27-6326,  
9788-27-5353

**TMBL:** 7.85.J.9, 7.85.J.10, 7.85.J.11



Stream locations are approximate and must be verified by survey.





# USGS 24K Topographic / County Soil Survey Maps

 Site Parcel Boundary

**Address:** 210, 212, 214, 220, 222 West Rosemary Street

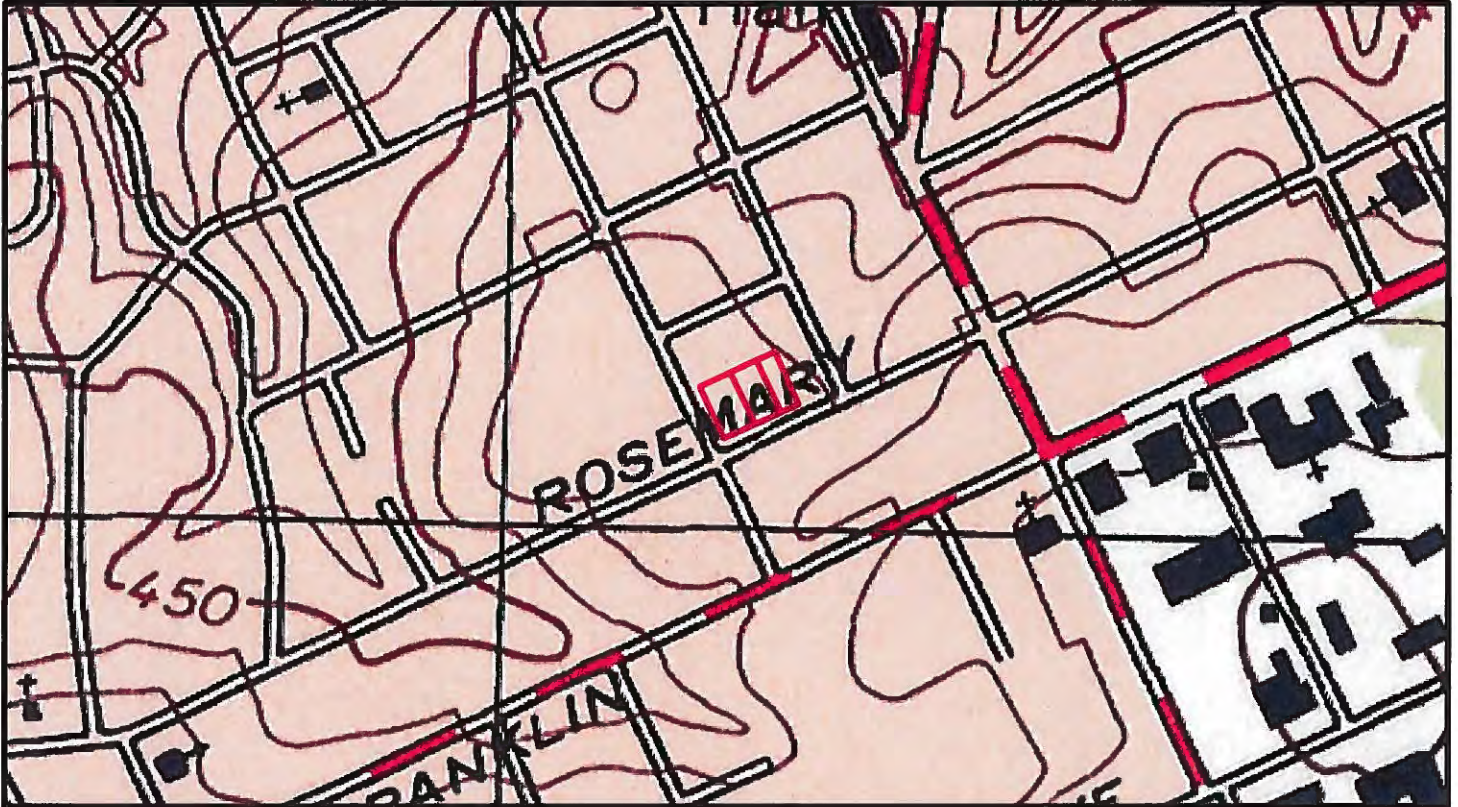


0 150 300 450 600 Feet

**Parcel ID:** 9788-27-6387, 9788-27-6326, 9788-27-5353

1 Inch = 500 feet

*Created by Town of Chapel Hill Public Works Department - Stormwater Management Division- 1/27/2015*







## **Mitigation Measures/Recommendations**

### **Planned Improvements**

There are no Town of Chapel Hill or North Carolina Department of Transportation improvement projects for study area roadway facilities within the analysis year time frame of 2015-2018.

### **Background Committed Improvements**

The 123 West Franklin project is expected to provide geometric and signal timing operational improvements to the W. Franklin Street/N. Church Street intersection (see **Figure ES-3**). Several other recent traffic impact studies for developments in or near the downtown area have recommended corridor signal retiming and/or geometric improvements for other study area intersections. For the purposes of this study, only the optimized signal timing improvements are expected to be completed in the 2015-2018 time frame.

### **Applicant Committed Improvements**

Based on the preliminary site concept plans and supporting development information provided, there are no external transportation-related improvements proposed adjacent to the West Rosemary Street Hotel – other than the removal of two existing access driveways along the W. Rosemary Street frontage.

### **Necessary Improvements**

Based on the 2018 design year peak hour intersection capacity analyses, none of the study area intersections are expected to be over capacity (overall LOS E or F) in any of the three weekday peak hours studied in this report. Two intersections, W. Rosemary St/N. Columbia St and Franklin St/Columbia St, are currently operating at a LOS E in the 2015 PM peak hour, due to high existing traffic volumes at some approaches and existing coordinated signal timings.




With the recommended retiming of the study area intersections as a potential improvement listed in other recent traffic impact studies for nearby study area developments (the Lux, 123 West Franklin, Carolina North Phase 1), overall delays likely to be experienced in the 2018 design year are projected to be less than the current 2015 peak hour analysis results.

- Retiming of the four study area traffic signals by the 2018 build-out year to optimize traffic progression, and to investigate the usage of lead/lag left-turn phasing where appropriate, should be implemented whether or not the West Rosemary Street Hotel is constructed and was considered to be implemented for the 2018 analysis year to compare whether or not additional mitigation due to West Rosemary Street Hotel site traffic was needed.

One recommended improvement, unrelated to intersection capacity analyses, is to lengthen the eastbound left-turn bay, via pavement marking upgrades, at the intersection of Rosemary Street and N. Columbia Street. This improvement would be necessary with or without site-related traffic from the West Rosemary Street Hotel development. Estimated 95<sup>th</sup> percentile queue lengths for this movement are up to 250 feet (PM peak hour) in the 2018 – With Site Traffic scenario. Field observation and review of existing aerial imagery of the W. Rosemary Street corridor indicate that adequate pavement width exists beyond the existing 100 foot eastbound left-turn bay to adjust pavement markings without geometric widening. This improvement would improve the overall efficiency of the intersection and is shown in **Figure ES-3**.

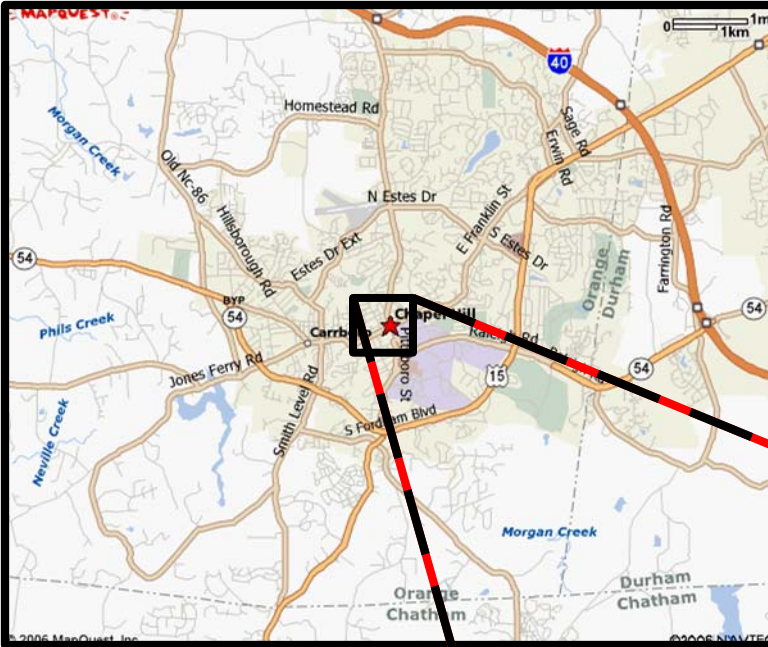
- This improvement is recommended whether or not the West Rosemary Street Hotel project is constructed.

**LEGEND**

-  = Existing Study Area Intersections
-  = Proposed Site Driveway
-  = Existing Building Footprints



**NOT  
TO  
SCALE**

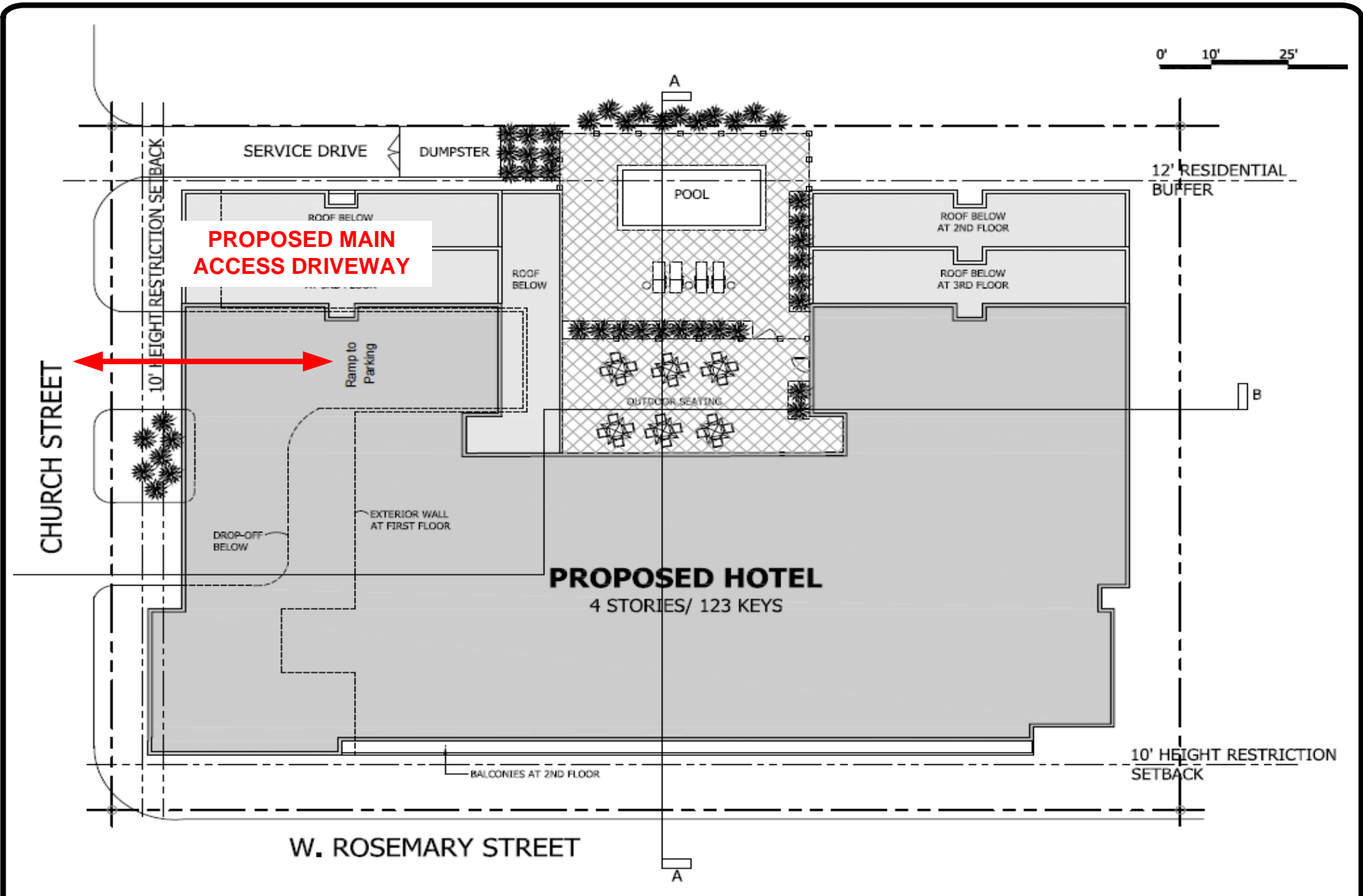


**West Rosemary Street Hotel  
Traffic Impact Study**

SITE LOCATION MAP

DATE: February 2015

**FIGURE ES-1**



**HNTB**



**West Rosemary Street Hotel  
Traffic Impact Study**

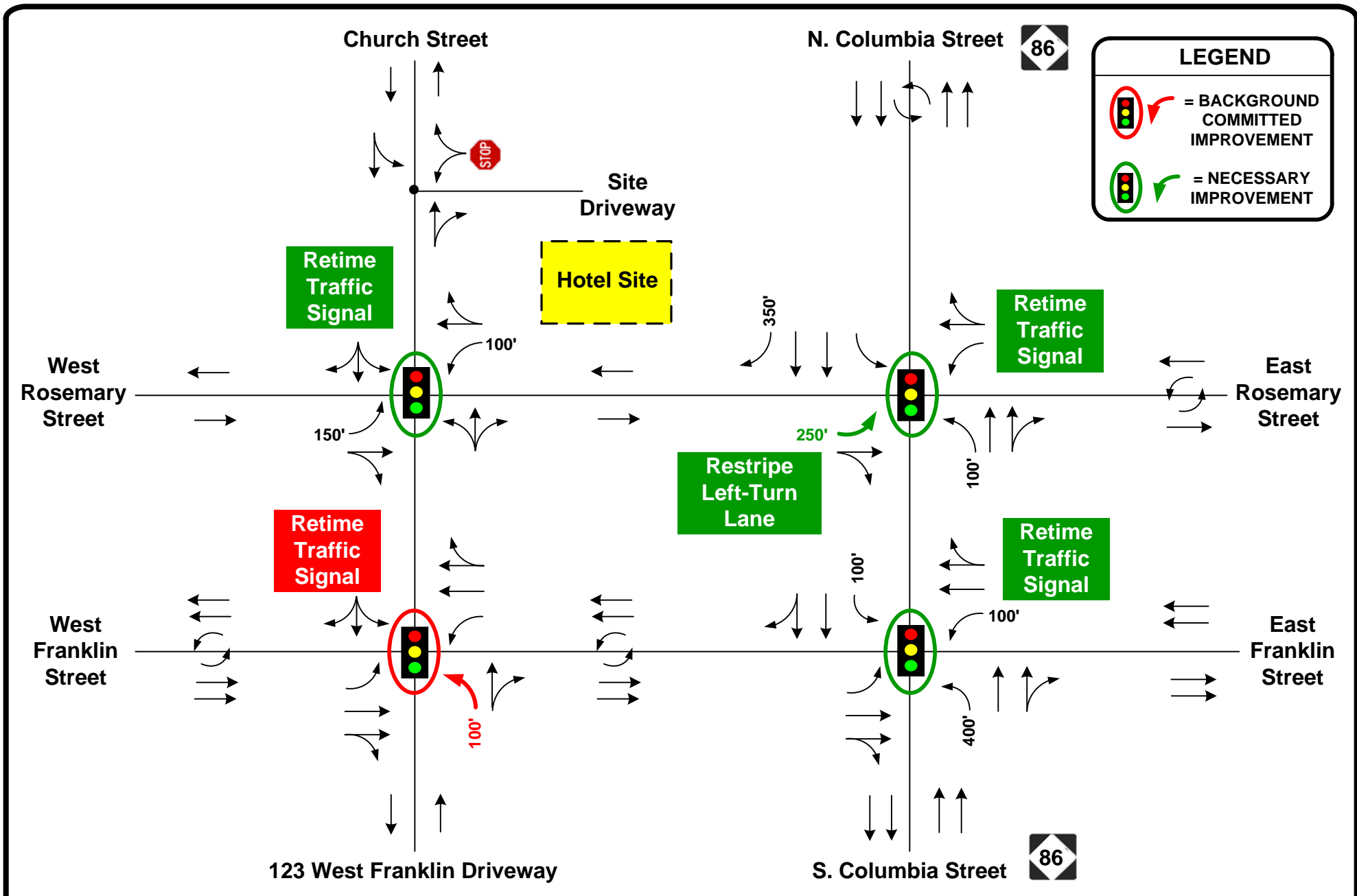
PRELIMINARY SITE PLAN



**NOT  
TO  
SCALE**

DATE: February 2015

**FIGURE ES-2**



**LEGEND**

- = BACKGROUND COMMITTED IMPROVEMENT
- = NECESSARY IMPROVEMENT

**HNTB**



**West Rosemary Street Hotel  
Traffic Impact Study**

RECOMMENDED IMPROVEMENTS



NOT  
TO  
SCALE

DATE: February 2015

**FIGURE ES-3**

# BOLAN SMART ASSOCIATES, INC.

1150 K STREET, NW, SUITE 1211, WASHINGTON, DC 20005 • (202) 371-1333

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March 2015

**RE: Fiscal Impact Analysis** (*per Special Use Permit Application Submittal Requirements*)  
**AC by Marriott, Chapel Hill, NC**

Bolan Smart Associates (BSA) has been asked to provide an overview of the fiscal impact to the Town of Chapel Hill (ToCH) associated with the development of a 131,000 gsf, 123 room, 112 parking space hotel. The on-site parking, food & beverage and other non-room spaces are all intended primarily to support hotel guest use. Construction is anticipated to take 18 months beginning in 2016. The building program and hotel operating assumptions have been provided by the project sponsor, OTO Development. The estimated municipal revenues and costs are based on assumptions prepared by Bolan Smart Associates, compiled from a combination of review of other recent ToCH fiscal impact analysis, discussion with Town staff, and BSA's background with fiscal impact analysis in other jurisdictions. Other than for a Town requested \$3,000 cost to reprogram traffic signals, it is assumed there are no upfront Town capital costs required for infrastructure or other site development support.

**A. ToCH Direct Annual General Fund Revenues (\$2015)** The principal direct general fund tax revenues to the Town of Chapel Hill resulting from the completion of this project total approximately \$242,000 per year (calculated in \$2015). The primary components, as summarized in Table 1, are estimated to include:

- 1) \$74,400 per year in real and personal property taxes;
- 2) \$165,000 in hotel use related taxes, derived from \$4,882,300 in room revenues and \$673,400 in limited food & beverage service and other revenues;
- 3) \$1,685 in parking taxes from a 112 space parking component; and
- 4) \$800 per year in hotel and retail employment related sales tax revenues, attributable to \$115,100 in employee generated annual taxable sales (\$45 per week in retail expenditures per employee).

Note that in addition to the on-site project direct revenues there are indirect Town of Chapel Hill tax revenues generated by virtue of the additional hotel guest visits to downtown, the primary component derived from hotel guest off-site retail sales.

**B. Other ToCH Property Tax Revenues (\$2015)** (See Table 1)

Transit Fund	\$8,100
Debt Service Contribution	\$14,700
Downtown Service District	<u>\$14,000</u>
Total Other ToCH	\$36,800

**C. One-time Development Related Revenues** The combination of permits, tap fees, inspections, recordation fees and other development and initial occupancy municipal costs are budgeted at approximately \$825,000. This is at the lower end of the \$5.00 to \$10.00 per gsf of building area that is typically part of the development budget for a major, urban form commercial development. Assuming 85% of these one-time development costs are retained entirely by the Town of Chapel Hill, the net revenue approximates \$700,000. In addition, \$48,350 in estimated sales tax collected on construction materials are projected, producing a total one-time tax project direct revenue ToCH impact on the order of \$750,000.

**One-time Fees (2015 to 2017)**

permits, tap fees, inspections, recordation fees, etc. \$700,000 ToCH (85% of \$825,000 total)

**Construction Materials Sales Tax Revenue**

\$16,000,000 construction cost x 55% materials cost x 80% locally sourced taxed at 0.6870% TOCH sales tax rate, realizing \$48,350 in one-time construction materials sales tax.

**D. Overlapping Jurisdictions Direct Annual Tax Revenue (\$2015)** (See Table 1)

Orange County Real and Personal Tax Revenue	\$172,800
School District Real and Personal Tax Revenue	\$41,000
Orange County Occupancy Tax	\$146,500
Orange County Other Hotel Guest Sales Tax	\$11,200
Orange County Parking Revenue Tax	\$4,100
Orange County Employee Related Sales Tax	<u>\$1,900</u>
Other Non-Chapel Hill Tax and Fee Revenues	not considered
Total Other Municipal Related Annual Taxes	\$377,500

**E. Municipal Costs (\$2015)** The ToCH Special Use Permit (SUP) Application Submittal Requirements asks for a breakdown of the costs for providing Town services. Bolan Smart has reviewed two prior fiscal impact reports prepared in 2014 for The Edge and Obey Creek. These other examples are for large and complicated mixed use developments in different settings. Respectful of the many differences between the subject application and the other precedents, Bolan Smart has attempted to craft a cost allocation methodology specific to a new downtown hotel.

To meet the SUP Submittal requirements, two sets of municipal cost assumptions need to be established. One is to categorize the likely cost impacts, and the other is to assign some kind of monetary cost metrics.



Cost Category Impacts: The following provides an illustration of likely incremental impact on Town service costs compared with other types of land uses found in Chapel Hill. The key premise is to evaluate to what degree the existing civic infrastructure and provision of services will require further municipal expense in order to maintain current overall levels of performance.

<u>Service Area</u>	<u>Incremental Cost Impact</u>
Solid waste collection	<i>none, hotel contracted</i>
Street maintenance	<i>limited, no added impact beyond immediate streetfront. (Note project will fund adjacent sidewalk improvements.)</i>
Stormwater	<i>limited, on-site retention system</i>
Parks & Recreation	<i>minor</i>
Library Services	<i>minimal</i>
Police Services	<i>some, offset partly by special event on-site funded security</i>
Fire Services	<i>some</i>
Planning/Inspections	<i>yes</i>
General Government	<i>some</i>
Public Transportation/Transit	<i>some (assumed at \$4,000, or 50% of Transit Fund Revenue)</i>
Debt Service	<i>very limited (assumed at 20% of Debt Service Revenue)</i>
Downtown Service District	<i>some (assumed at 100% of Service District Revenue)</i>

Municipal Cost Source: Given the imprecision of allocating costs from a town-wide budget for the above service items, the following approach for assessing a per unit cost for a hotel is suggested. The \$58.3 million Town General Fund supported costs are more or less allocated in the following bundles:

<u>Town Data Point</u>	<u>General Fund Budget Assumption</u>	<u>Implied Cost per Unit</u>
60,000 Residents	<i>65%, or \$37.9 million</i>	<i>\$630 per resident</i>
30,000 Jobs & University	<i>35%, or \$20.4 million</i>	<i>variable measure</i>

Hotel Cost Equivalent: Assuming that one year of hotel nights represents 100% the cost of a Chapel Hill resident means that one annualized hotel room night costs \$630 in overall unallocated Town services. With a total number of annualized room nights of 92 (123 rooms at 75% occupancy), this suggests there would be an annual Town general fund cost of \$58,000 for the proposed hotel.

**F. ToCH Net Fiscal Impact (\$2015)**

General Fund Hotel Revenue	\$242,000
General Fund Cost	<u>\$58,000</u> (per above)
Net Impact	\$184,000

The above estimate is grounded in a formula that employs cost averaging across the spectrum of existing municipal servicing expenses. Were an incremental approach to be tracked, where the added service demand is on top of the existing level of service, it is likely that the marginal cost of accommodating the actual extra requirements imposed by a new hotel would be much less than were the costs to be reallocating on based on cost averaging.

Transit Fund Revenue	\$8,000
Transit Fund Cost (50% of revenue)	<u>\$4,000</u> (Bolan Smart assumption)
Net Impact	\$4,000

Debt Service Revenue	\$14,800
Additional Debt Service (20% of revenue)	<u>\$2,900</u> (Bolan Smart assumption)
Net Impact	\$11,900

Downtown Service District Revenue	\$14,000
Service District Cost	<u>\$14,000</u> (helps support hotel guest in lieu of Town services)
Net Impact	\$0

Overall TOCH Revenue	\$282,000
Overall Cost	<u>\$78,000</u>
Overall Net Impact	\$204,000

In sum, a new upscale hotel in downtown Chapel Hill represents a substantial gain of net revenue for the Town of Chapel Hill.

**Table 1 - AC Hotel, Chapel Hill, NC  
ANNUAL DIRECT DC TAX REVENUE NET OF MULTIPLIERS - \$2015**

**Project Description**

**Direct Project Full Time Employment (a)**

1) Hotel Space	68,000 gsf (net of parking)		
2)	123 rooms (550 gsf / room)	0.40 jobs / room	49 jobs
3) Food Service	included above	included above	
4) Other Spaces	included above	included above	
5) Parking	112 spaces	included above	

**Real and Personal Property Tax**

<b>Building / Parking</b>			
		<b>one room</b>	<b>total rooms</b>
6) General Real Estate Value	\$235 per gsf	\$130,000	\$15,990,000
7) Parking (included in hotel)			NA
8) Business Personal Property	\$30,000 FF&E per room	<u>\$30,000</u>	<u>\$3,690,000</u>
9)	\$289 per gsf	\$160,000	\$19,680,000
10) Total Taxable Value (Franklin / Hampton approx \$160k / room)	100% of value	\$160,000	\$19,680,000
11) <b>ToCH Real and Personnel Tax General Fund Revenue</b>	0.3780% general fund tax rate	\$605	<b>\$74,390</b>

**Hotel Direct Tax Revenues**

12) Room Nights	75% occupancy	274	33,671
13) Room Revenue (net of parking)	\$145 average daily rate	<u>\$39,694</u>	<u>\$4,882,331</u>
14) ToCH Occupancy Tax	3.0000% tax rate	\$1,191	\$146,470
15) Other Hotel Expenditures (food & beverage, other services)	\$20 per occupied room night	<u>\$5,475</u>	<u>\$673,425</u>
16) ToCH Other Hotel Guest Sales Tax	0.6870% tax rate	\$38	\$4,626
17) Gross Revenue (net of parking)		\$45,169	\$5,555,756
18) Other Corporate, Operating Taxes and Fees (b)	0.2500% of gross revenue	<u>\$113</u>	<u>\$13,889</u>
19) <b>Total Direct Hotel Use Related General Fund Revenue</b>		\$161,341	<b>\$164,986</b>

**Parking**

		<b>one space</b>	<b>total spaces</b>
20) Parking Income	\$8.00 per space per room night		
21)	\$2,190 per space per year	<u>\$2,190</u>	<u>\$245,280</u>
22) <b>ToCH Parking Tax General Fund Revenue</b>	0.6870% of gross revenue	\$15	<b>\$1,685</b>

**Employee Related Sales Tax Revenue**

		<b>one room</b>	<b>total rooms</b>
23) FTE Employee Retail Expenditures Subject to Sales Tax	\$45.00 per FTE per week	<u>\$936</u>	<u>\$115,128</u>
24) <b>ToCH Employee Related Sales Tax General Fund Revenue</b>	0.6870% tax rate	\$6	<b>\$791</b>

**Total Annual General Fund Tax Revenue**

25) <b>ToCH General Fund Related Revenues</b>		\$1,966	<b>\$241,852</b>
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**Other ToCH Direct Annual Property Tax Revenue**

26)	0.0410% transit fund tax rate	\$66	\$8,069
27)	0.0750% debt service tax rate	\$120	\$14,760
28)	0.0710% downtown service district	<u>\$114</u>	<u>\$13,973</u>
29) <b>Total Other ToCH Real &amp; Personal Property Tax Revenue</b>		\$299	<b>\$36,802</b>

**Total ToCH Annual Tax Revenue**

		\$2,265	<b>\$278,654</b>
--	--	---------	------------------

**Non-ToCH Municipal Annual Taxes**

30) Orange County Real and Personal Tax Revenue	0.8780% tax rate	\$1,405	\$172,790
31) School District Real and Personal Tax Revenue	0.2084% tax rate	\$333	\$41,013
32) Orange County Occupancy Tax	3.0000% tax rate	\$1,191	\$146,470
33) Orange County Other Hotel Guest Sales Tax	1.6660% tax rate	\$91	\$11,219
34) Orange County Parking Revenue Tax	1.6660% tax rate	\$33	\$4,086
35) Orange County Employee Related Sales Tax	1.6660% tax rate	<u>\$16</u>	<u>\$1,918</u>
36) Other Non-Chapel Hill Tax and Fee Revenues		not considered	
37) <b>Total Other Municipal Related Annual Taxes</b>		\$3,069	<b>\$377,497</b>

Notes:

- (a) FTE is full time equivalent employee, assumed at 40 hours per week.
- (b) Hotel franchise fees, operations purchases, utility and telecommunications fees, other business license fees and charges.



## WASTE INDUSTRIES

148 Stone Park Court Durham, NC 27703  
Bus: (919) 596-1363 Fax: (919) 598-1852 [www.waste-ind.com](http://www.waste-ind.com)

AC by Marriott  
W Rosemary & Church St.  
Chapel Hill, NC  
March 18, 2015

To Whom It May Concern,

We are in receipt of the plans provided to us by Overcash Demmit for the AC by Marriott project in Chapel Hill, North Carolina detailing the proposed solid waste and recycling receptacle location and service requirements (plans marked A1.1 and C-4). After review of the plans, Waste Industries is prepared to provide service for the projected amount of 96 gallon carts (6 solid waste, 4 recycling; twice per week service). Waste Industries does not require the segregation of cardboard from the recycling stream. Solid waste and recycling collection will be performed at curbside on West Rosemary Street during the early hours of the morning so as to alleviate traffic concerns. The location will be serviced by CNG semi-automated trucks that measure 34'x10'x13'.

Regards,

Jason McMillan  
Sales Representative  
Waste Industries USA, Inc.





# AC BY MARRIOTT

## SPECIAL USE PERMIT

PIN # 9788276387, 9788276326 & 9788275353  
212, 214 & 220 W. ROSEMARY STREET  
CHAPEL HILL, NORTH CAROLINA 27614  
PROJECT NUMBER: OTO-14000

DATE: JANUARY 26, 2015  
REVISED: MARCH 20, 2015  
REVISED: MAY 13, 2015

### SHEET INDEX

C-1	AREA MAP
C-2	EXISTING CONDITIONS PLAN
C-3	DEMOLITION PLAN
C-4	SITE PLAN - GROUND FLOOR
C-5	GRADING & STORMWATER MANAGEMENT PLAN
C-6	UTILITY PLAN
C-7	SOLID WASTE & CONSTRUCTION MANAGEMENT PLAN
LS-1	PLANTING & STREETScape PLAN
A1.1	GROUND FLOOR PLAN
A1.2	SECOND FLOOR PLAN
A1.3	THIRD FLOOR PLAN
A1.4	FOURTH FLOOR PLAN
A1.5	BUILDING/SITE SECTIONS
A1.5a	SITE DETAILS/SUN STUDY/ROOF DIAGRAM
A1.1G	PARKING GARAGE (LEVEL-1)
A1.2G	PARKING GARAGE (LEVEL-2)
A1.6	BUILDING ELEVATIONS/RENDERINGS
A1.7	BUILDING ELEVATIONS/RENDERINGS

### DEVELOPER:

OTO DEVELOPMENT, LLC  
100 DUNBAR STREET, SUITE 402  
SPARTANBURG, SOUTH CAROLINA 29306



CONTACT: DENNIS MITCHELL

PHONE: 864.327.4046

EMAIL: [dmitchell@otodevelopment.com](mailto:dmitchell@otodevelopment.com)



THE JOHN R. McADAMS  
COMPANY, INC.

2905 Meridian Parkway  
Durham, North Carolina 27713  
License No.: C-0293  
(800) 733-5648 • [McAdamsCo.com](http://McAdamsCo.com)  
Contact: William H. Derks, PE  
[derks@mcadamsco.com](mailto:derks@mcadamsco.com)



VICINITY MAP  
NTS





REVISIONS:  
 2015-09-27 TOWN OF CHAPEL HILL COMMENTS

DEVELOPER:  
**OTO DEVELOPMENT, LLC**  
 100 DUNBAR ST., SUITE 402  
 SPARTANBURG, SC 29306  
 PHONE: 864-696-8930

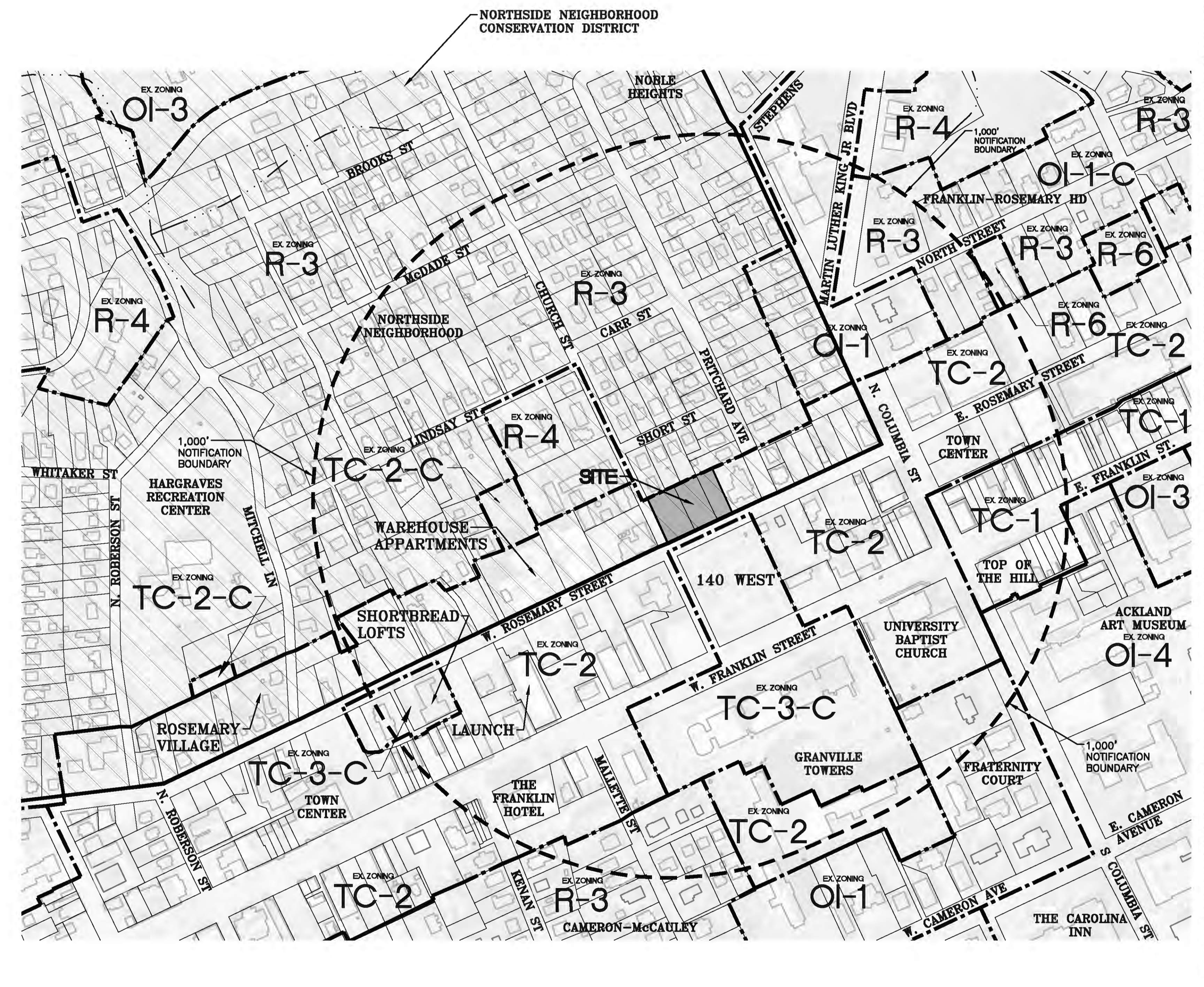
**AC BY MARIOTT**  
 CHAPEL HILL, NORTH CAROLINA  
 AREA MAP

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 DRAWN BY: KWH  
 SCALE: 1"=200'  
 DATE: 01-26-2015  
 SHEET NO: C-1



**LAND PARCEL SUMMARY**

OWN	OWNER	ADDRESS	STATUS	ZONING	PERM	OWNER	ADDRESS	STATUS	ZONING
01-3	CHAPL HILL FOUNDATION REAL ESTATE HOLDINGS INC	385 SOUTH BLVD QUAKER UNIC	CHAPL HILL NC 27959-1000	EX ZONING	7/29/1969	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING
01-3	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING	7/29/1969	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING
01-3	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING	7/29/1969	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING
01-3	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING	7/29/1969	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING
01-3	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING	7/29/1969	UNIVERSITY DEVELOPMENT GROUP LLC	428 WESTWOOD DR	CHAPL HILL NC 27513	EX ZONING



**SITE INFORMATION:**

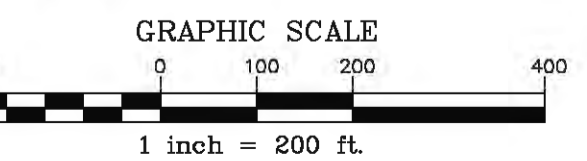
**FIN:** 9788276387, 9788276326, 9788275353  
**ADDRESS:** 212, 214 & 220 W. ROSEMARY ST. CHAPEL HILL, NC 27514  
**OWNER:** SLOMIANY, MICHAEL AND KIMBERLY  
**ACREAGE:** 0.72

**GENERAL NOTES:**

1. PLANIMETRIC INFORMATION OBTAINED FROM ORANGE COUNTY GIS.

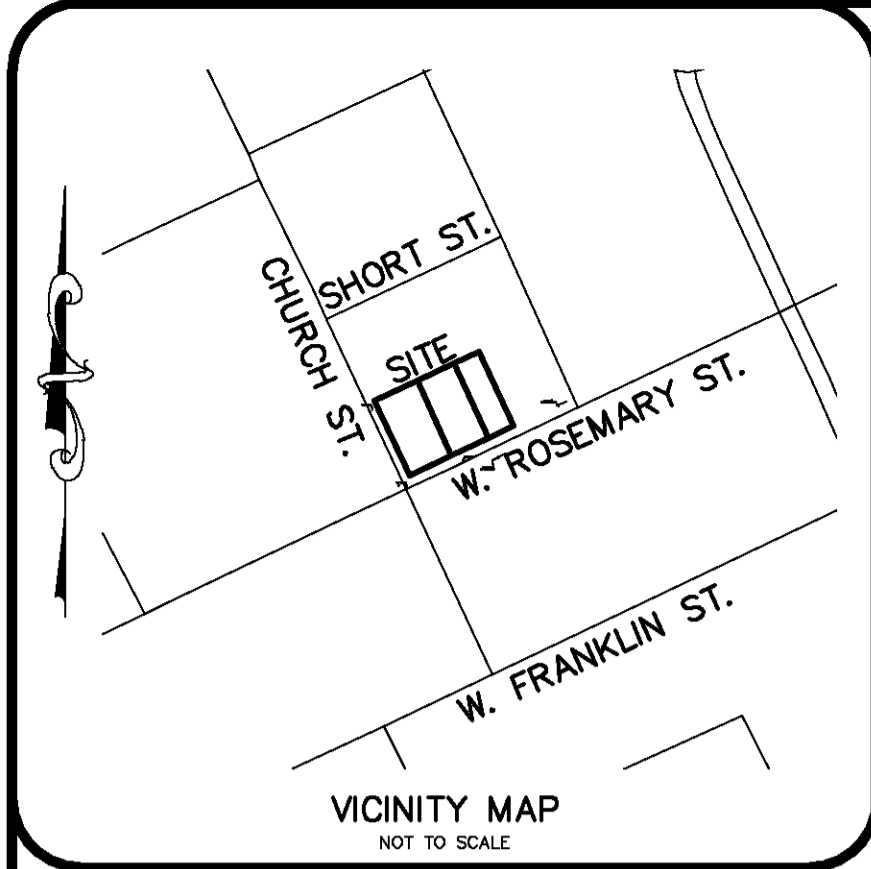
**LEGEND**

- - - 1,000' NOTIFICATION BOUNDARY
- EXISTING ZONING BOUNDARY



PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION



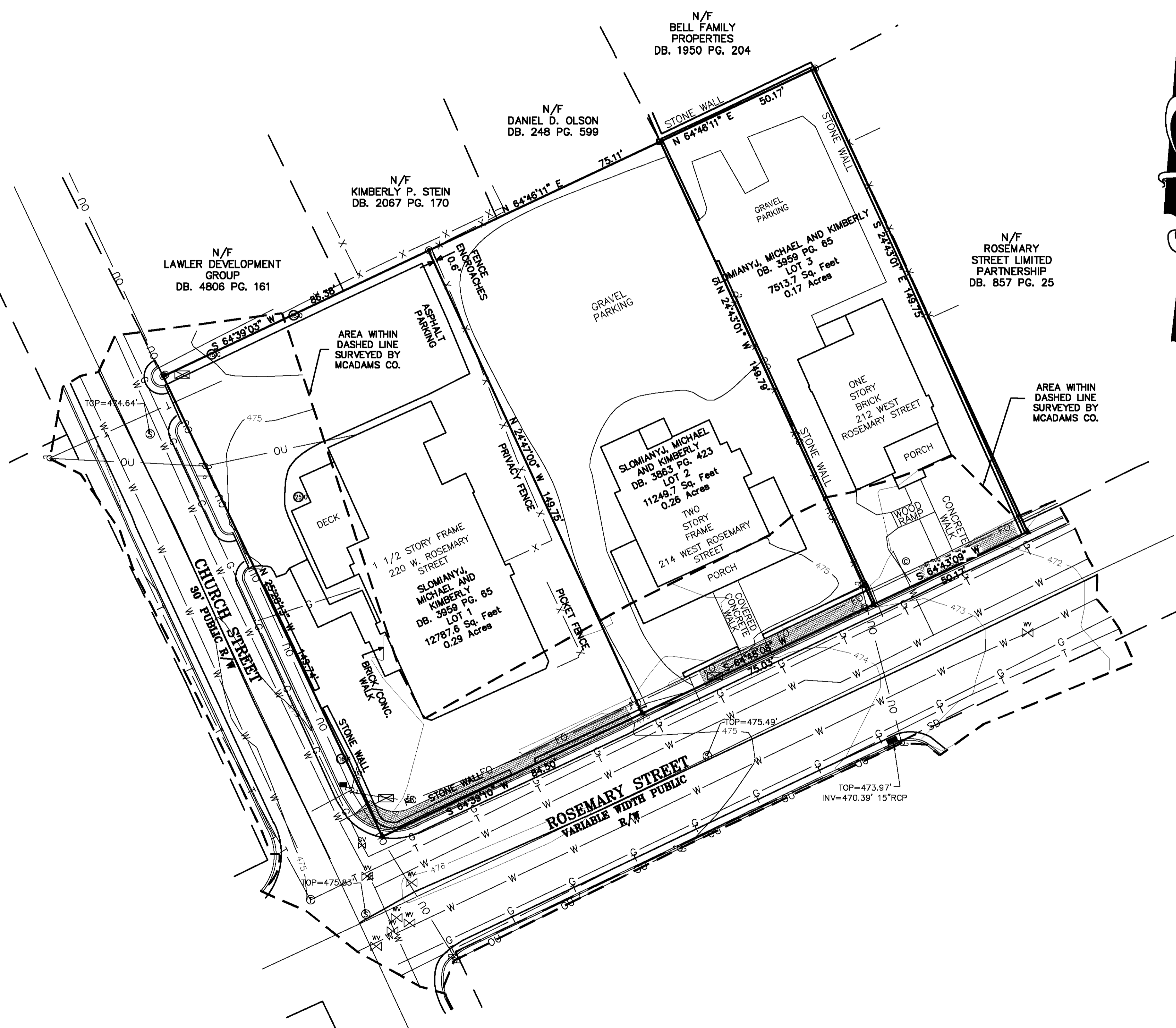


I, RONALD T. FREDERICK, CERTIFY THAT THIS PLAT WAS DRAWN UNDER MY SUPERVISION FROM AN ACTUAL SURVEY MADE UNDER MY SUPERVISION FROM DEED DESCRIPTION RECORDED IN BOOK AND PAGE (AS SHOWN); THAT THE BOUNDARIES NOT SURVEYED ARE CLEARLY INDICATED AND DRAWN FROM INFORMATION FOUND IN BOOK AND PAGE (AS SHOWN); THAT THE RATIO OF PRECISION AS CALCULATED IS 1:10,000±; AND THAT THIS PLAT WAS PREPARED IN ACCORDANCE WITH G.S. 47-30 AS AMENDED; WITNESS MY ORIGINAL SIGNATURE, REGISTRATION NUMBER, AND SEAL THIS

DAY OF \_\_\_\_\_ A.D. 20\_\_\_\_  
RONALD T. FREDERICK PLS L-4720

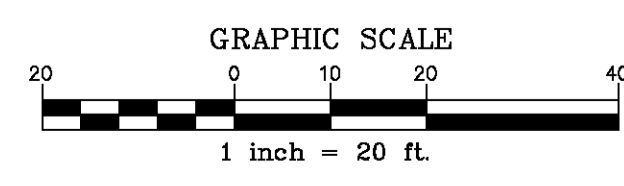
I HEREBY CERTIFY THAT THIS PLAT IS OF THE FOLLOWING TYPE: G.S. 47-30 (1)(11)(c)(1). THIS SURVEY IS OF AN EXISTING PARCEL OR PARCELS OF LAND AND DOES NOT CREATE A NEW STREET OR CHANGE AN EXISTING STREET.

RONALD T. FREDERICK PLS L-4720



- ### GENERAL NOTES
- THIS IS A SURVEY OF AN EXISTING PARCEL(S) OF LAND. THIS IS A BOUNDARY AND TOPOGRAPHIC SURVEY.
  - BEARINGS FOR THIS SURVEY ARE BASED ON NC GRID NAD 83.
  - ALL DISTANCES ARE HORIZONTAL GROUND DISTANCES.
  - ZONING: CHAPEL HILL TC-2
  - AREA BY COORDINATE GEOMETRY.
  - FLOOD NOTE: THIS PROPERTY IS NOT LOCATED IN A SPECIAL FLOOD HAZARD ZONE. IT IS LOCATED IN ZONE "X" AS DEFINED BY F.E.M.A. F.I.R.M. COMMUNITY PANEL #0000000000 DATED XXXX XX XXXX.
  - REFERENCES: XXXXXXXXXX OF THE XXXXX COUNTY REGISTRY. PIN: XXXXXXXXXX
  - THIS SURVEY PERFORMED AND MAP PREPARED WITHOUT BENEFIT OF A TITLE REPORT. THIS SURVEY SUBJECT TO ANY FACTS AND EASEMENTS WHICH MAY BE DISCLOSED BY A FULL AND ACCURATE TITLE SEARCH.
  - NO GRID MONUMENTS FOUND WITHIN 2000' OF SITE.
  - ELEVATIONS FOR THIS SURVEY ARE BASED ON NAVD 88 DATUM ESTABLISHED BY GPS SESSIONS.
  - AREA WITHIN THE BOLD DASHED LINE SURVEYED BY MCADAMS COMPANY. AREA OUTSIDE OF LINE WAS TAKEN FROM A SURVEY PROVIDED BY FREEHOLD LAND SURVEYS INC.

**PRELIMINARY PLAT  
NOT FOR RECORDATION  
CONVEYANCES OR SALES**



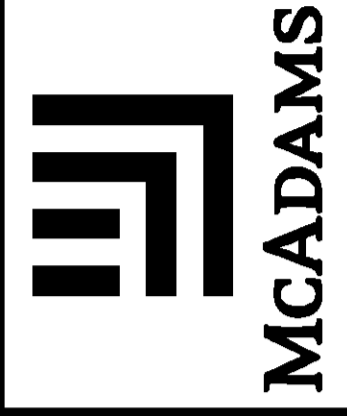
#### TREE LEGEND

- ⊙ SWEET GUM
- ⊙ OAK

#### LEGEND

- ⊙ EXISTING IRON PIPE
- IRON PIPE SET
- ▲ CALCULATED POINT
- ⊙ BORE HOLE
- ⊙ SANITARY SEWER MANHOLE
- ⊙ SANITARY SEWER CLEANOUT
- ⊙ WATER VALVE
- ⊙ WATER METER
- ⊙ FIRE HYDRANT
- ⊙ TELEPHONE PEDESTAL
- ⊙ TELEPHONE MANHOLE
- ⊙ ELECTRIC BOX
- ☆ LIGHT POLE
- ⊙ POWER POLE
- ⊙ CURB INLET
- ⊙ STORM DRAINAGE MANHOLE
- YARD INLET
- SD — STORM DRAIN PIPE
- OU — OVERHEAD UTILITY LINES
- W — WATER LINE
- SS — SANITARY SEWER LINE
- T — TELEPHONE LINE
- G — GAS LINE
- UE — UNDERGROUND ELECTRIC
- UT — UNDERGROUND TELEPHONE
- X — FENCE LINE

THE JOHN R. MCADAMS COMPANY, INC.  
2905 Meridian Parkway  
Durham, North Carolina 27713  
Tel: 919-428-2888  
Fax: 919-428-2888  
www.mcadamsco.com



REVISIONS:

NO.	DATE	DESCRIPTION

SURVEY FOR:  
OTO DEVELOPMENT LLC,  
100 DUNBAR ST. SUITE 402  
SPARTANBURG, SC 29306

**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA  
EXISTING CONDITIONS

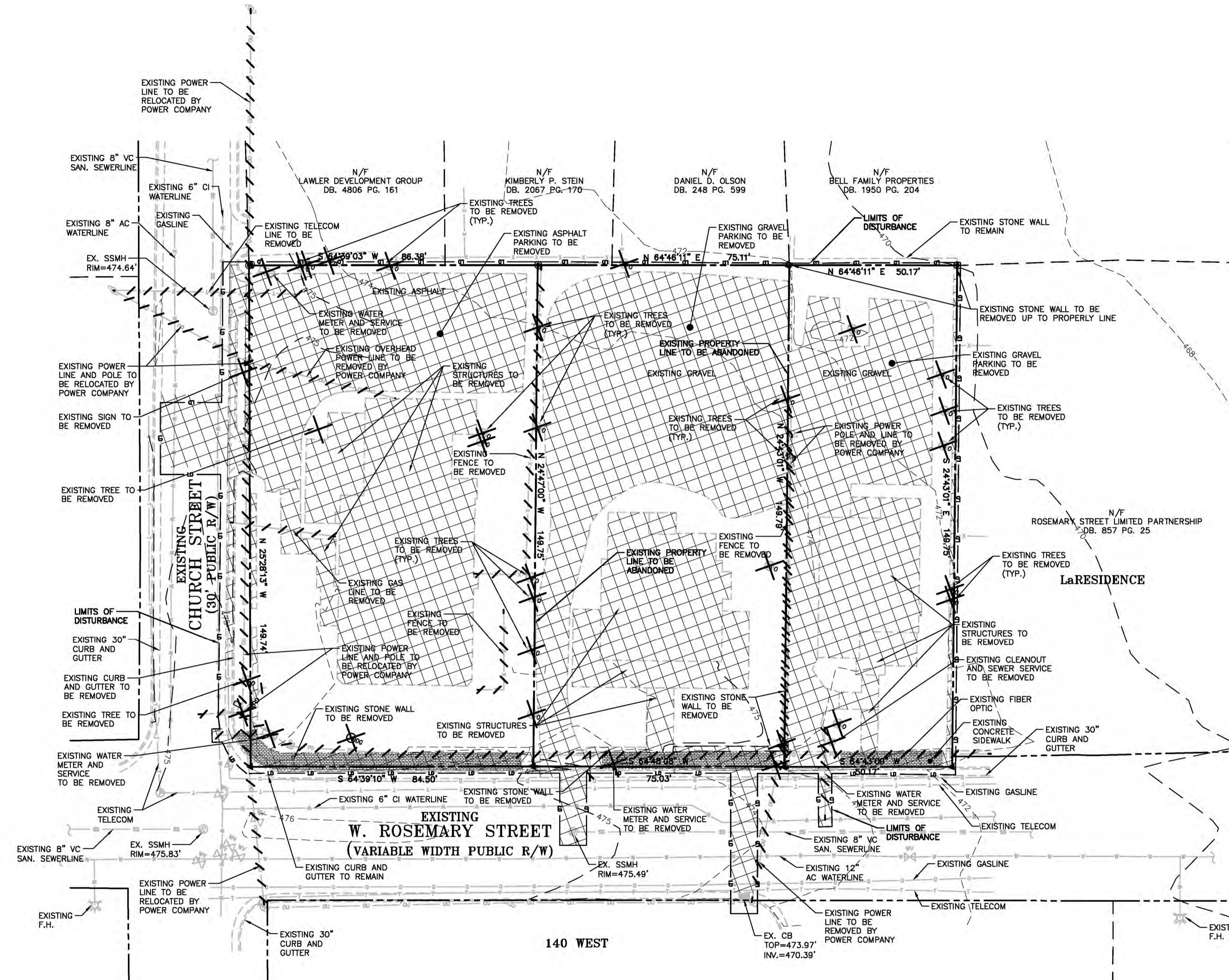
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SHEET NO. C-2



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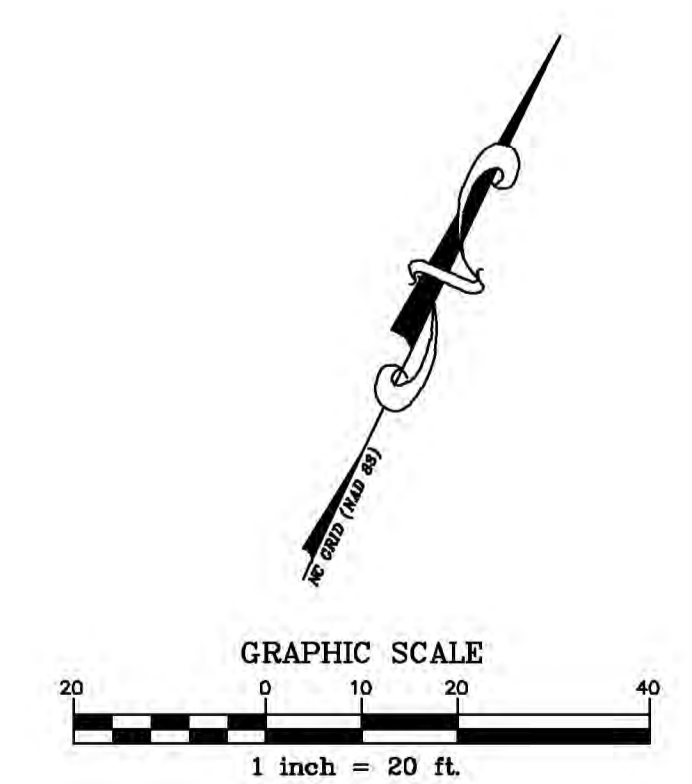


**DEMOLITION NOTES:**

1. THE LOCATION OF ALL UTILITIES SHOWN ON THESE PLANS ARE BASED ON A FIELD SURVEY PERFORMED BY THE JOHN R. MCADAMS COMPANY. CONTRACTOR IS RESPONSIBLE FOR CONTACTING NORTH CAROLINA "ONE CALL" (811) FOR ASSISTANCE IN LOCATING EXISTING UTILITIES. CALL AT LEAST 48 HOURS PRIOR TO DIGGING.
2. PRIOR TO BEGINNING ANY WORK IN THE STREET OR RIGHT-OF-WAY IT WILL BE NECESSARY TO CONTACT LARRY TUCKER (919-989-5084) IN THE TOWN OF CHAPEL HILL ENGINEERING AND DESIGN SERVICES DIVISION TO APPLY FOR AN ENGINEERING CONSTRUCTION PERMIT.
3. CONTRACTOR IS RESPONSIBLE FOR DEMOLITION, REMOVAL AND DISPOSAL OF ALL INFRASTRUCTURE INCLUDING, BUT NOT LIMITED TO, SANITARY AND STORM SEWER, WATER LINES, GAS AND ELECTRICAL SERVICES, CABLE SYSTEMS, UTILITY POLES, BUILDINGS AND FOUNDATIONS THAT MAY INTERFERE WITH PROPOSED CONSTRUCTION.
4. CONTRACTOR IS RESPONSIBLE FOR COORDINATION WITH APPROPRIATE UTILITY PERSONNEL AT THE TOWN OF CHAPEL HILL, OWASA, DUKE ENERGY, PSNC AND OTHER PROVIDERS FOR STEPS REQUIRED FOR TEMPORARY OR PERMANENT SHUT DOWN OF SERVICES AS REQUIRED FOR DEMOLITION.
5. ALL DEMOLITION DEBRIS BECOMES PROPERTY OF THE CONTRACTOR ONCE OFF-SITE. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO PROPERLY DISPOSE OF OR RECYCLE DEMOLITION MATERIALS IN ACCORDANCE WITH THE SOLID WASTE MANAGEMENT PLAN.
6. CONTRACTOR SHALL MAINTAIN ACCESS AND UTILITY SERVICES TO ALL ADJACENT BUSINESSES AND RESIDENCES THROUGHOUT BUILDING DEMOLITION AND CONSTRUCTION.
7. CONTRACTOR SHALL REPORT ANY DISCREPANCIES TO ENGINEER/OWNER REP IMMEDIATELY.
8. CONTRACTOR SHALL HOLD A PRECONSTRUCTION/PREDEMOLITION MEETING WITH ORANGE COUNTY SOLID WASTE STAFF (919-996-2788) PRIOR TO ANY DEMOLITION.
9. REMOVAL OF EXISTING HYDRANTS, WATER AND/OR SEWER SERVICES SHALL BE COMPLETED IN ACCORDANCE WITH OWASA STANDARDS AND SPECIFICATIONS.
10. CONSTRUCTION WASTE:
  - ALL EXISTING STRUCTURES 500 SQUARE FEET AND LARGER IN SIZE SHALL BE ASSESSED PRIOR TO DEMOLITION TO ENSURE COMPLIANCE WITH THE COUNTY'S REGULATED RECYCLABLE MATERIALS ORDINANCE (RRMO) AND TO ASSESS THE POTENTIAL FOR DE-CONSTRUCTION AND/OR THE RE-USE OF SALVAGEABLE MATERIALS.
  - BY ORANGE COUNTY ORDINANCE, CLEAN WOOD WASTE, SCRAP METAL, AND CORRUGATED CARDBOARD PRESENT IN CONSTRUCTION OR DEMOLITION WASTE MUST BE RECYCLED.
  - BY ORANGE COUNTY ORDINANCE, ALL HAULERS OF MIXED CONSTRUCTION AND DEMOLITION WASTE THAT INCLUDES ANY REGULATED RECYCLABLE MATERIALS SHALL BE LICENSED BY ORANGE COUNTY.
  - PRIOR TO ANY DEMOLITION OR CONSTRUCTION ACTIVITY ON THE SITE THE APPLICANT SHALL HOLD A PRE-DEMOLITION/PRE-CONSTRUCTION CONFERENCE WITH THE SOLID WASTE STAFF. THIS MAY BE THE SAME PRE-CONSTRUCTION MEETING HELD WITH OTHER DEVELOPMENT/ENFORCEMENT OFFICIALS.
  - THE PRESENCE OF ANY ASBESTOS CONTAINING MATERIALS ("ACM") AND/OR OTHER HAZARDOUS MATERIALS IN CONSTRUCTION AND DEMOLITION WASTE SHALL BE HANDLED IN ACCORDANCE WITH ANY AND ALL LOCAL, STATE, AND FEDERAL REGULATIONS AND GUIDELINES.
11. RELOCATION OF EXISTING GAS, FIBER OPTIC AND POWER LINES MAY BE REQUIRED ALONG PROJECT FRONTAGE ON W. ROSEMARY AND CHURCH STREETS. RELOCATION TO BE COORDINATED WITH APPROPRIATE COMPANY DURING PREPARATION OF FINAL PLANS.

**DEMOLITION LEGEND**

- POWER POLE, SIGN, HYDRANT, ETC. TO BE REMOVED
- CURB AND GUTTER, SIDEWALK OR UNDERGROUND UTILITY TO BE REMOVED
- AREA TO BE REMOVED - SEE NOTE B (ASPHALT, CONCRETE PAVING, ETC.)
- LIMITS OF DISTURBANCE
- TREE TO BE REMOVED



PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION

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Durham, North Carolina 27713  
Tel: C-288  
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REVISIONS:

NO.	DATE	DESCRIPTION
1	2015-02-27	TOWN OF CHAPEL HILL COMMENTS

DEVELOPER:  
OTO DEVELOPMENT, LLC  
100 DUNBAR ST., SUITE 402  
SPARTANBURG, SC 29306  
PHONE: 864-596-8930

**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA

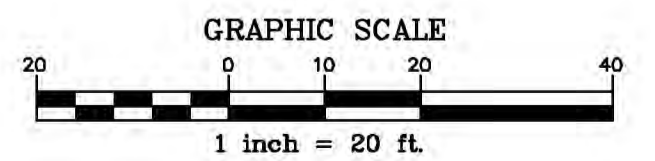
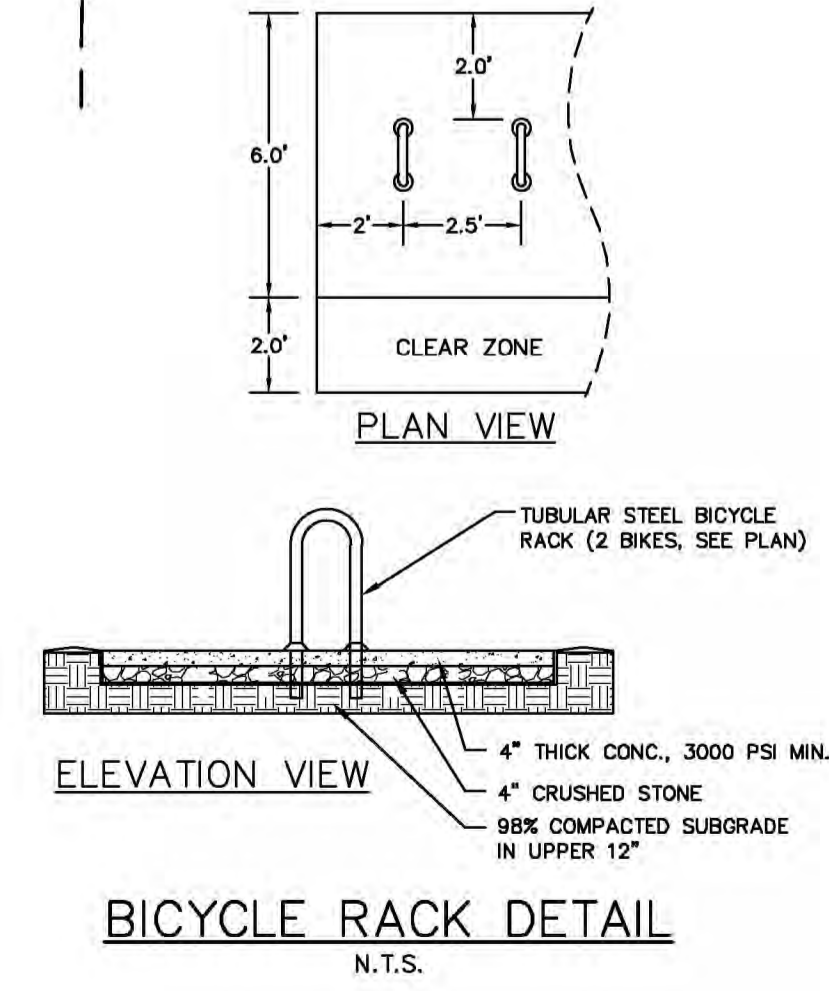
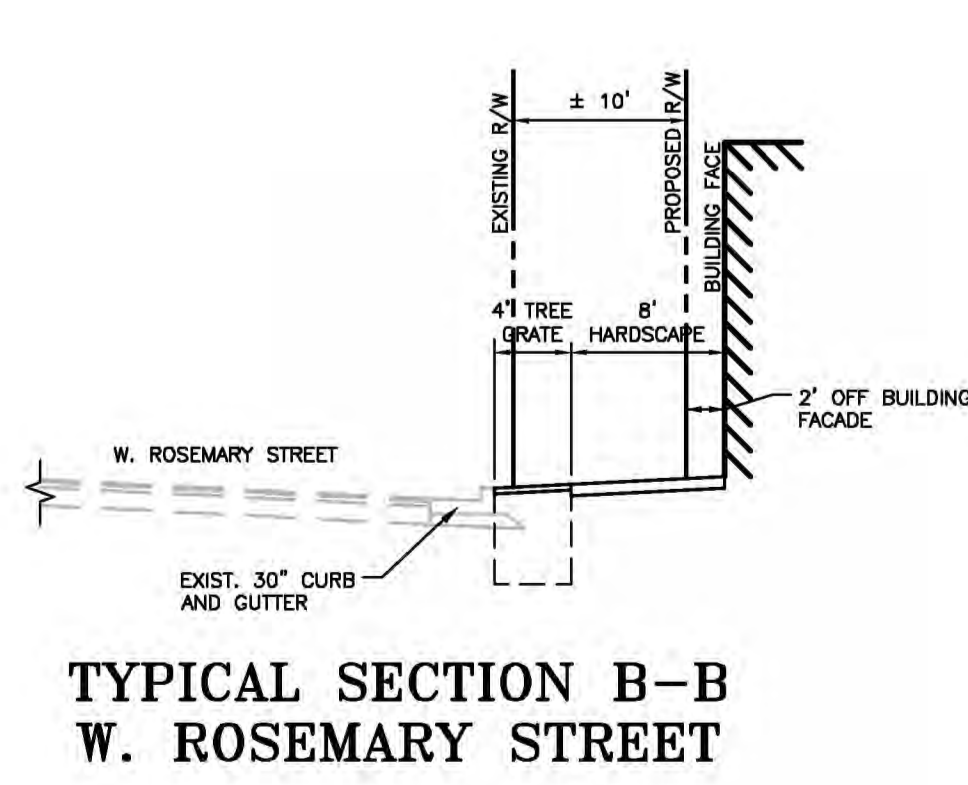
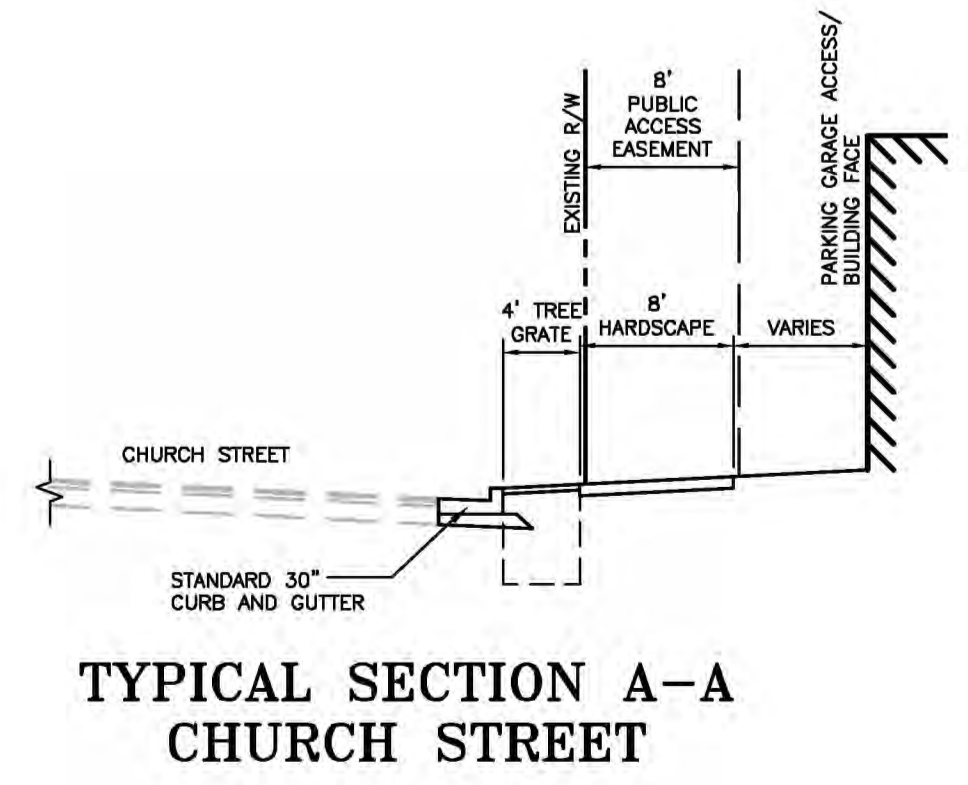
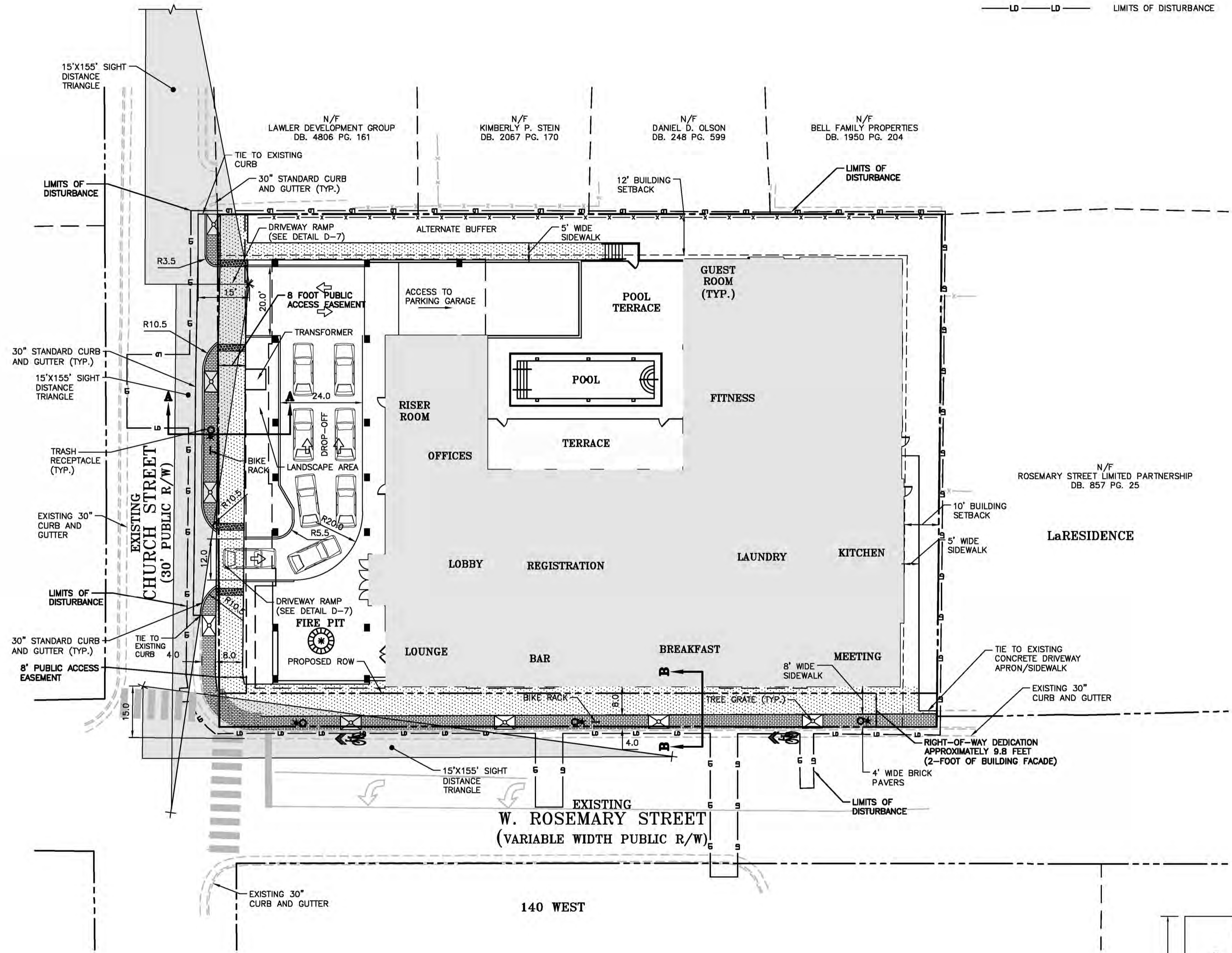
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DESIGNED BY:	-
DRAWN BY:	KWH
SCALE:	1"=20'
DATE:	01-26-2015
SHEET NO.	C-3



DEMOLITION PLAN



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**SITE LEGEND**

	CONCRETE SIDEWALK
	BRICK PAVERS
	TREE GRATE
	PEDESTRIAN SCALE LIGHT
	TRASH RECEPTACLE
	BIKE RACK
	PROPERTY LINE
	RIGHT-OF-WAY
	LIMITS OF DISTURBANCE

**SITE SUMMARY:**

PIN #:	9788276387, 9788276326, 9788275353
LOT SIZE:	0.72 AC
EXISTING ZONING:	TC-2
JURISDICTION:	TOWN OF CHAPEL HILL
PROPOSED USE:	HOTEL

**BUILDING DATA**

EXISTING:	GROSS FLOOR AREA:	8840 SF
PROPOSED:	GROSS SF:	131,000
	DECK SF:	63,000
	ENCLOSED SPACE SF:	68,300
	BUILDING HEIGHT:	
	W. ROSEMARY CHURCH	+/- 50'
		+/- 40', +/- 80'
	# OF STORIES:	4
	# OF UNITS:	123

**PARKING DATA**

VEHICLE PARKING	REQUIRED (MAX):	111
	PROPOSED:	
	DECK/UNDERGROUND:	111
	STANDARD:	82
	COMPACT:	24
	ACCESSIBLE:	5

**BIKE PARKING**

REQUIRED:	8
PROVIDED:	
TOTAL:	12
CLASS I:	8 (IN PARKING GARAGE)
CLASS II:	4 (2 RACKS)

**SETBACKS**

STREET (MIN):	REQUIRED:	0	PROPOSED:	0
INTERIOR (MIN):	8	12 (NORTH)	10 (EAST)	12
SOLAR (MIN):	0	10	12	

**IMPERVIOUS COVERAGE**

EXISTING:	23,015 SF
PROPOSED:	31,532 SF*

\*TO BE CONFIRMED WITH FINAL PLANS(ZCP)

**PUBLIC ART**

THE AC HOTEL BY MARRIOTT AIMS TO PROVIDE A UNIQUE, UPSCALE, HOTEL OPTION IN DOWNTOWN CHAPEL HILL. THE AC HOTEL INTENDS TO COMPLEMENT THE EXISTING CULTURE OF THE COMMUNITY AND ENGAGE THE GUESTS TO EXPERIENCE THE CULTURE. THE INTERIOR DESIGNER WILL BE CHALLENGED TO INCORPORATE LOCAL ART IN TO THE GROUND FLOOR COMMON AREA. INCORPORATING LOCAL ART WILL OFFER A GATEWAY FOR GUESTS TO EXPLORE CHAPEL HILL.

- GENERAL NOTES:**
- THIS PROPERTY IS NOT LOCATED IN A FLOOD HAZARD AREA AS DEFINED BY THE USDOH AND URBAN DEVELOPMENT REF: FIRM PANEL #3710978800J DATED FEB 2, 2007
  - BOUNDARY INFORMATION IS FROM A SURVEY BY THE JOHN R MCADAMS COMPANY FROM DECEMBER 2014.
  - PRIOR TO ANY WORK IN THE PUBLIC RIGHT-OF-WAY, CONTACT LARRY TUCKER (919-969-5084) WITH TOWN OF CHAPEL HILL ENGINEERING TO APPLY FOR AN ENGINEERING CONSTRUCTION PERMIT.
  - SEVEN CALENDAR DAYS PRIOR TO ANY PROPOSED STREET LANE CLOSURE, CONTACT ERNIE ROGERS (919-969-5100) WITH TOWN OF CHAPEL HILL TRAFFIC ENGINEERING TO APPLY FOR A LANE CLOSURE PERMIT.
  - ALL DAMAGE TO TOWN MAINTAINED STREETS DUE TO PROJECT CONSTRUCTION WILL BE REQUIRED TO BE REPAIRED AT NO COST TO THE TOWN PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY.

- FIRE DEPARTMENT NOTES:**
- A FIRE REPORT SHALL BE SUBMITTED AS PART OF THE FINAL PLANS SHOWING THE FIRE FLOW FOR THIS PROJECT.
  - BUILDING SHALL HAVE AN AUTOMATIC FIRE SPRINKLER SYSTEM INSTALLED PRIOR TO ISSUANCE OF A CERTIFICATE OF OCCUPANCY.
  - ALL STRUCTURES MUST BE LOCATED WITHIN 500' OF A FIRE HYDRANT. THE FIRE DEPARTMENT CONNECTION CAN BE NO FURTHER THAN 100' FROM THE CLOSEST HYDRANT.
  - APPROVED BUILDING ADDRESS NUMBERS, PLACED IN ACCEPTABLE POSITION TO THE FIRE CODE OFFICIAL, SHALL BE REQUIRED ON ALL NEW BUILDINGS. NCFPC 2012, 505.1
  - W. ROSEMARY STREET IS DESIGNATED FIRE APPARATUS ACCESS ROAD. FACE OF BUILDING IS 15'-30' OFF EDGE-OF-PAVEMENT.

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REVISIONS:

NO.	DATE	DESCRIPTION
1	2015-02-27	TOWN OF CHAPEL HILL COMMENTS

DEVELOPER:  
OTO DEVELOPMENT, LLC  
100 DUNBAR ST., SUITE 402  
SPARTANBURG, SC 29306  
PHONE: 864-596-8930

**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA  
SITE PLAN - GROUND FLOOR

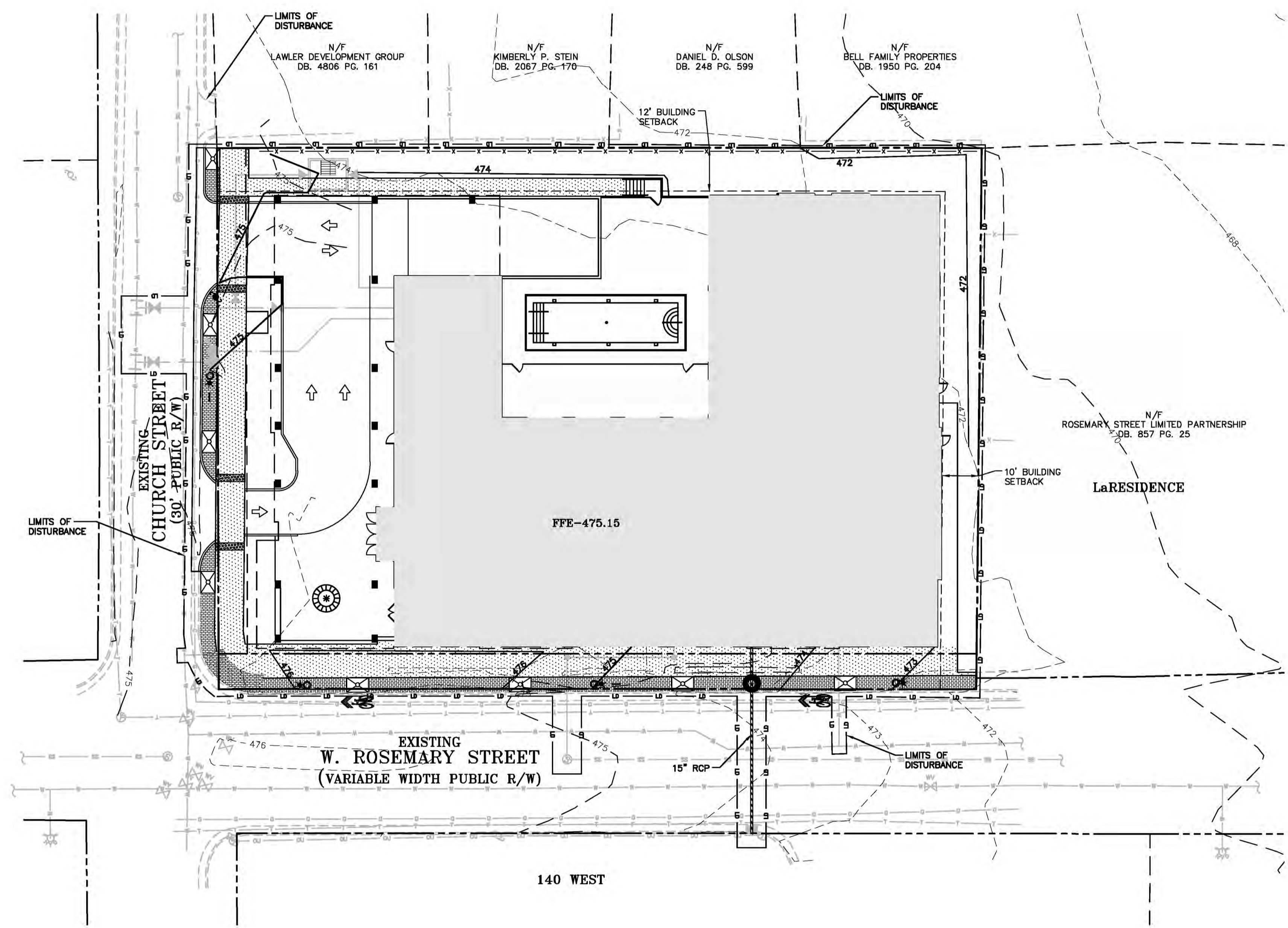
PROJECT NO.	OTO-14000
FILENAME:	OT014000-S1
DESIGNED BY:	-
DRAWN BY:	KWH
SCALE:	1"=20'
DATE:	01-26-2015
SHEET NO.	C-4



PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION



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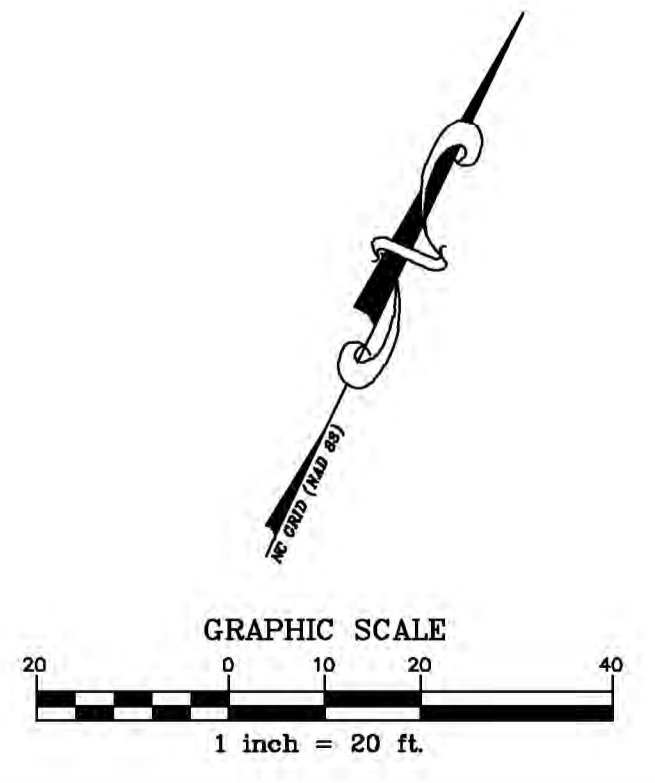


- GRADING/STORMWATER NOTES:**
1. THE PROPOSED ELEVATORS, WHICH MAY REQUIRE SLUMPS IF HYDRAULIC, WILL BE REQUIRED TO BE EQUIPPED WITH OIL SEPARATORS AND PLUMBED TO THE SANITARY SEWER SYSTEM, NOT THE STORM DRAINAGE SYSTEM.
  2. ALL STORM DRAINAGE PIPES SHOWN ARE TO BE CLASS III REINFORCED CONCRETE (RCP) UNLESS NOTED OTHERWISE.
  3. ALL STORM DRAINAGE CONSTRUCTION SHALL BE IN ACCORDANCE WITH THE TOWN OF CHAPEL HILL LATEST STANDARDS AND SPECIFICATIONS.
  4. DOWNSPOUTS SHALL BE SIZED ACCORDING TO BUILDING CODE AND SHOWN ON THE ARCHITECTURAL PLANS. ROOF LEADERS TO TIE TO THE PROPOSED STORM DRAIN SYSTEM UNDERGROUND.
  5. ALL DAMAGE TO STREETS MAINTAINED BY THE TOWN OF CHAPEL HILL DUE TO PROJECT CONSTRUCTION SHALL BE REPAIRED AT NO COST TO THE TOWN PRIOR TO A CERTIFICATE OF OCCUPANCY BEING ISSUED.
  6. NO STORMWATER RUNOFF FROM THE PARKING GARAGES IS TO BE CONVEYED INTO THE STORM DRAINAGE SYSTEM. ANY STORMWATER RUNOFF FROM WITHIN THE GARAGES IS TO BE CONVEYED TO A SC-TYPE OIL/WATER SEPARATOR THROUGH A DRAIN LOCATED IN THE GARAGE SLAB AND CONNECTED TO SANITARY SEWER.

**GRADING LEGEND**

	STORM DRAINAGE
	LIMITS OF DISTURBANCE
	WOODED AREA
	5' CONTOUR
	1' CONTOUR
	EXISTING 5' CONTOUR
	EXISTING 1' CONTOUR
	EASEMENT LINE
	JUNCTION BOX

DISTURBED AREA = 0.79 AC.



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REVISIONS:

▲	2015-05-27	TOWN OF CHAPEL HILL COMMENTS
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DEVELOPER:  
OTO DEVELOPMENT, LLC  
100 DUNBAR ST., SUITE 402  
SPARTANBURG, SC 29306  
PHONE: 864-596-8930

**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA  
GRADING AND STORMWATER MANAGEMENT PLAN

PROJECT NO.	OTO-14000
FILENAME:	OT014000-G1
DESIGNED BY:	-
DRAWN BY:	KWH
SCALE:	1"=20'
DATE:	01-26-2015
SHEET NO.	C-5





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**FIRE DEPARTMENT NOTES**

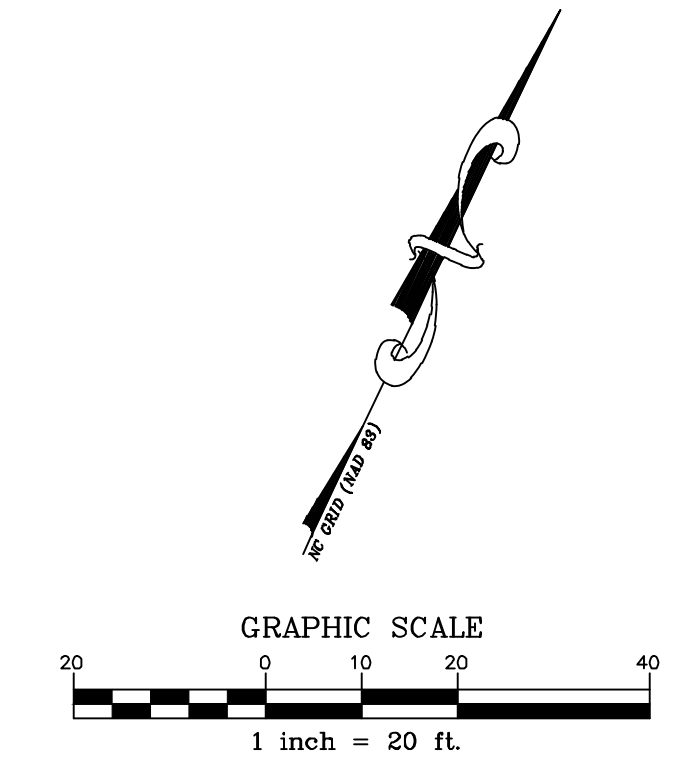
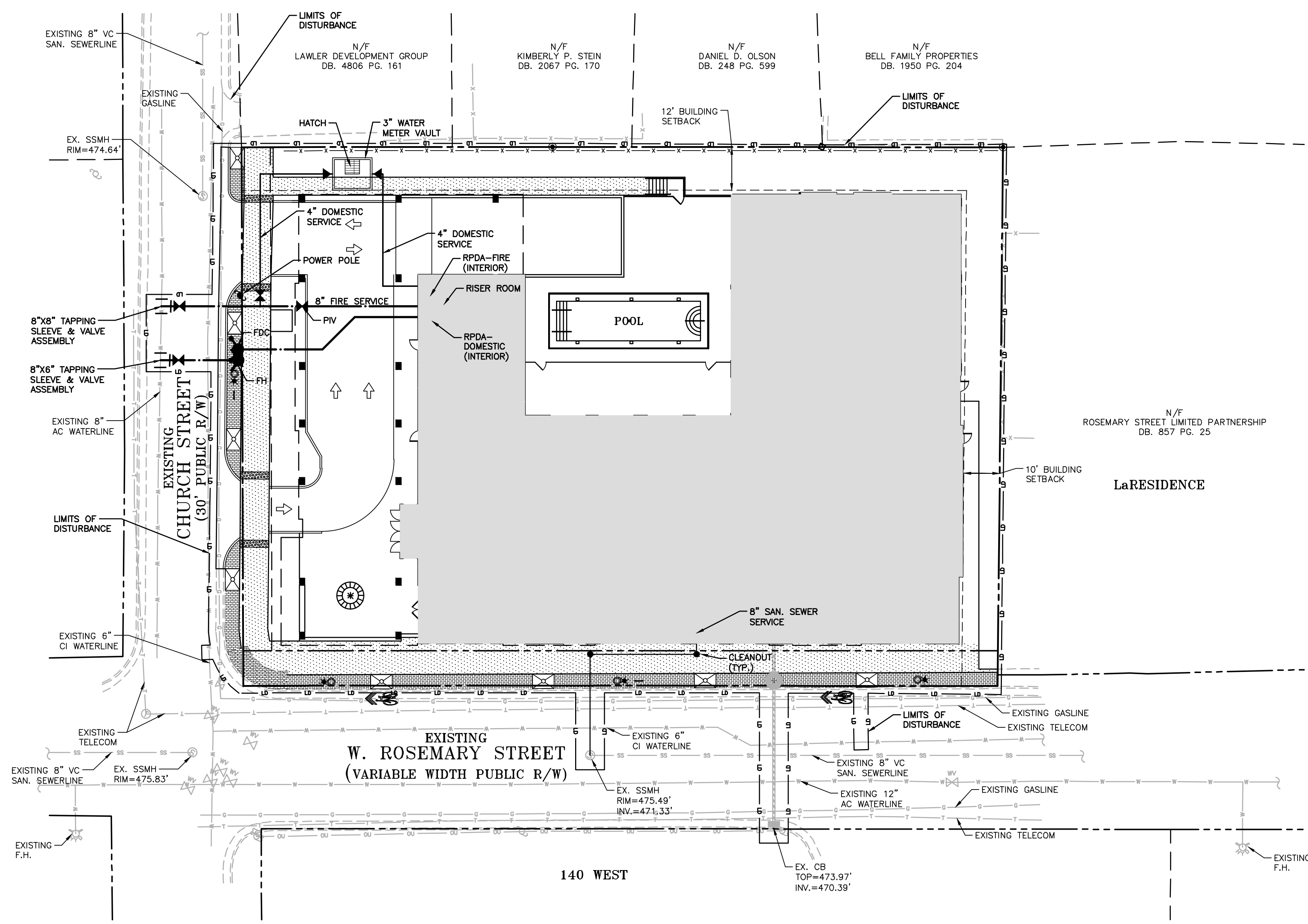
1. FIRE RISER ROOM SHALL BE EQUIPPED WITH AN EXTERNAL LOCK AND KNOX BOX.
2. RISER ROOM SHALL HAVE ADEQUATE DRAINAGE FOR EMERGENCY RPZ DISCHARGE.
3. A TEMPORARY STANDPIPE CONNECTION WILL BE REQUIRED WITH READY FIRE DEPARTMENT ACCESS WHEN THE BUILDING IS MORE THAN 40' IN HEIGHT IN ACCORDANCE WITH NC FPC SECTION 2012 SECTION 1413.
4. PRIVATE FIRE SERVICE MAINS AND APPURTENANCES SHALL BE INSTALLED IN ACCORDANCE WITH NFPA 24, NC FPC 507.2.1.

**UTILITY LEGEND**

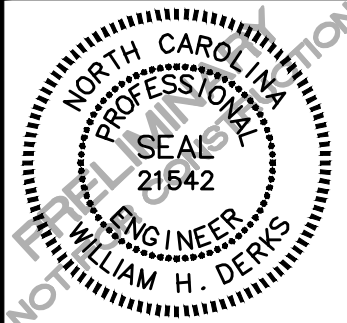
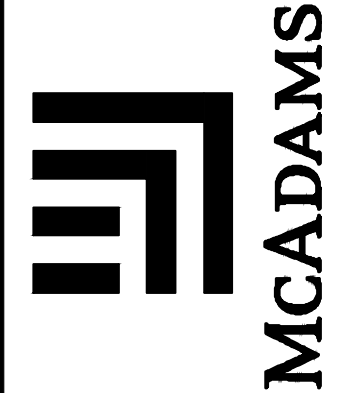
	FIRE HYDRANT ASSEMBLY		GREASE TRAP
	FIRE DEPARTMENT CONNECTION (FDC)		SEWER FLOW DIRECTION ARROW
	WATER METER		WATERLINE
	WATER VALVE		WATER SERVICE LINE
	POST INDICATOR VALVE		UTILITY SLEEVE
	VALVE IN MANHOLE		SANITARY SEWER
	METER & VAULT		SEWER SERVICE LINE
	BACKFLOW PREVENTER		GAS LINE
	REDUCER		OVERHEAD UTILITY
	PLUG		UNDERGROUND ELECTRIC
	BLOW-OFF ASSEMBLY		TELEPHONE
	SANITARY SEWER MANHOLE		EASEMENT LINE
	SEWER CLEAN-OUT		

**UTILITY NOTES**

1. ALL CONSTRUCTION SHALL BE IN ACCORDANCE WITH OWASA STANDARDS AND SPECIFICATIONS DATED AUGUST, 2003, OR LATEST REVISED VERSION.
2. PLEASE BE ADVISED THAT OWASA APPROVAL OF THIS PROJECT IS FOR COMPLIANCE WITH OWASA POLICIES, STANDARDS, AND SPECIFICATIONS ONLY. ALL OTHER MATTERS PERTAINING TO THIS PROJECT ARE THE RESPONSIBILITY OF THE DESIGN ENGINEER. OWASA APPROVAL DOES NOT PRECLUDE THE DEVELOPER, PROJECT ENGINEER, CONTRACTOR, OR OTHER AGENTS OR PARTIES ACTING ON THEIR BEHALF FROM FULL COMPLIANCE WITH OWASA CURRENT STANDARDS, SPECIFICATIONS, AND PROCEDURES OR FROM COMPLYING WITH ANY AND ALL STATUTES, RULES, REGULATIONS, AND ORDINANCES WHICH MAY BE IMPOSED BY OTHER GOVERNMENT AGENCIES (LOCAL, STATE, AND FEDERAL) WHICH MAY HAVE JURISDICTION. VIOLATIONS WILL RESULT IN THE OWASA PROJECT APPROVAL BEING RESCINDED.
3. PRECONSTRUCTION CONFERENCE - A PRECONSTRUCTION CONFERENCE WITH THE OWASA CONSTRUCTION INSPECTOR IS REQUIRED BEFORE BEGINNING ANY WATER OR SEWER UTILITY CONSTRUCTION.
4. CONTACT THE NATIONAL "CALL BEFORE YOU DIG" 811 AND OWASA (919-968-4421) AT LEAST 48 HOURS PRIOR TO ANY EXCAVATION OR CONSTRUCTION TO HAVE EXISTING UTILITIES LOCATED.
5. EXISTING WATER VALVES SHALL BE OPERATED BY OWASA PERSONNEL ONLY. VALVES THAT SEPARATE PURITY APPROVED WATER AND UNAPPROVED WATER ARE TO REMAIN CLOSED AT ALL TIMES. VALVES MAY BE TEMPORARILY OPENED FOR LOADING AND FLUSHING BY THE OWASA INSPECTOR ONLY.
6. DISCHARGE FROM THIS PROJECT MUST BE IN COMPLIANCE WITH THE OWASA SEWER USE ORDINANCE. A GREASE INTERCEPTOR SHALL BE PROVIDED WHEN IN THE OPINION OF OWASA IT IS NECESSARY FOR THE PROPER HANDLING OF WASTEWATER CONTAINING EXCESSIVE AMOUNTS OF GREASE. ALL INTERCEPTION UNITS MUST BE OF THE TYPE AND CAPACITY WHICH IS CERTIFIED BY THE PROJECT ENGINEER AS MEETING THE REQUIREMENTS OF OWASA.
7. BACKFLOW PREVENTION WILL BE REQUIRED IN ACCORDANCE WITH OWASA CROSS-CONNECTION CONTROL ORDINANCE AND MANUAL.
8. ALL IRRIGATION SYSTEMS SHALL BE EQUIPPED WITH AUTOMATIC CONTROLLERS THAT ACTIVATE THE SYSTEM ACCORDING TO A DESIRED FREQUENCY AND DURATION, AND SHALL ALSO BE EQUIPPED WITH RAIN OR SOIL MOISTURE SENSORS THAT WILL PREVENT IRRIGATION DURING PERIODS OF RAINFALL OR WHEN THERE IS SUFFICIENT MOISTURE IN THE GROUND FOR PLANT HEALTH AND SURVIVAL IN ACCORDANCE WITH THE LOCAL GOVERNMENT WATER CONSERVATION ORDINANCES.
9. FIRE PROTECTION SYSTEMS - PRESSURE TESTING, CHLORINATION, AND PURITY TESTING SHALL BE COMPLETED BEFORE THE INSTALLATION OF THE RPDA UNIT.
10. REMOTE READ-OUT DEVICES - RPDA AND RPZ DEVICES LOCATED INSIDE BUILDINGS MUST BE EQUIPPED WITH AN AMR METER TO BE PURCHASED FROM OWASA AND INSTALLED BY THE CONTRACTOR. CONTACT THE OWASA CONSTRUCTION INSPECTOR FOR DETAILS OF THIS INSTALLATION.
11. EXISTING UTILITIES AND STRUCTURES SHOWN, BOTH UNDERGROUND AND ABOVE GROUND, ARE BASED ON A FIELD SURVEY AND THE BEST AVAILABLE RECORD DRAWINGS. THE CONTRACTOR SHALL VERIFY FIELD CONDITIONS PRIOR TO BEGINNING RELATED CONSTRUCTION. ANY DISCREPANCIES SHALL BE REPORTED TO THE OWNER'S REPRESENTATIVE IMMEDIATELY.
12. FOR FIRE SERVICE BACKFLOW DEVICES, USE A DOUBLE CHECK DETECTOR, REDUCED PRESSURE ZONE ASSEMBLY MEETING THE STANDARDS OF THE OWASA CROSS CONNECTION CONTROL ORDINANCE AND MANUAL. INSTALL THE DEVICE IN AN INTERNAL RISER ROOM WITH EXTERNAL ACCESS. CLEARANCES SHALL COMPLY WITH THE OWASA CROSS CONNECTION CONTROL ORDINANCES AND MANUAL. ACCESS TO RISER ROOM FROM OUTSIDE SHALL INCLUDE PROVISIONS FOR THE FIRE DEPARTMENT SUCH AS A KNOX BOX.
13. ALL BUILDINGS WITH FIRE SUPPRESSION LINES MUST HAVE BACKFLOW DEVICES LOCATED INSIDE THE BUILDING IN AN ABOVE GROUND LOCATION ADJACENT TO THE OUTSIDE WALL WHERE THE WATERLINE ENTERS THE BUILDING.
14. FOR IRRIGATION SERVICE BACKFLOW DEVICE, USE A REDUCED PRESSURE ZONE ASSEMBLY MEETING THE STANDARDS OF THE OWASA CROSS CONNECTION CONTROL ORDINANCE AND MANUAL. INSTALL THE IRRIGATION DEVICE INSIDE AN ABOVE GROUND HOT BOX.
15. UNDERGROUND VAULTS FOR METERS SHALL BE DRAINED BY PVC SCHEDULE 40 DRAINS TO DAYLIGHT INTO SLOPES. CONNECTION TO STORM STRUCTURES IS ONLY ALLOWED WITH THE CONSENT OF OWASA. DRAIN SIZES SHALL BE 2" DIAMETER FOR METERS OR DEVICES LESS THAN 2" AND SHALL BE 4" DIAMETER FOR DEVICES OVER 2".
16. SANITARY SEWER SERVICES SHALL BE SCHEDULE 40 PVC. CLEANOUTS SHALL BE SPACED NO MORE THAN 75 FEET APART. CLEANOUTS LOCATED IN PAVEMENT AREAS SHALL BE HEAVY DUTY TRAFFIC RATED CONSTRUCTION.
17. CONNECTION OF SANITARY SEWER SERVICE TO AN EXISTING MANHOLE SHALL COMPLY WITH OWASA STANDARDS, INCLUDING: CORE DRILL FOR OPENING INTO MANHOLE AND INSTALL WITH FLEXIBLE BOOT. IF PAVEMENT CUT IS REQUIRED, CONTRACTOR SHALL PATCH PAVEMENT IN ACCORDANCE WITH TOWN OF CHAPEL HILL STANDARDS AND PROVIDER GEOTECHNICAL INFORMATION FOR SOIL LOADING.
18. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE LOCATION AND/OR RELOCATION OF ALL EXISTING UTILITIES IN COORDINATION WITH THE APPROPRIATE UTILITY, AGENCY, OR COMPANY.



THE JOHN R. MCADAMS COMPANY, INC.  
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REVISIONS:

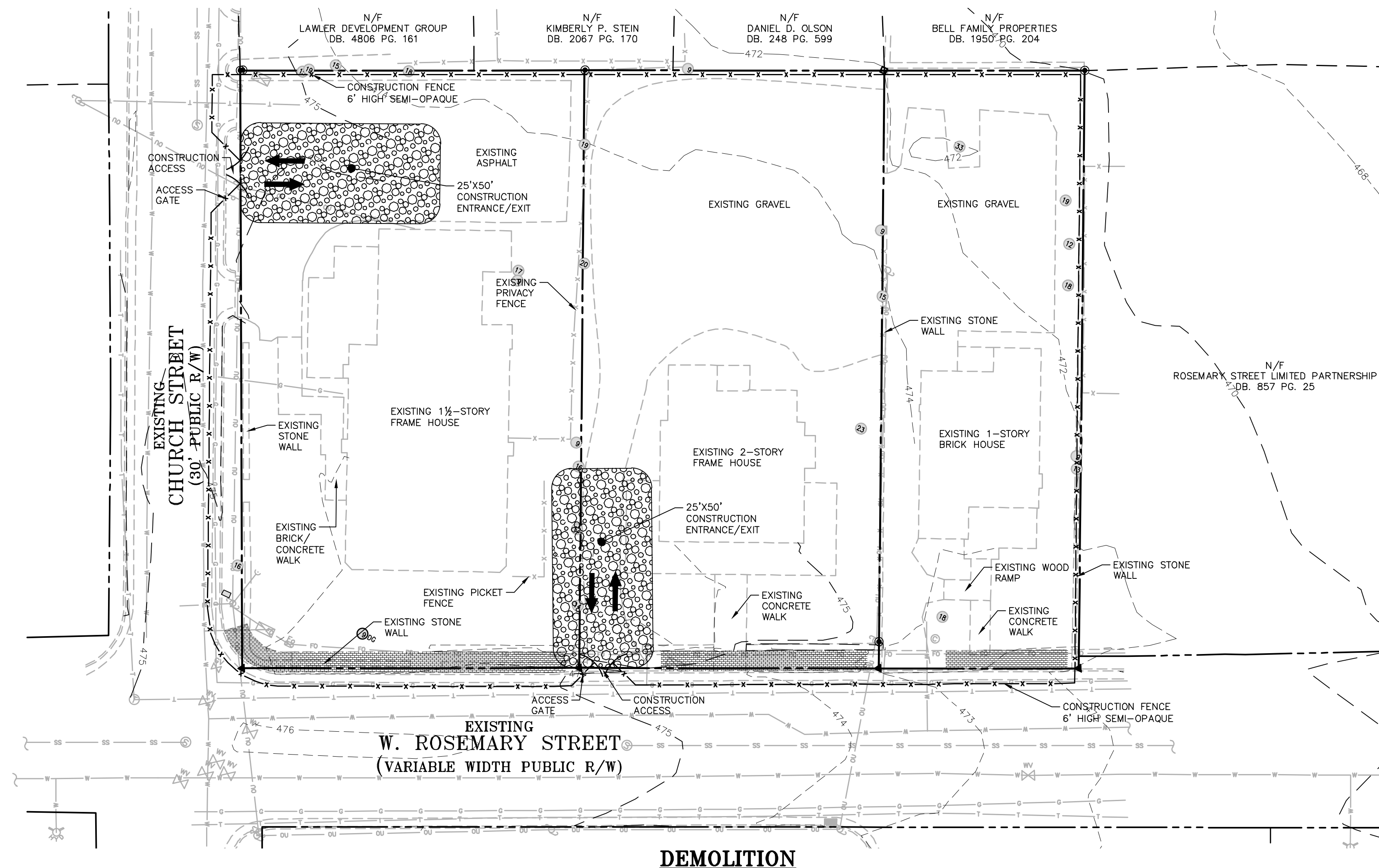
1	2015-02-27	TOWN OF CHAPEL HILL COMMENTS
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DEVELOPER:  
OTO DEVELOPMENT, LLC  
100 DUNBAR ST., SUITE 402  
SPARTANBURG, SC 29306  
PHONE: 864-596-8930

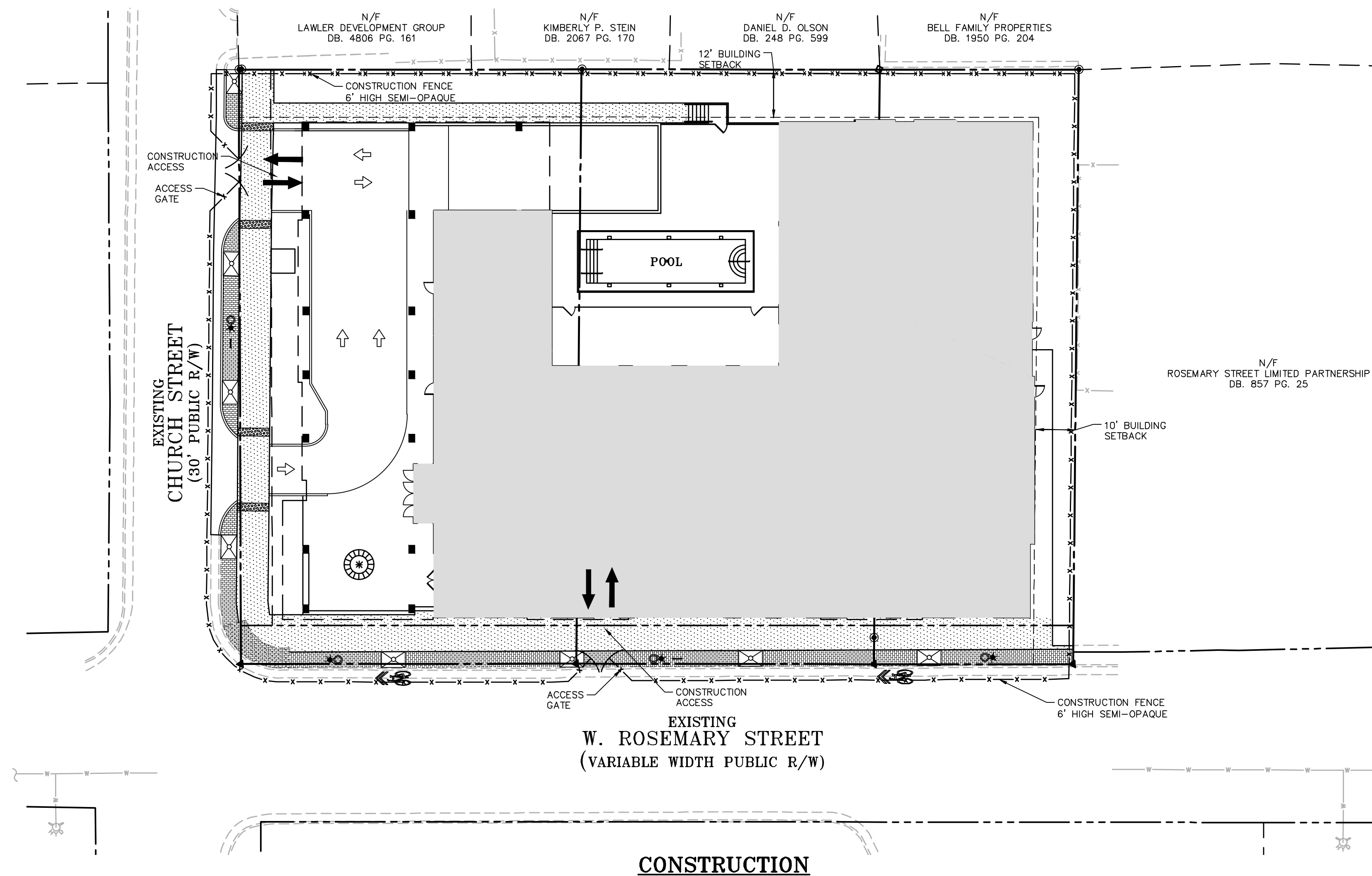
**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA

PROJECT NO.	OTO-14000
FILENAME	OTO14000-U1
DESIGNED BY	-
DRAWN BY	KWH
SCALE	1"=20'
DATE	01-26-2015
SHEET NO.	C-6





**DEMOLITION**



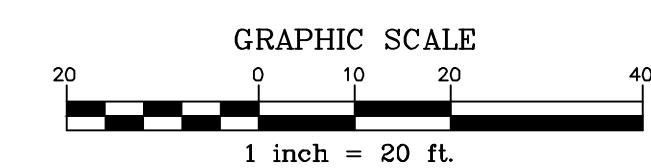
**CONSTRUCTION**

**SOLID WASTE NOTES:**

1. ALL EXISTING STRUCTURES 500 SQUARE FEET AND LARGER IN SIZE SHALL BE ASSESSED PRIOR TO DEMOLITION TO ENSURE COMPLIANCE WITH THE COUNTY'S REGULATED RECYCLABLE MATERIALS ORDINANCE (RMO) AND TO ASSESS THE POTENTIAL FOR DE-CONSTRUCTION AND/OR THE RE-USE OF SALVAGEABLE MATERIALS.
2. BY ORANGE COUNTY ORDINANCE, CLEAN WOOD WASTE, SCRAP METAL, AND CORRUGATED CARDBOARD PRESENT IN CONSTRUCTION OR DEMOLITION WASTE MUST BE RECYCLED.
3. BY ORANGE COUNTY ORDINANCE, ALL HAULERS OF MIXED CONSTRUCTION AND DEMOLITION WASTE THAT INCLUDES ANY REGULATED RECYCLABLE MATERIALS SHALL BE LICENSED BY ORANGE COUNTY.
4. PRIOR TO ANY DEMOLITION OR CONSTRUCTION ACTIVITY ON THE SITE THE APPLICANT SHALL HOLD A PRE-DEMOLITION/PRE-CONSTRUCTION CONFERENCE WITH THE SOLID WASTE STAFF (919-996-2788). THIS MAY BE THE SAME PRE-CONSTRUCTION MEETING HELD WITH OTHER DEVELOPMENT/ENFORCEMENT OFFICIALS.
5. THE PRESENCE OF ANY ASBESTOS CONTAINING MATERIALS (ACM) AND/OR OTHER HAZARDOUS MATERIALS IN CONSTRUCTION AND DEMOLITION WASTE SHALL BE HANDLED IN ACCORDANCE WITH ANY AND ALL LOCAL, STATE, AND FEDERAL REGULATIONS AND GUIDELINES.
6. IF ANY VEHICLES ARE PARKED IN THE REFUSE OR RECYCLABLES COLLECTION VEHICLE ACCESS AREA, THE CONTAINERS WILL NOT RECEIVE SERVICE UNTIL THE NEXT SCHEDULED COLLECTION DAY.
7. MIXED RECYCLING WILL BE IN ROLL OUT CARTS TO BE COLLECTED BY PRIVATE WASTE COLLECTION CONTRACTOR. OWNER ACKNOWLEDGES THAT RIGHTS ARE BEING WAIVED FOR PUBLIC COLLECTION OF MIXED RECYCLABLES BY ORANGE COUNTY AND THAT PAYMENT OF ANNUAL FEE WITH REAL ESTATE TAXES WILL STILL BE REQUIRED.
8. TRASH AND CARDBOARD WILL BE IN ROLL OUT CARTS TO BE COLLECTED BY A PRIVATE WASTE COLLECTION CONTRACTOR.
9. ALL CONSTRUCTION WASTE MATERIALS SHALL BE RECYCLED WHENEVER POSSIBLE. CONTRACTORS SHALL RETAIN PICKUP SLIPS TO RECORD/VALIDATE RECYCLING AS MAY BE REQUIRED FOR ENVIRONMENTAL CREDITS.
10. TRASH AND RECYCLE COLLECTION INCLUDES 5 - 96 GALLON ROLL OUT CARTS, 2 FOR TRASH AND 3 FOR RECYCLING.
11. THE OWNER, OR THEIR DESIGNEE, SHALL BE RESPONSIBLE FOR MAINTAINING A FIRE WATCH DURING CONSTRUCTION AND DEMOLITION WHERE MATERIALS SUBJECT TO SPONTANEOUS COMBUSTION OR OTHER HAZARDOUS CONSTRUCTION OR DEMOLITION IS OCCURRING.
12. ALL CONSTRUCTION AND DEMOLITION SHALL BE CONDUCTED IN COMPLIANCE WITH THE CURRENT EDITION OF CHAPTER 14 OF THE NC FPC.

**CONSTRUCTION MANAGEMENT NOTES:**

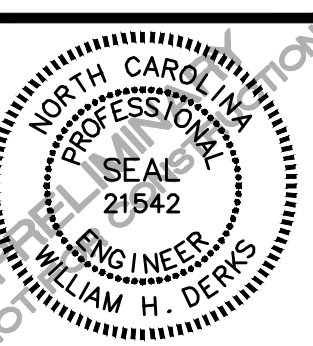
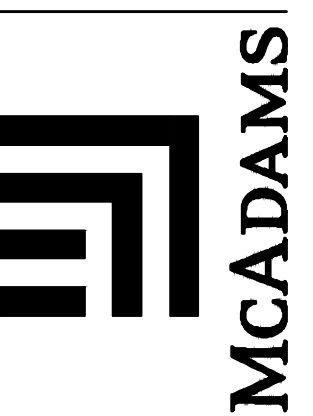
1. ALL LARGE TRUCKS AS RELATED TO VEHICULAR CONSTRUCTION TRAFFIC SHALL ENTER/LEAVE THE SITE THROUGH THE TEMPORARY CONSTRUCTION ENTRANCES & EXITS AS SHOWN ON THIS PLAN.
2. ENTRANCE AND EXIT GATES SHALL BE CLOSED UNLESS ACTIVELY BEING USED FOR ENTERING AND EXISTING VEHICLES.
3. ONSITE CONSTRUCTION MANAGER PARKING FOR SMALL CONSTRUCTION VEHICLES AND MATERIAL STAGING CAN OCCUR ON-SITE DURING EARLY STAGES OF CONSTRUCTION. ADDITIONAL CONTRACTOR PARKING AREA TO BE PROVIDED OFF-SITE.
4. CONSTRUCTION WILL BE LIMITED TO BETWEEN 7:00 AM AND 6:00 PM MONDAY THROUGH FRIDAY.
5. THE CONTRACTOR SHALL PROVIDE WORK ZONE TRAFFIC CONTROL DEVICES -- REFER TO THE ZONING COMPLIANCE PERMIT TRAFFIC AND PEDESTRIAN CONTROL PLAN FOR SPECIFIC NOTES AND DETAILS.
6. NO OPEN BURNING SHALL BE PERMITTED.
7. THE APPLICANT SHALL POST A CONSTRUCTION SIGN ON THE DEVELOPMENT SITE THAT LISTS THE FOLLOWING:
  - PROPERTY OWNER'S REPRESENTATIVE & TELEPHONE NUMBER
  - CONTRACTOR'S REPRESENTATIVE & TELEPHONE NUMBER
  - TELEPHONE NUMBER FOR REGULATORY INFO AT TIME OF BUILDING PERMIT
  - SIGN MAY BE A MAXIMUM OF 32 SF AND A MAXIMUM HEIGHT OF 8 FT.



PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION

X:\Projects\010-14000\Land\Site\Current Drawings\01014000-SWCM.dwg, 5/13/2015 12:39:02 PM, LeBlanc, Marc

THE JOHN R. MCADAMS COMPANY, INC.  
2905 Meridian Parkway  
Durham, North Carolina 27713  
Tel: C-0288  
Fax: C-0288  
(800) 733-5646 • mcadamsco.com



REVISIONS:

NO.	DATE	DESCRIPTION
1	2015-02-27	TOWN OF CHAPEL HILL COMMENTS

DEVELOPER:  
OTO DEVELOPMENT, LLC  
100 DUNBAR ST., SUITE 402  
SPARTANBURG, SC 29306  
PHONE: 864-596-8930

**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA

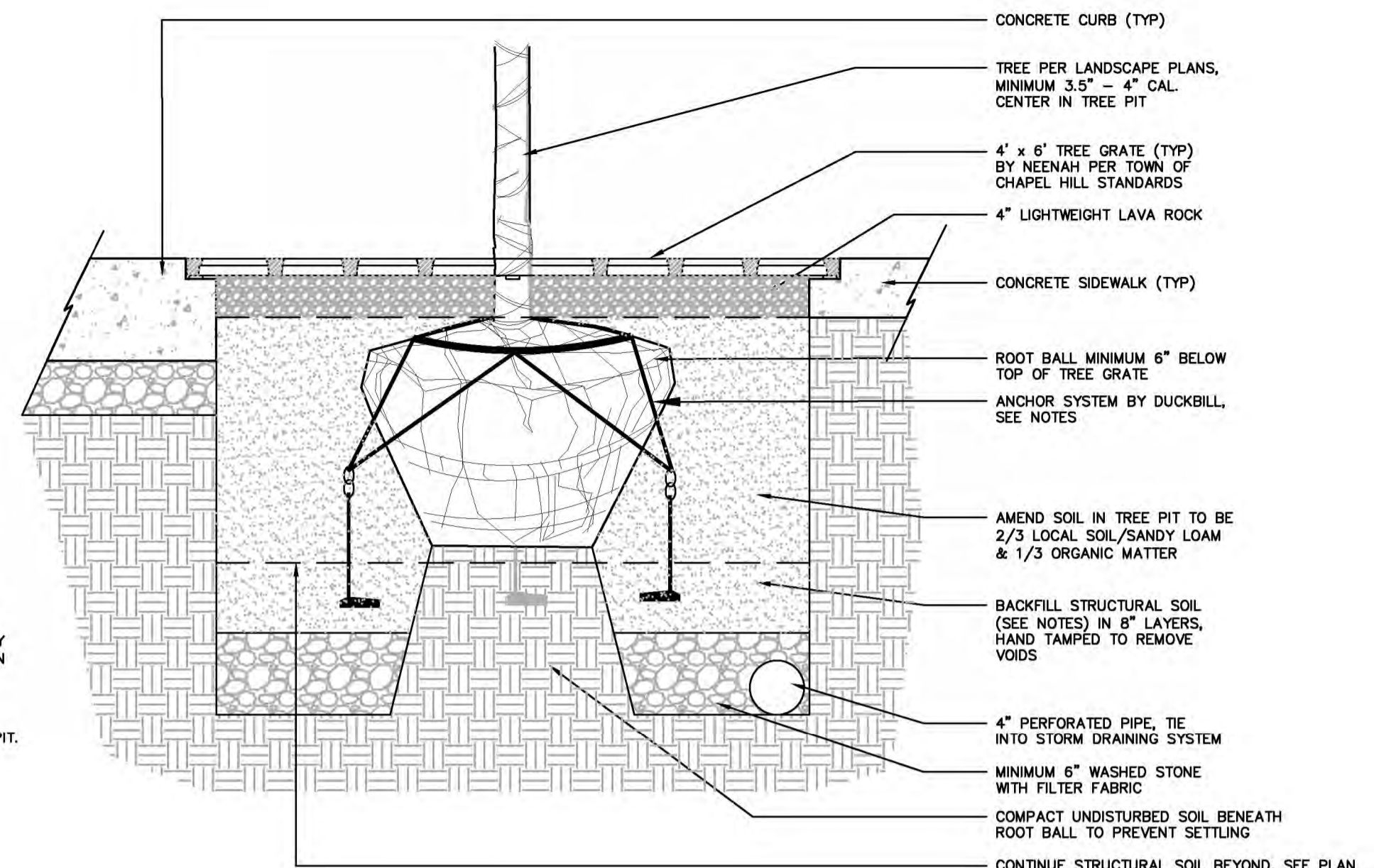
**SOLID WASTE AND CONSTRUCTION MANAGEMENT PLAN**

PROJECT NO.	OTO-14000
FILENAME	01014000-SWCM
DESIGNED BY	-
DRAWN BY	KWH
SCALE	1"=20'
DATE	01-26-2015
SHEET NO.	C-7

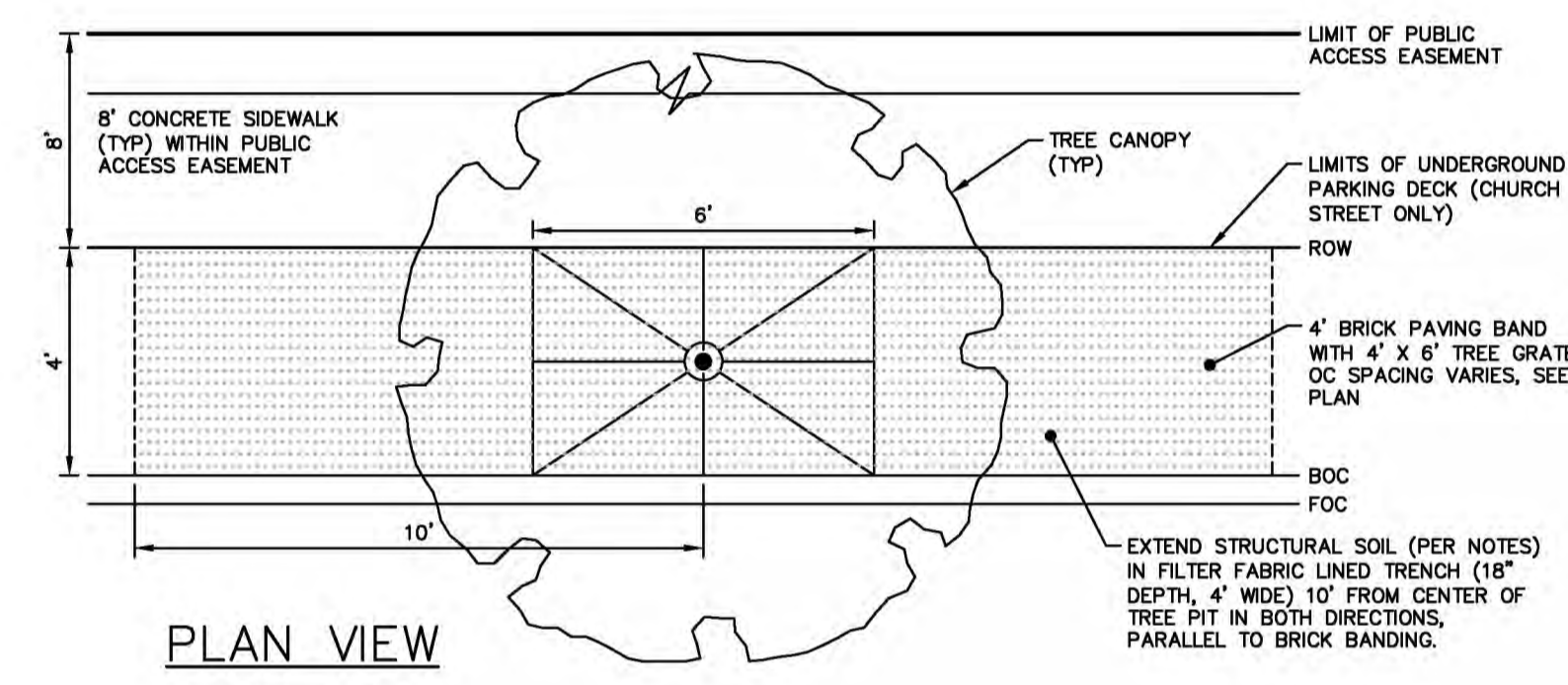




- NOTES:**
- ROOT BALL ANCHOR SYSTEM BY DUCKBILL 1-800-325-5360. SEE MANUFACTURERS SPECIFICATIONS FOR DIMENSIONS AND INSTALLATION INSTRUCTIONS.
  - STRUCTURAL SOIL SHALL BE 70% STAUTE EXPANDED SLATE AND 30% SANDY LOAM.
  - CUT BURLAP, ROPE AND WIRE BASKET AWAY FROM TOP 1/2 OF ROOT BALL, ROOT CROWN SHALL BE EXPOSED.
  - DO NOT WRAP TREE TRUNK.
  - SCARIFY SIDES AND BOTTOM OF PLANTING PIT.

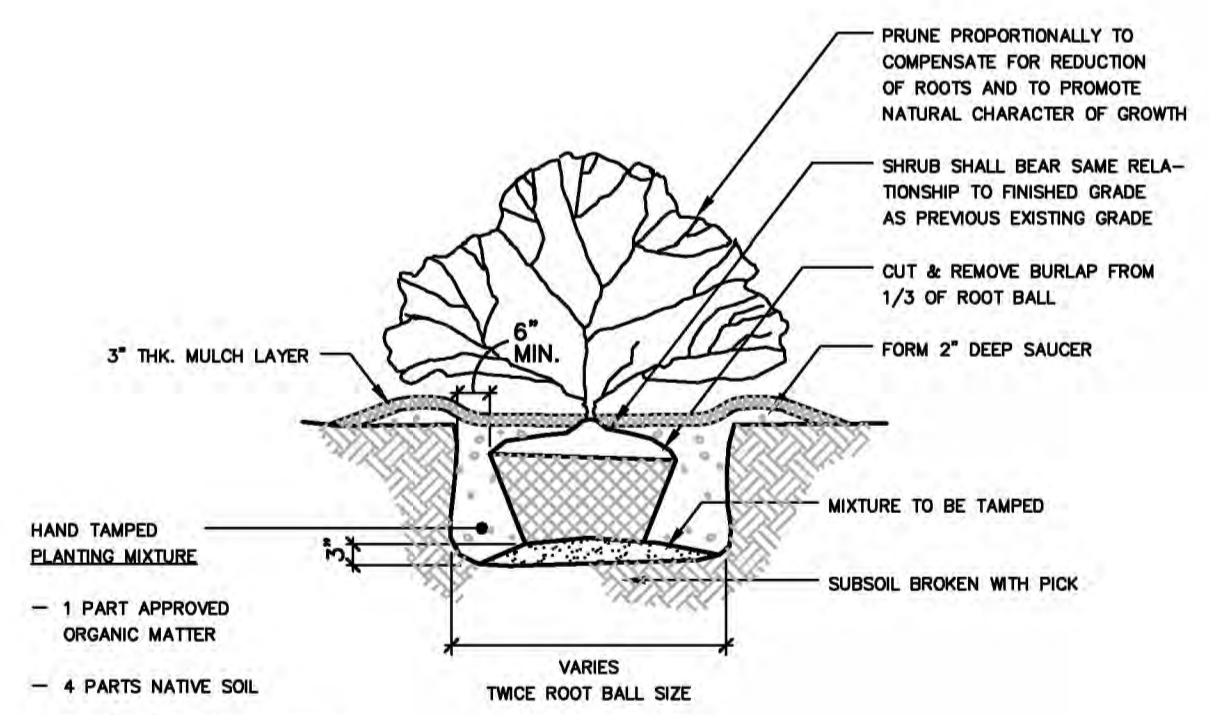


SECTION

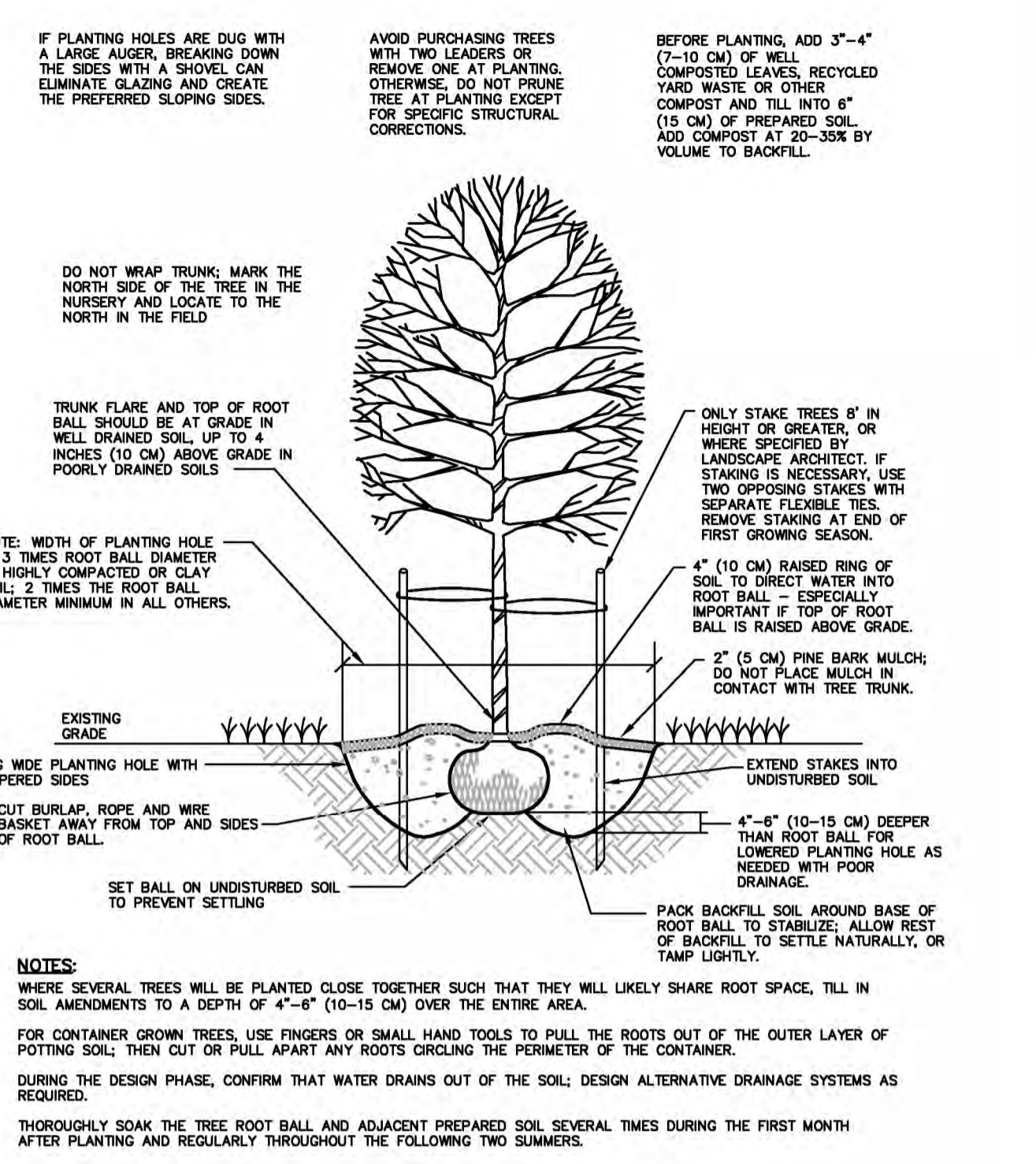


PLAN VIEW

TREE GRATE DETAIL  
N.T.S.



SHRUB INSTALLATION DETAIL  
N.T.S.



TREE INSTALLATION DETAIL  
N.T.S.

**PLANT SCHEDULE**

TREES	QTY	COMMON NAME	BOTANICAL NAME	CALL	HGT
AG	9	'Autumn Brilliance' Serviceberry	Amelanchier x grandiflora 'Autumn Brilliance'	-	8'
CV	7	White Fringetree	Chionanthus virginicus	2"	3.5"-4" Min.
CB	4	Maidenhair Tree	Ginkgo biloba 'Autumn Gold' TM	3.5"-4" Min.	3.5"-4" Min.
UP	4	Allee Lacebark Elm	Ulmus parvifolia 'Allee'		
SHRUBS	QTY	COMMON NAME	BOTANICAL NAME	HGT	
ICC	6	Dwarf Japanese Holly	Ilex crenata 'Compacta'	18"	

**BUFFER REQUIREMENTS:**

**CURRENT DESIGN MANUAL:**  
REQUIRED PLANTS PER 100 LINEAR FEET

BUFFER TYPE	MINIMUM WIDTH 'C'	LOCATION	LARGE TREES	SMALL TREES	SHRUBS
INTERNAL	20'	INTERNAL	4	8	30

**PROPOSED ALTERNATE BUFFER/PLANTING MODIFICATIONS:**  
PROPOSED PLANTS PER 100 LINEAR FEET

BUFFER TYPE	MINIMUM WIDTH	LOCATION	LARGE TREES	SMALL TREES	SHRUBS
ALTERNATE	VARIABLE	INTERNAL	40	17	20

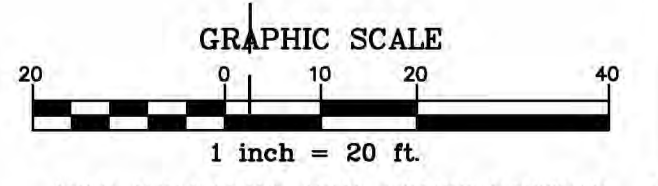
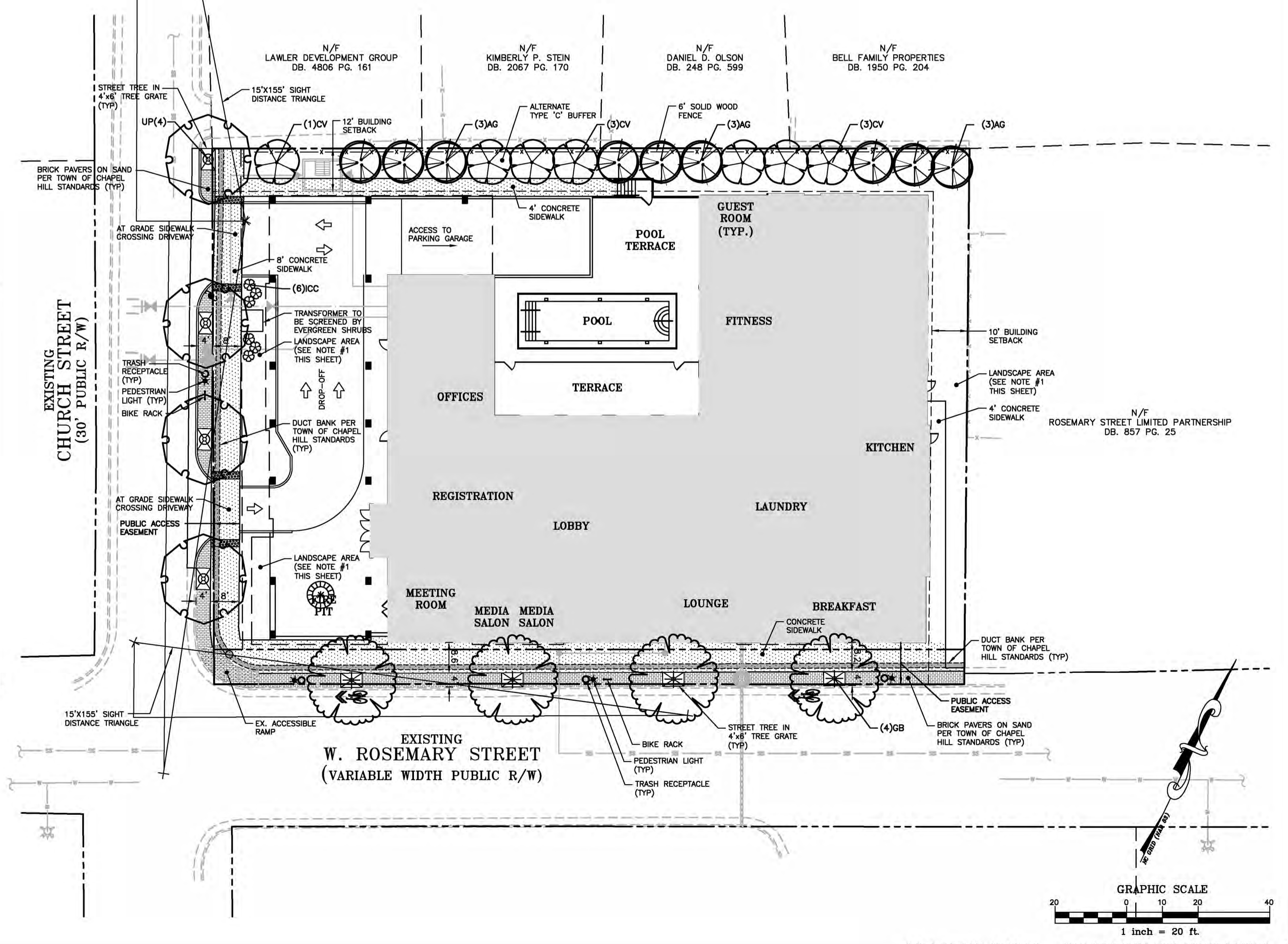
\*PROPOSED PLANTING MODIFICATIONS INCORPORATE A 6' SOLID WOOD FENCE.

**SCHEDULE OF PROPOSED STREETScape FURNISHINGS:**

- TREE GRATES:**  
NEENAH FOUNDRY, METROPOLITAN SERIES (MODEL # R-8815-1)
- TRASH RECEPTACLES:**  
VICTOR STANLEY IRONSITES SERIES S-424 36 GALLON CAPACITY WITH STANDARD LID VICTOR STANLEY GREEN
- BIKE RACK:**  
SUNSHINE U-LOK CORP MODEL: "SIMPLE-LOK" SINGLE LOOP INVERTED "U" BIKE RACK, BLACK, IN-GROUND INSTALLATION
- PEDESTRIAN LIGHTS:**  
UNION METAL 15" CAST ALUMINUM OCTAFUTE POLE #68 BASE "DENVER" WITH 3" TAPERED STEEL LUMINAIRE ARM AND A 100 WATT METAL HALIDE LUMEC DOMUS 50 FIXTURE OR COMPARABLE LED FIXTURE PAINT COLOR: RAL 6012

**LANDSCAPE NOTES:**

- LANDSCAPE BEDS TO BE PLANTED WITH ONE OR MORE OF THE FOLLOWING: SMALL TREES, EVERGREEN AND/OR DECIDUOUS SHRUBS, PERENNIALS, AND ANNUALS.



PRELIMINARY DRAWING - NOT RELEASED FOR CONSTRUCTION

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2605 Meridian Parkway  
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Tel: 919-428-8800  
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**REVISIONS:**

NO.	DATE	DESCRIPTION
1	2015-05-27	TOWN OF CHAPEL HILL COMMENTS

**DEVELOPER:**  
OTO DEVELOPMENT, LLC  
100 DUNBAR ST., SUITE 402  
SPARTANBURG, SC 29306  
PHONE: 864-596-8930

**AC BY MARRIOTT**  
CHAPEL HILL, NORTH CAROLINA  
PLANTING & STREETScape PLAN

PROJECT NO. OTO-14000  
FILENAME: 01014000-LS1  
DESIGNED BY: -  
DRAWN BY: KWH  
SCALE: 1"=20'  
DATE: 01-26-2015  
SHEET NO. **LS-1**







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overcash demmitt

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web.www.odarch.com

**AC**  
by  
Marriott

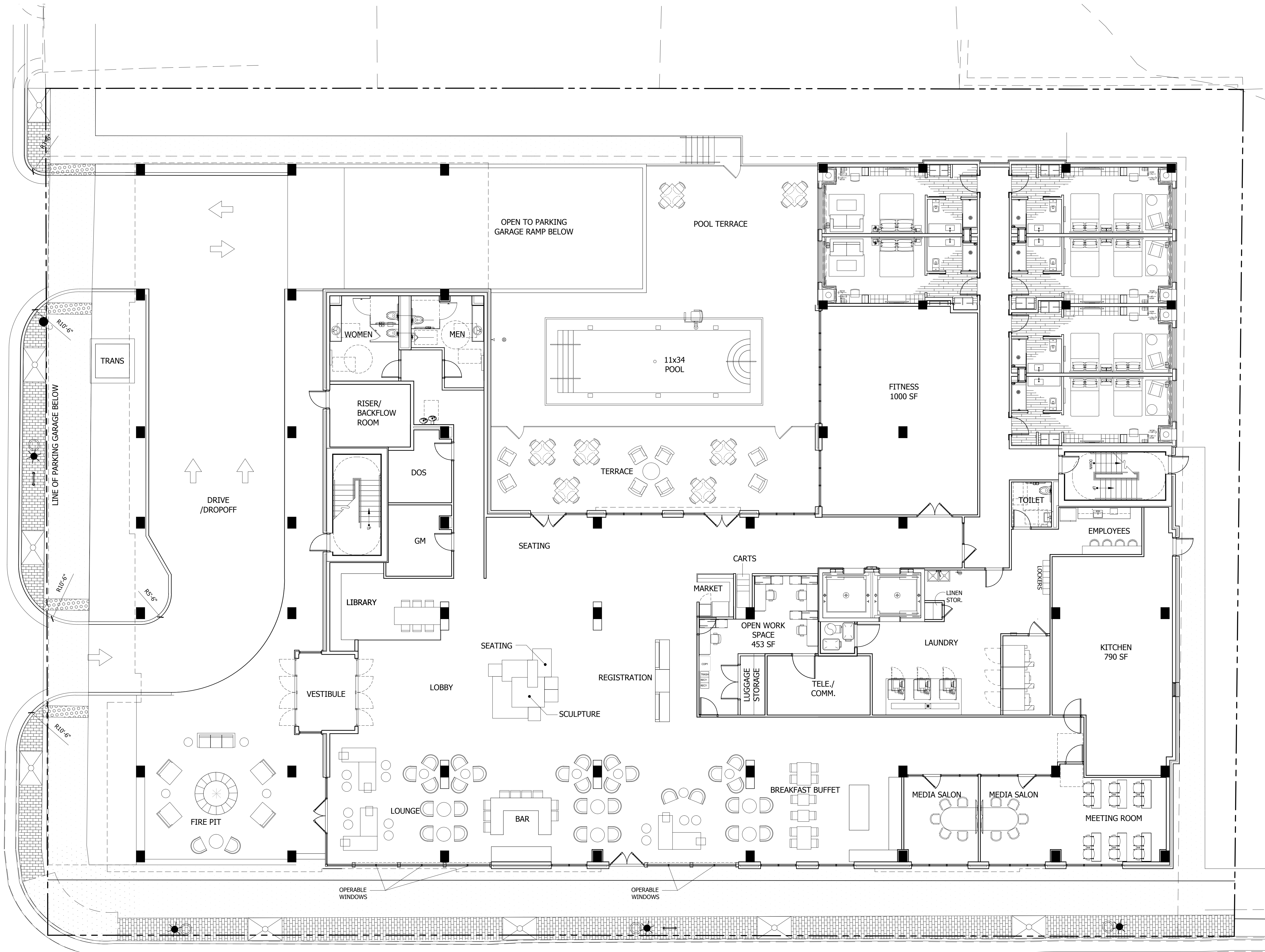
W. ROSEMARY ST. & CHURCH ST.  
CHAPEL HILL, North Carolina

ISSUE	DATE
SUP SUBMITTAL	01/26/15
TRT COMMENTS	03/20/15
ADVISORY BOARD	06/30/15
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**GROUND FLOOR PLAN**

**A1.1**

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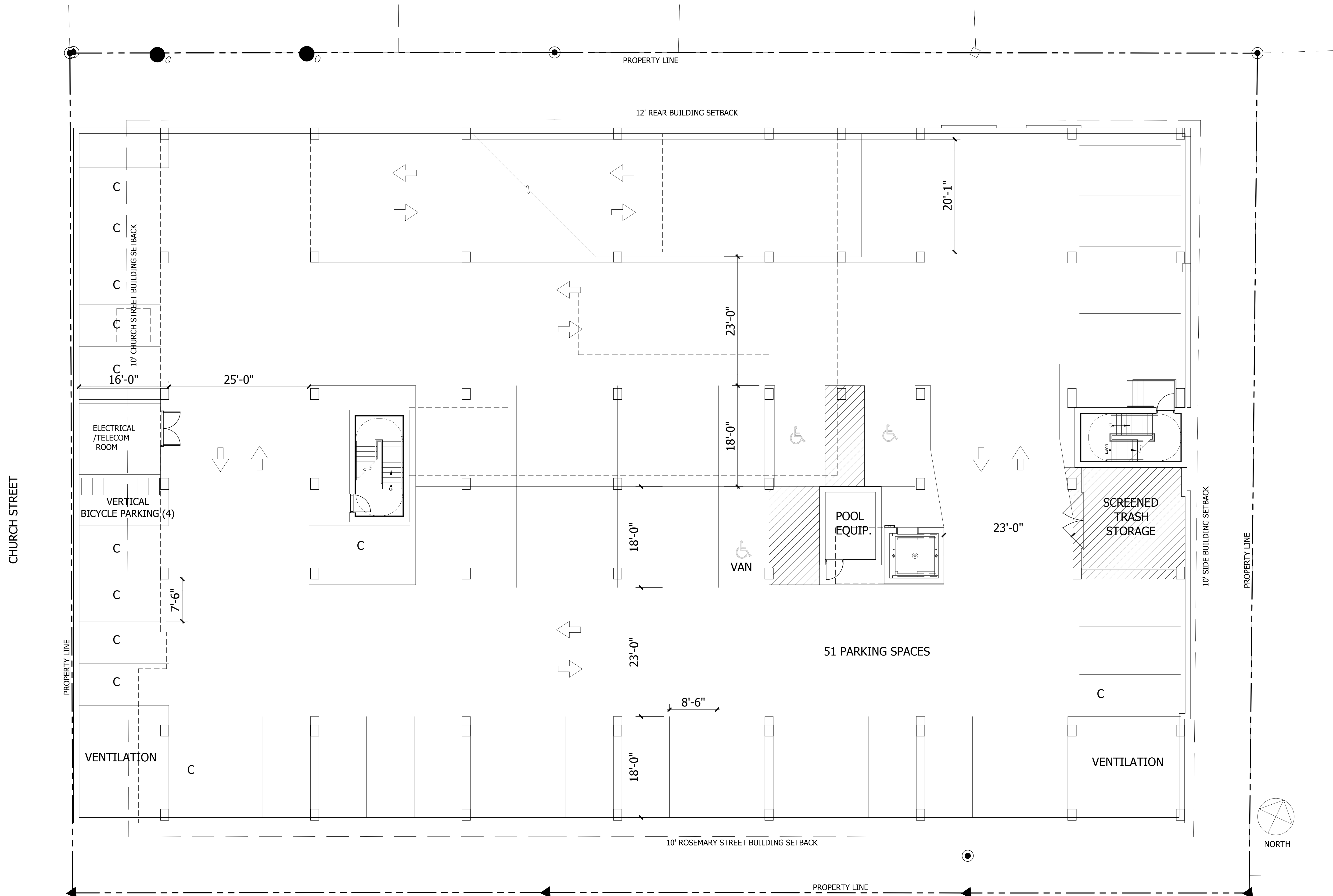
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CHAPEL HILL, North Carolina

ISSUE	DATE
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**PARKING GARAGE FLOOR PLAN**

**A1.1G**

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**PARKING COUNT:**

GARAGE LEVEL	STANDARD PARKING	HANDICAP PARKING	COMPACT PARKING	TOTAL VEHICLE PARKING	TOTAL BICYCLE PARKING
LEVEL 1	36	3	12	51	4
LEVEL 2	46	2	12	60	4
				111	8

**PARKING DIMENSIONS:**

LOCATION	STANDARD PARKING	COMPACT PARKING	MINIMUM AISLE WIDTH
AC Chapel Hill	W 8'-6" X D 18'-0"	W 7'-6" X D 16'-0"	23'-0"
Raleigh, NC	W 8'-6" X D 18'-0"	W 7'-6" X D 15'-0"	22'-0"
Durham, NC	W 8'-6" X D 18'-0"	W 7'-6" X D 14'-0"	24'-0"
Charlotte, NC	W 8'-6" X D 18'-0"	W 7'-6" X D 14'-0"	23'-0"
National Parking Assoc.	W 8'-6" X D 18'-0"	N/A	23'-0"





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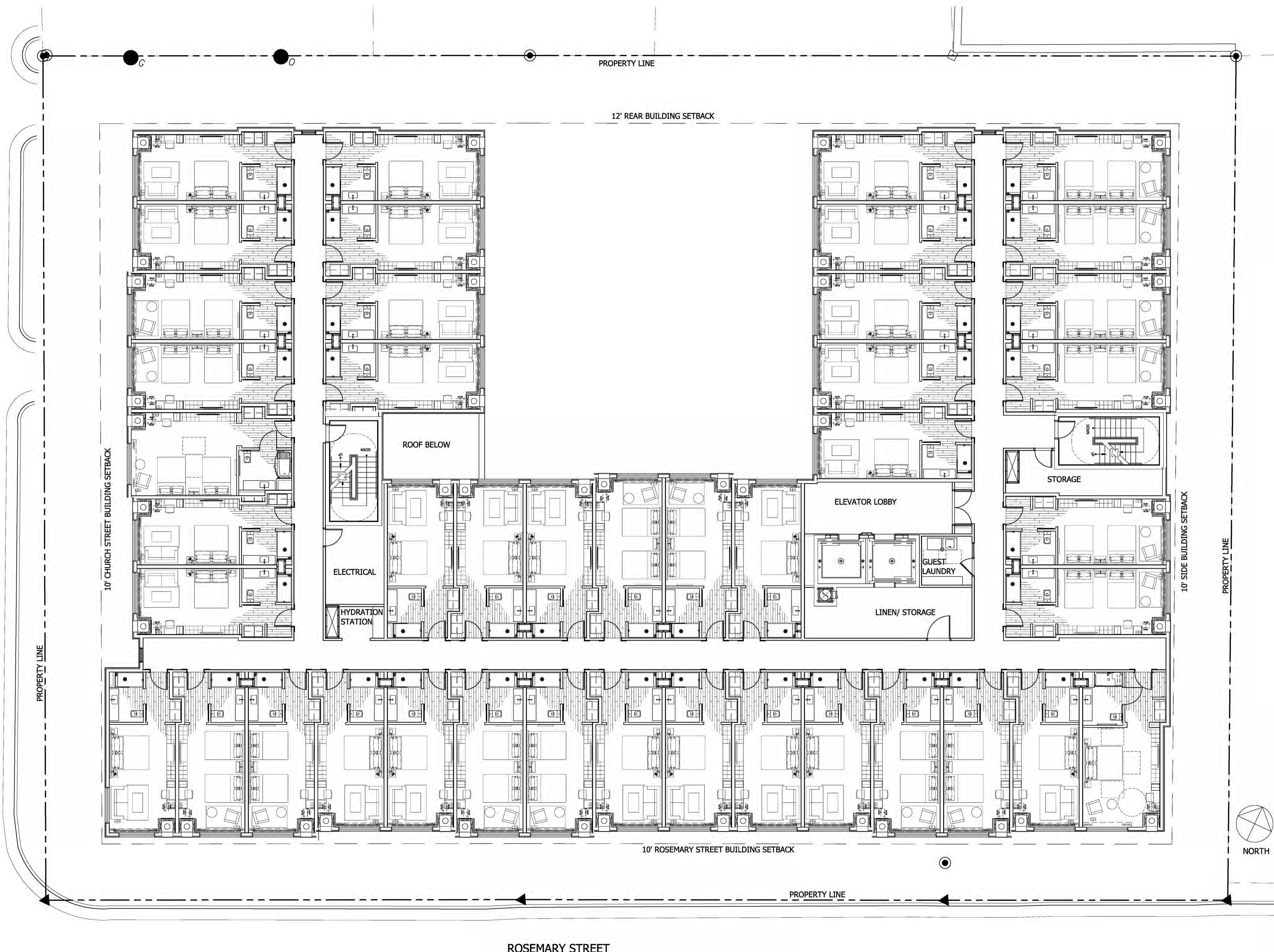
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CHAPEL HILL, North Carolina

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**SECOND FLOOR PLAN**

**A1.2**

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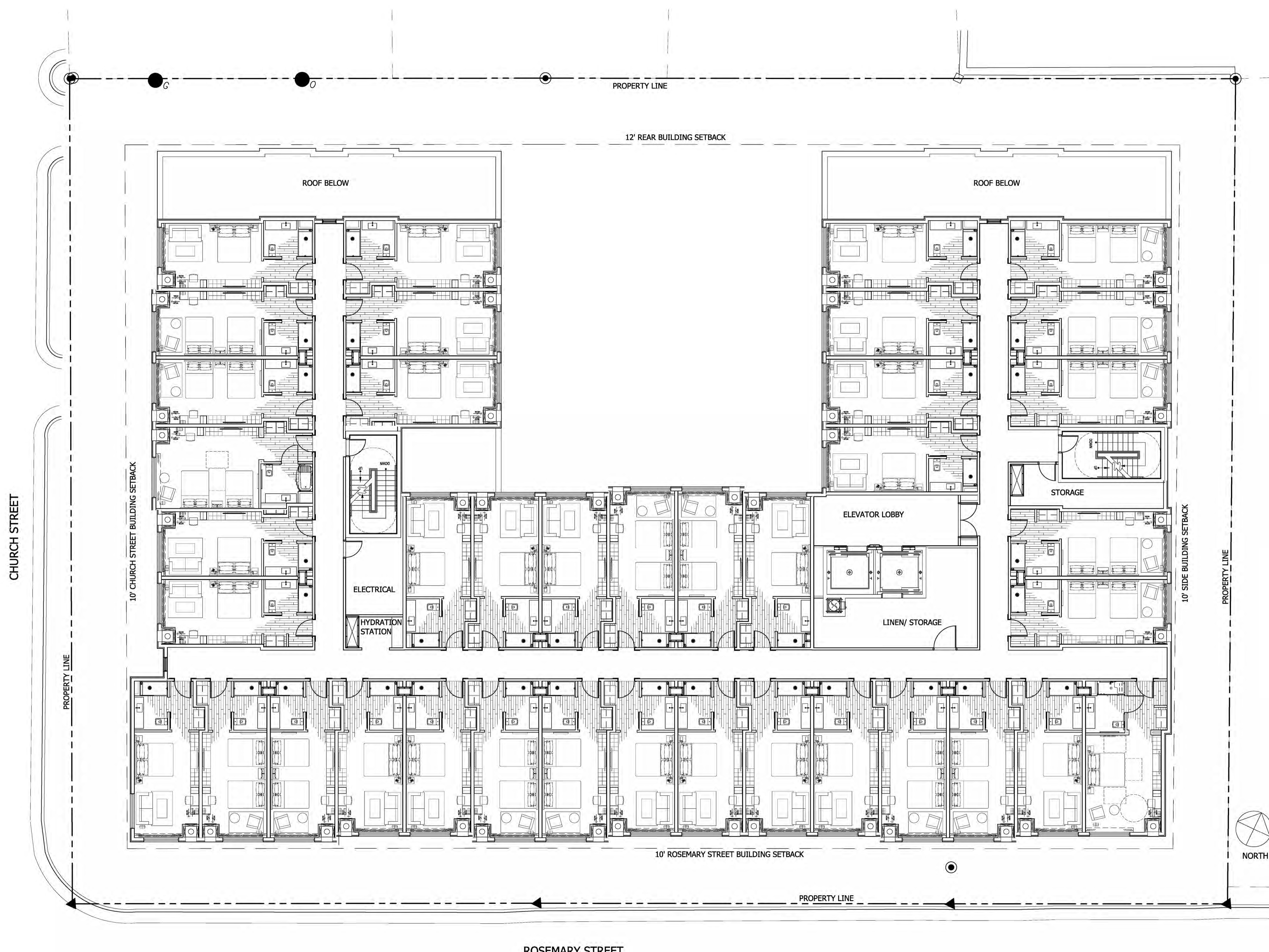
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**THIRD FLOOR PLAN**

**A1.3**

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**NOTES:**  
 APPROXIMATELY 10,000 SF OF GREEN ROOF TO BE INCLUDED. LOCATION TO BE DETERMINED BY ARCHITECT AND OWNER AT LATER DATE.



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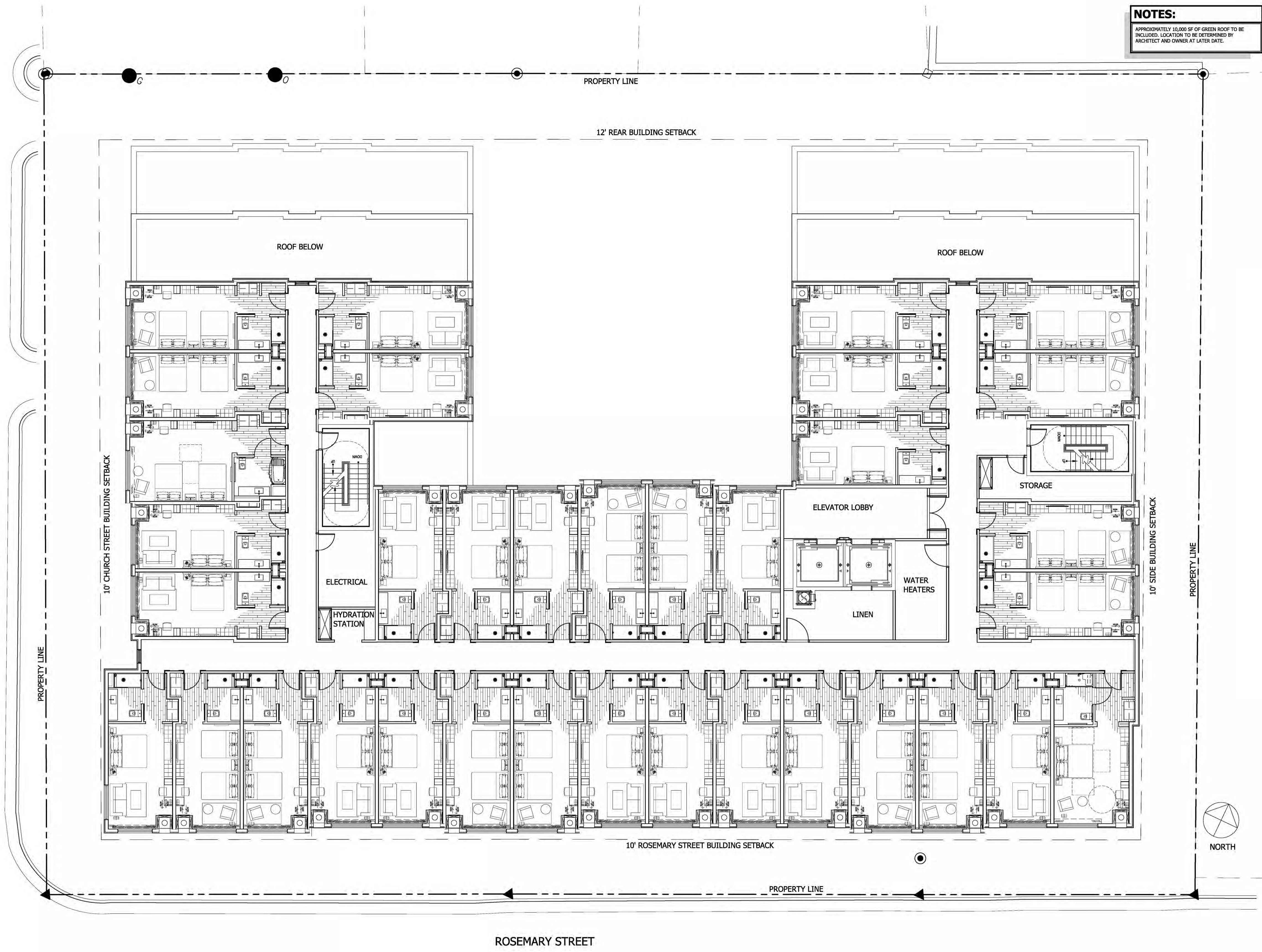
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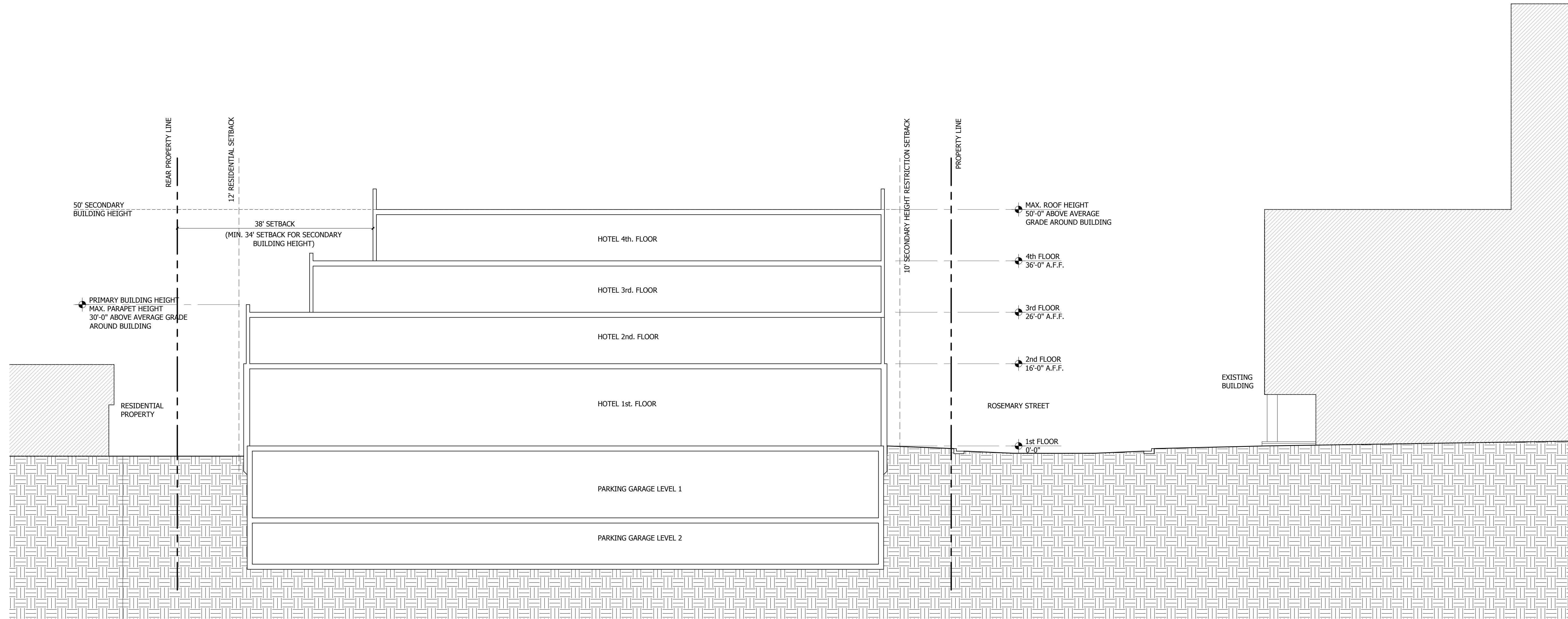
**FOURTH FLOOR PLAN**

**A1.4**

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 ODA No. 132711 draw/2711\_A1.1.dwg

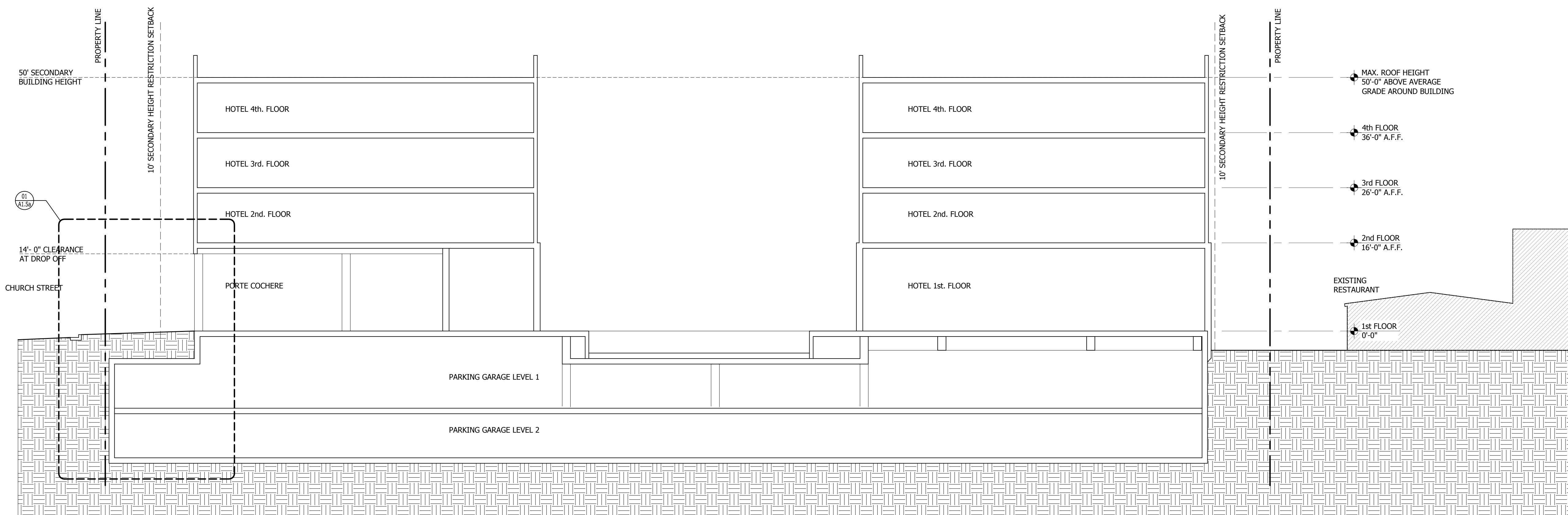






**02** BUILDING / SITE SECTION

scale: 3/32"=1'-0"



**01** BUILDING / SITE SECTION

scale: 3/32"=1'-0"



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charlotte north carolina 28203  
office .704.332.1615  
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Marriott

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CHAPEL HILL, North Carolina

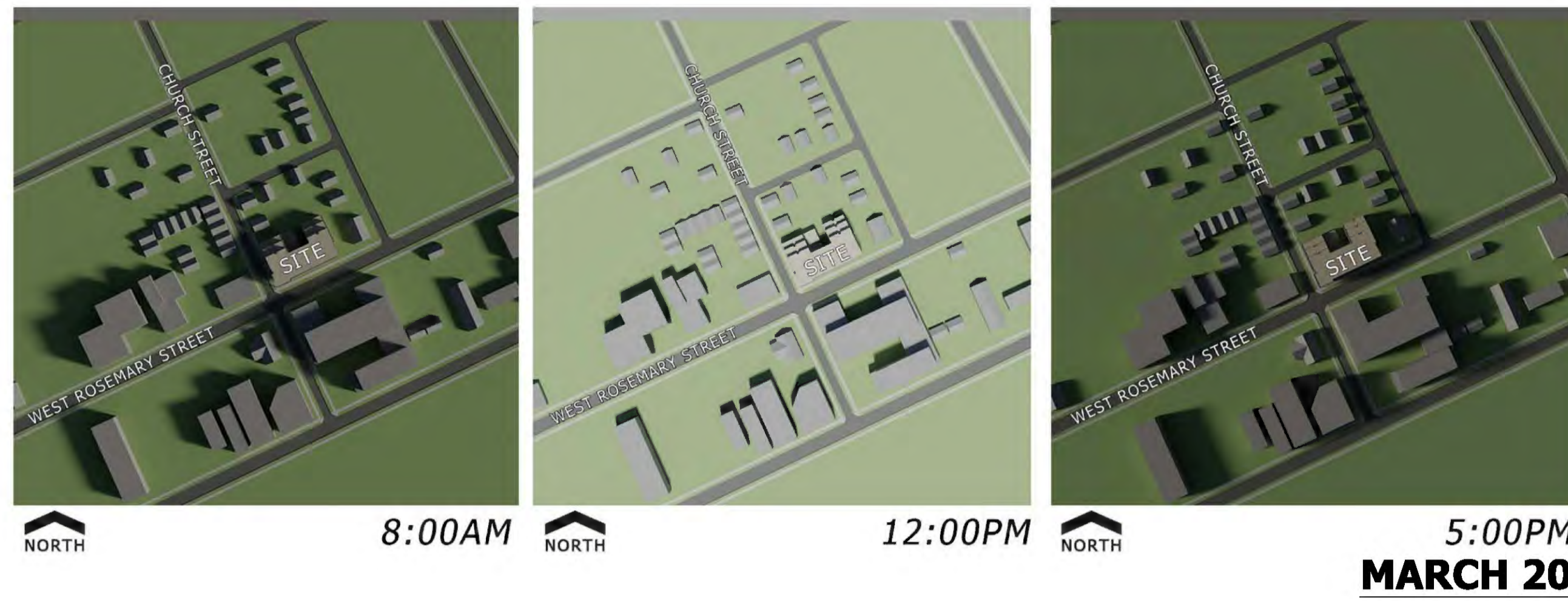
ISSUE	DATE
SUP SUBMITTAL	01/26/15
TRT COMMENTS	03/20/15
ADVISORY BOARD	6/30/15
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**BUILDING/  
SITE SECTIONS**

**A1.5**

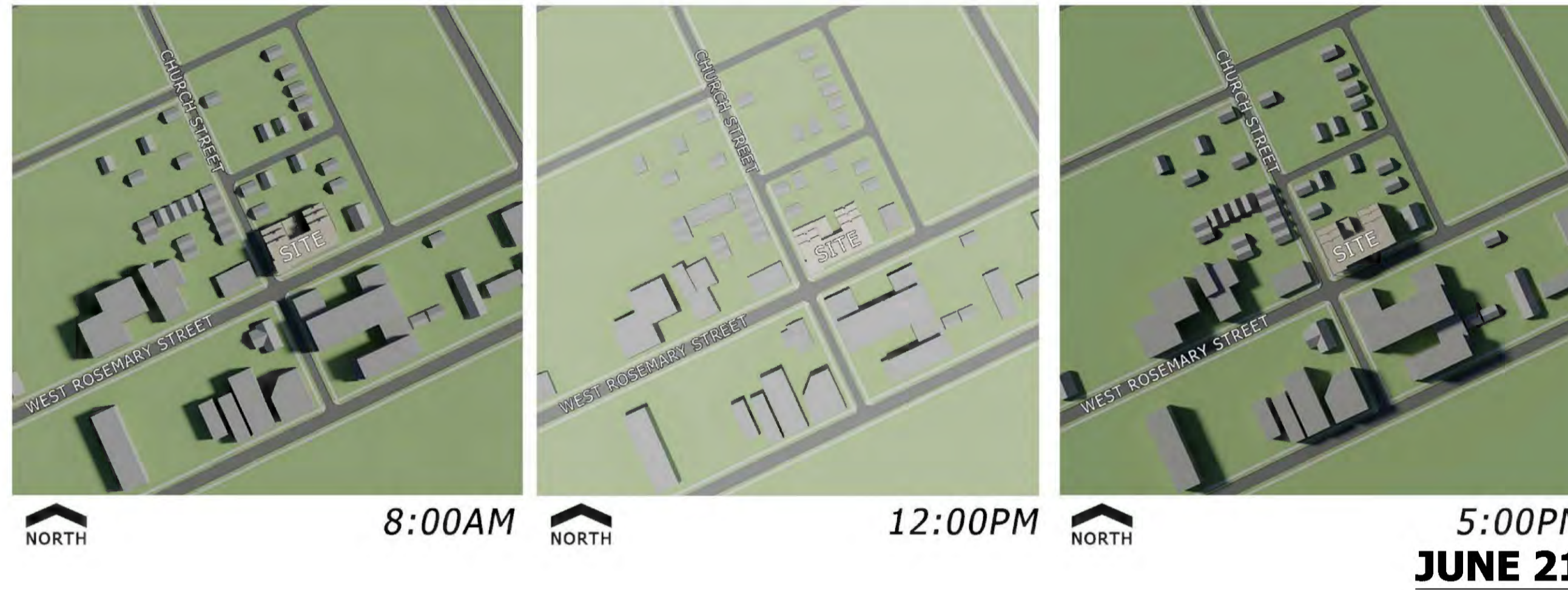
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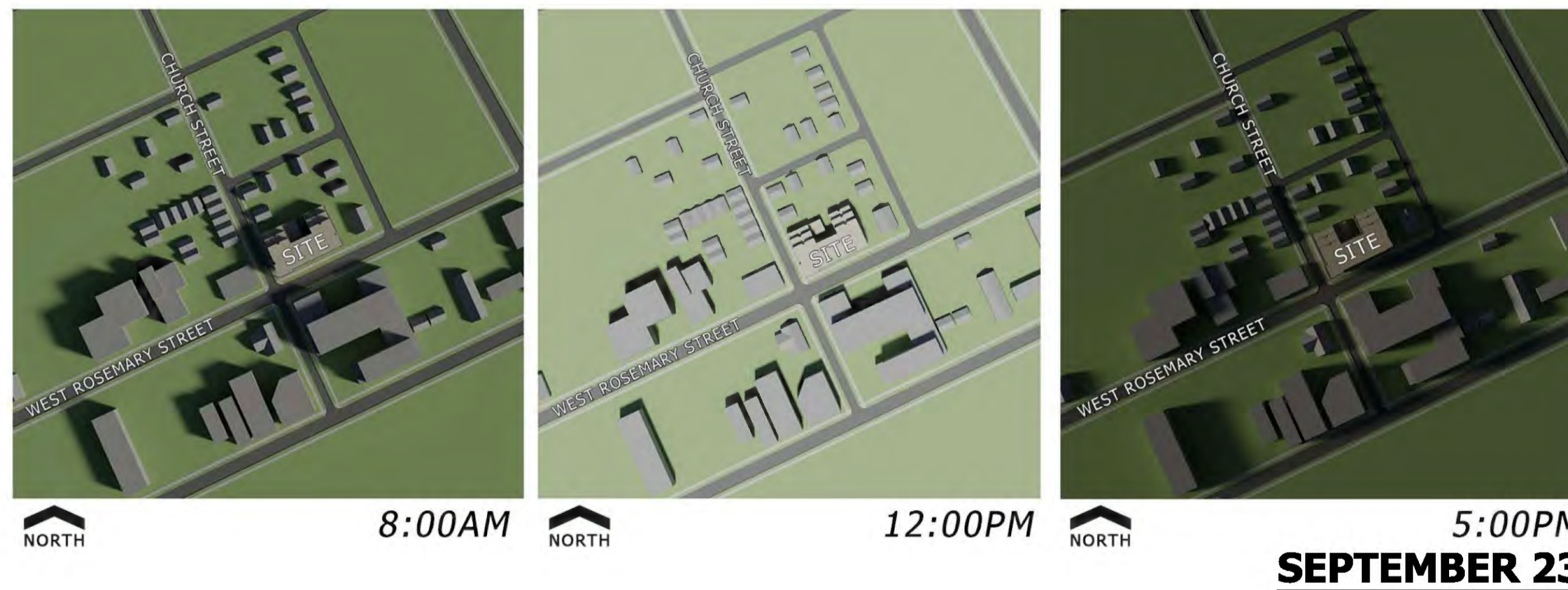
**02** SITE SUN STUDY

scale: NTS



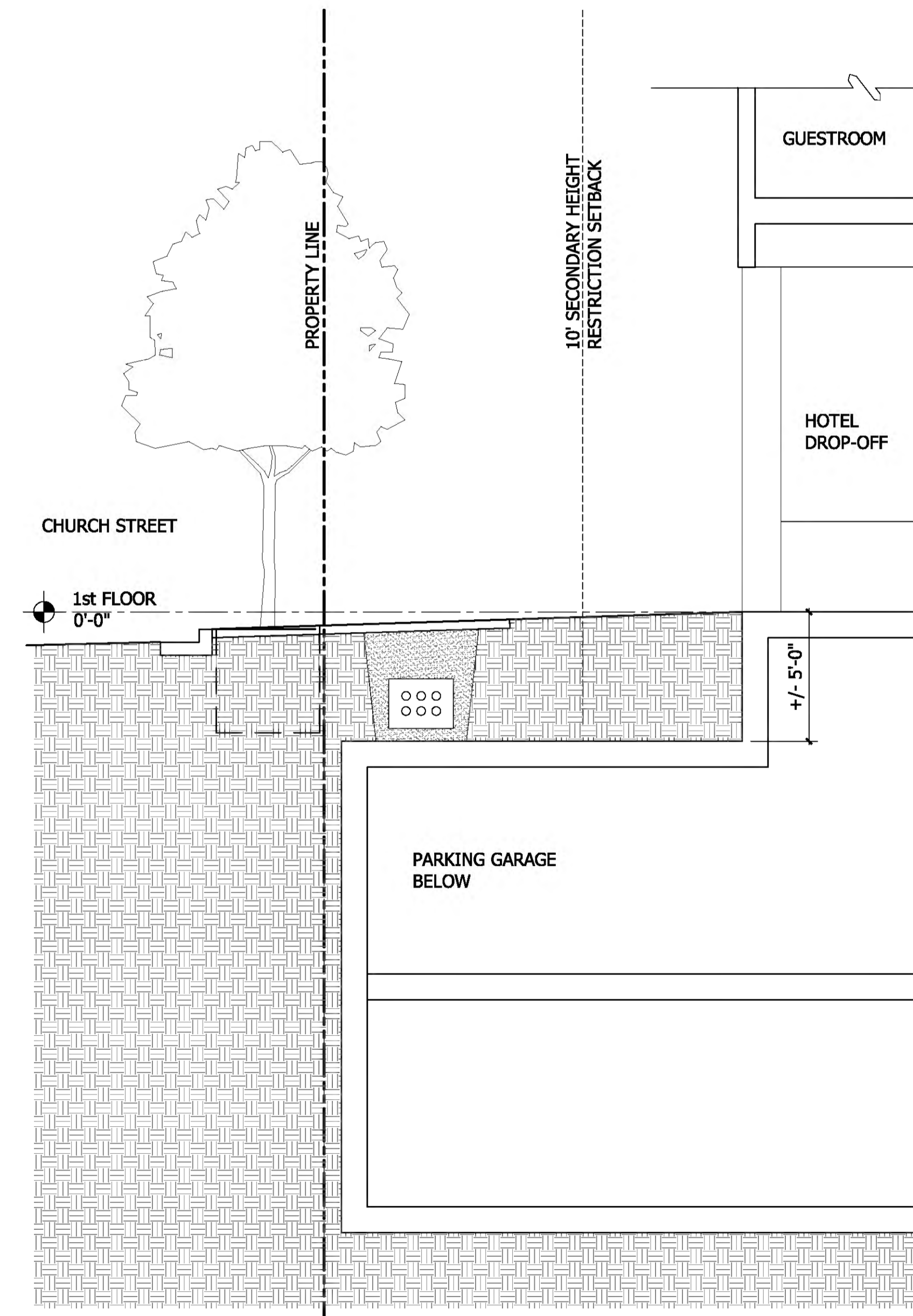
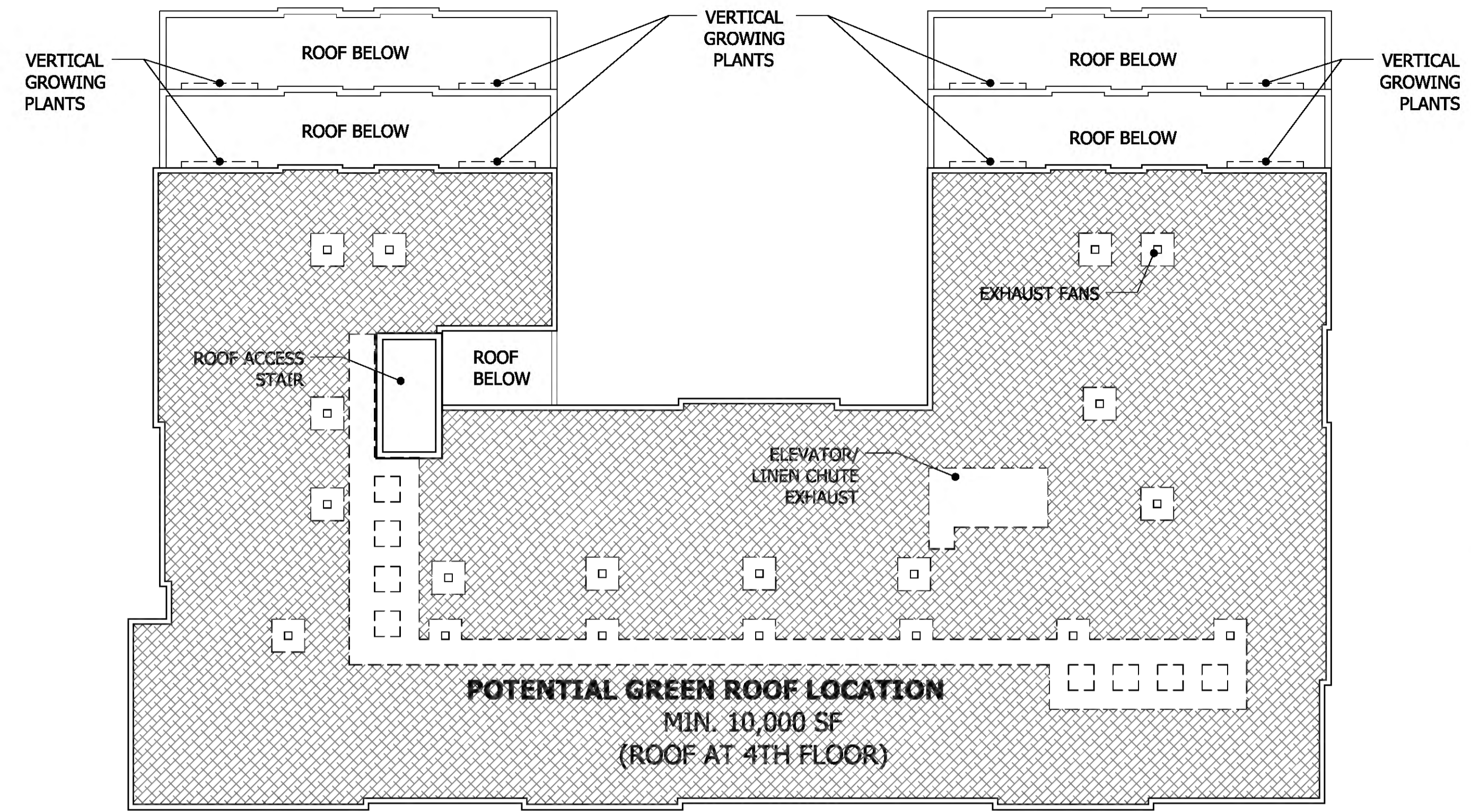
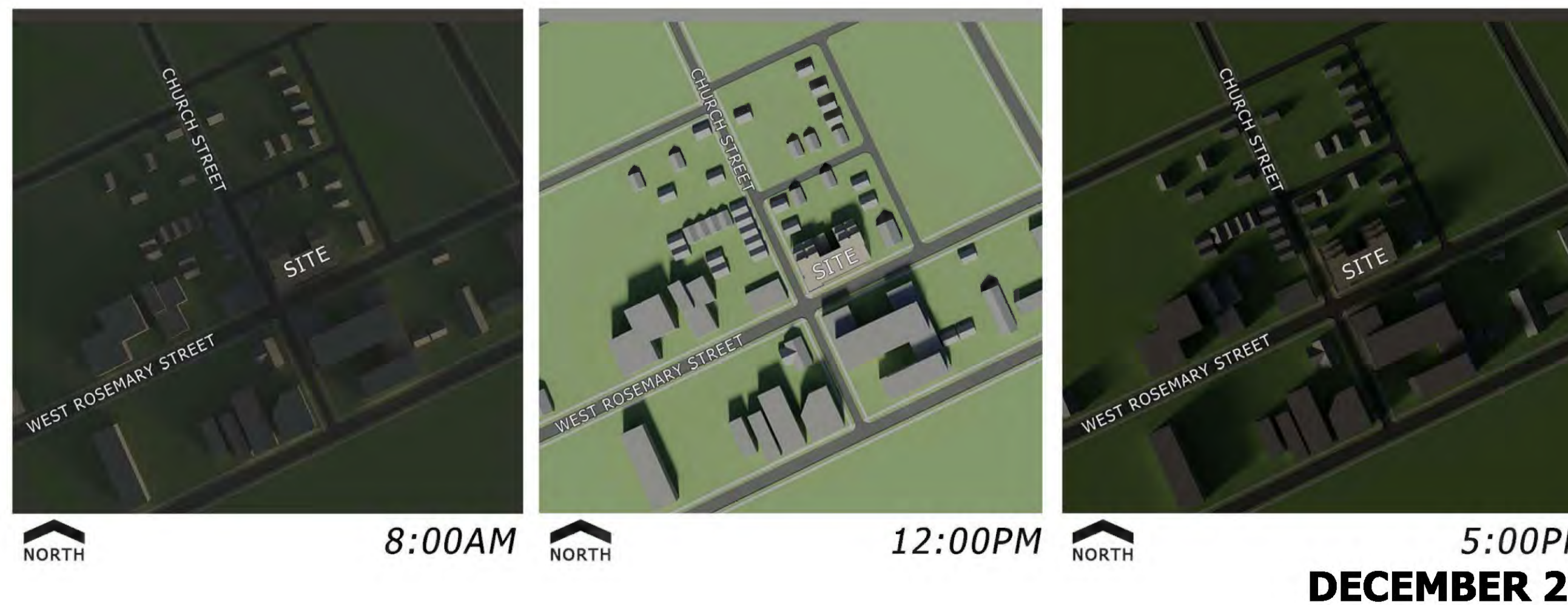
**03** ROOF DIAGRAM

scale: 1/16"=1'-0"



**01** CHURCH ST. STREETSCAPE SECTION

scale: 1/4"=1'-0"



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ISSUE	DATE
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SITE DETAILS/  
SUN STUDY/  
ROOF DIAGRAM

**A1.5a**

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**04** SOUTH BUILDING ELEVATION | NTS



**03** EAST BUILDING ELEVATION | NTS



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CHAPEL HILL, North Carolina



**02** RENDERING - ROSEMARY ST. | NTS

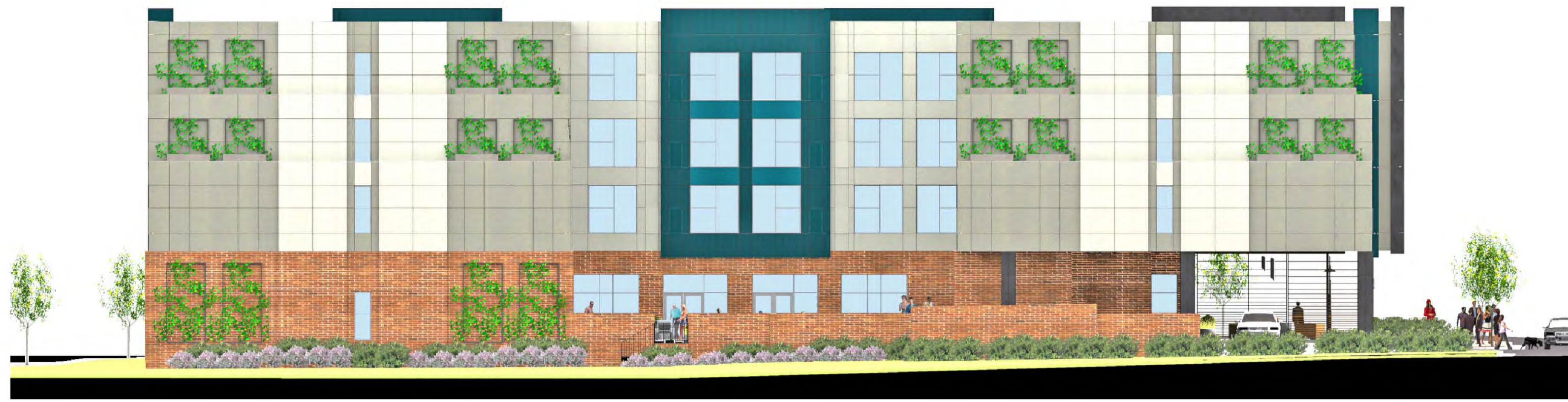


**01** RENDERING - CHURTH ST. AND ROSEMARY ST. | NTS

ISSUE	DATE
SUP SUBMITTAL	01/26/15
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ELEVATIONS/  
RENDERINGS





**04** NORTH BUILDING ELEVATION

NTS



**03** WEST BUILDING ELEVATION

NTS



**02** RENDERING - POOL/ COURTYARD

NTS



**01** RENDERING - NORTH ELEVATION

NTS



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ELEVATIONS/  
RENDERINGS

**A1.7**

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January 26, 2015

**AC Hotel by Marriott  
West Rosemary & Church Streets**

**CDC and Town Council Comments**

The following are questions or concerns that were expressed about the above mentioned project during Concept Plan review. Our response comments in response to these concerns are in **bold**.

**Community Design Commission – August 20, 2014**

---

1. Address the possibility of underground utilities.

**Comment: The developer is also interested in placing utilities along the project frontage underground. The existing power lines are 3-phase transmission lines. The utility company has initially expressed an inability to place the power lines below ground. The developer and consultants will continue to pursue the placement of the other utilities below ground.**

2. Screen service areas from neighbors in order to buffer noise from adjacent homes.

**Comment: The services area, while in the same location as the current, unscreened service area will be surrounded by landscape and at a minimum 8 foot tall fence. Final height and fence material (wood or masonry) dependent on discussions with neighbors.**

3. Consider moving houses, have them removed by others or fire department exercise to burn down.

**Comment: The developer takes no issue with the Fire Department or Others moving the existing structures off site. However, On the Developer's past projects timing has proven very difficult to effect moving a structure. The Developer intends to take possession 30-60 days prior to commencing construction. 30-60 days to plan, permit, and effect the structure's movement is not enough time.**

4. Partial to the idea of green roofs.

**Comment: Green roofs are proposed for the roofs of floors 1, 2 & 3 where the building steps back from the northern property line.**

The John R. McAdams  
Company, Inc.

Raleigh / Durham, NC  
2905 Meridian Parkway  
Durham, North Carolina 27713  
(919) 361-5000

Charlotte, NC  
11301 Carmel Commons Blvd,  
Suite 111  
Charlotte North Carolina 28226  
(704) 527-0800





West Rosemary Hotel  
OTO Development, LLC  
January 26, 2015  
Page 2 of 6

5. Would like to see the pool moved to an upper level (There is concern regarding public drunkenness around the pool and noise that may create/incur for neighbors).

**Comment: Moving the pool an upper level is difficult structurally and does not place the pool near other public areas as desired by the hotel. The pool has been moved closer to the building and further from the adjacent neighbor. Also, the pool hours are limited, via automatically operated locks, from 9 am to 9 pm to limit impact on the neighbors and guestrooms adjacent to the pool.**

6. There is concern regarding Rosemary Street – Please review again to provide more “excitement” – variability in wall line.

**Comment: Connecting pedestrians and the building wall begins at the western corner with the fire pit. The fire pit is an extension of the sidewalk and the building. The fire pit area will be defined with plants and architectural features. The connection continues past the fire pit with brick and window wall. The earthen color and texture of brick grounds the lobby level to Rosemary while the large glass wall invites pedestrians to see what is inside the hotel and bar patrons to be alongside Rosemary.**

7. Continue conversations with the Northside neighborhood.

**Comment: The Developer understands the importance of keeping the Northside neighborhood informed. The developer is working to present the projects current status at the neighborhoods’ February or March meeting.**

8. Provide further detail in regards to street trees.

**Comment: A hardscape / landscape design, including street trees has been included in this submittal package.**

9. Incorporate stone walls as currently exist on the site; buffer wall must be “high quality”.

**Comment: The current stone walls cannot remain in their current place. In coordination with the city the new sidewalk will be**

West Rosemary Hotel  
OTO Development, LLC  
January 26, 2015  
Page 3 of 6

**approximately twice as wide as the current sidewalk, similar to the opposite side of West Rosemary. Any buffer fence to be erected will be of high quality.**

10. Provide further information regarding plans for sustainability.

**Comment: The project will seek USGBC LEED certification for new construction. A small sample of the system employed on other LEED hotels by this developer includes LED light fixtures, low flow water fixtures, efficient glazing technology, recycling, and sourcing regional material.**

11. Anchor corner of building more so fire pit is not “pass thru” view to car drop off.

**Comment: The designers are researching different methods and techniques to separate the fire pit while still engaging the hotel and pedestrian traffic. The final design will keep the view from passing through to the car drop-off.**

12. Consider transformer location

**Comment: The design consultants are working with the local power utility for the optimal location which conforms to the utilities requirements.**

13. Show more context (adjacencies) in plan and sections in future presentation materials.

**Comment: Exhibits used for the presentations to the review boards will incorporate more information to put into context of the adjacent buildings and surrounding area.**

14. Use 3D Views to help with understanding of building when ready.

**Comment: Colored renderings of the building are being developed for presentation.**

West Rosemary Hotel  
OTO Development, LLC  
January 26, 2015  
Page 4 of 6

### **Public Comments**

---

1. Concerns regarding the scale of the project.

**Comment: Our building has created an intermediary scale between the established residential neighborhood to our north, and the Town Center Development to our south. We have respected all building setbacks and height restrictions given by the city, as well as maintained the intended pedestrian streetscape for Church Street and Rosemary Street. The building height is terraced away from the north façade, abutting residential properties, to create a less intrusive feel. The “U” shape form of the building also helps create privacy for the hotel guests as well as keeping all guest windows an appropriate distance from the residential properties. To the south, the building creates a strong front on Rosemary Street across from the current 8-story development.**

2. Prefer there are no walls between properties.

**Comment: Walls are provided to screen refuse facilities and as an alternative buffer on the remainder of the north side of the building. No wall is proposed on the east side.**

3. Concerned about balconies overlooking fine dining (“private” instead?) area.

**Comment: The hotel will not have balconies. The step backs on the north are not guest accessible. Only hotel staff will have access for maintenance of roof and green roof plantings.**

4. Request CDC help guide to appropriate architecture that is not “anywhere-ville” USA.

**Comment: The architect has attempted to create a clean, modern feel for the building while trying to infuse the vibrant energy of the university and the city with the use of color and texture. The building has blue inset accent panels around window frames and colored blue glass at our corner feature element. We have created a solid first floor with the use of brick. The brick helps ground the building while speaking to the current and historic fabric of the city. As a way to invite**



West Rosemary Hotel  
OTO Development, LLC  
January 26, 2015  
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**pedestrians and encourage an open feel, we have carved out a public seating area to take advantage of the streetscape at the corner of Church and Rosemary.**

**Town Council – October 20, 2014**

---

1. Ensure sidewalk width is comparable/conducive to alternate sites.

**Comment: Sidewalk area and street trees as proposed are comparable to the other sites on W. Rosemary.**

2. Consider murals on building's north side to soften/lessen impact on neighbors.

**Comment: A combination of landscaping and fence are being incorporated to soften the impact with northern neighbors. The hotel brand does not allow murals on the exterior of the building.**

3. Consider exhibition space in the hotel for local artists (example: Indigo Hotel, Athens, Georgia).

**Comment: The hotel brand reviews and approves art and décor for the hotel. Collecting and submitting each piece of art to be displayed in an exhibition space for brand review, comment, and approval will put an undue burden on the brand. The Interior Designer will be challenged to source and incorporate local artist(s) as a permanent installation.**

4. Is the pool a necessity? If not, additional buffer can be provided to neighbors and/or additional rooms can be added.

**Comment: The pool is a necessary amenity not only for swimming but aesthetics. The pool hours are limited between 9:00 am to 9:00 pm to limit impact on neighbors and hotel rooms that overlook the pool area.**

5. Provide step backs in building on east end.

**Comment: Step backs are provided on the north side adjacent to existing residential. Step backs are not provided on the east end, adjacent to commercial uses which are also within the TC-2 zoning**



West Rosemary Hotel  
OTO Development, LLC  
January 26, 2015  
Page 6 of 6

**district. Stepping back the building on the east end is not practical because of the impact on the floor area.**

6. With the SUP provide to the neighbors a more authentic rendering of the appearance of the building.

**Comment: Multiple renderings of differing views are being developed.**

7. If houses are not relocated, re-use or recycle the materials from these houses into the new building.

**Comment: As part of the LEED program, demolished material is sorted for recycle or repurpose.**

8. Do not include such components as neon or A/C units with grates through the walls.

**Comment: No neon lights or neon signs are proposed. The hotel will have signage lit with internal LED fixtures. Through wall PTAC units with exterior grills are not being used. Vertical VTAC units are being specified with a thin line louver. They will be painted to blend into the building materials.**

9. Discuss the left turn only egress being impractical.

**Comment: The developer intends to inquire with the Town transportation engineers on the potential design for a left turn only exit lane onto Church Street to discourage traffic from exiting through the Northside Neighborhood. But, due to space constraints it is likely that the left turn exit movement will only be able to be discourage with signage and no physical barrier will be able to be constructed.**

**WEST ROSEMARY STREET HOTEL**

**TRAFFIC IMPACT STUDY**

***EXECUTIVE SUMMARY***



Prepared for:

The Town of Chapel Hill  
Public Works Department  
Traffic Engineering Division

***Prepared by:***

***HNTB North Carolina, PC***

*343 East Six Forks Road  
Suite 200  
Raleigh, NC 27609*

*NCBELS License #: C-1554*

March 2015

**HNTB**

# WEST ROSEMARY STREET HOTEL

## TRAFFIC IMPACT STUDY

### EXECUTIVE SUMMARY



#### Prepared for:

The Town of Chapel Hill  
Public Works Department  
Traffic Engineering Division

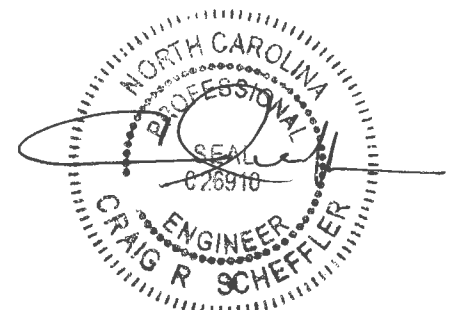
#### Prepared by:

**HNTB North Carolina, PC**

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NCBELS License #: C-1554

March 2015



3-9-15



## **EXECUTIVE SUMMARY - DRAFT**

### **Project Overview**

A commercial development known as the West Rosemary Street Hotel, located at the intersection of W. Rosemary Street and N. Church Street, is being proposed in Chapel Hill. The project proposes to construct a new four story 123-room hotel on three existing parcels along the north side of W. Rosemary Street. **Figure ES-1** shows the general location of the site. The project is anticipated to be fully complete by 2017. This report analyzes the complete build-out scenario for the year 2018 (one year after anticipated completion), the no-build scenario for 2018, as well as 2015 existing year traffic conditions.

The proposed site concept plan shows a single full movement access driveway along N. Church Street that would access an underground parking facility for hotel patrons, along with an adjacent drop-off driveway and a service driveway at the back of the property. No other vehicular access connections are proposed. **Figure ES-2** displays the preliminary concept plan of the West Rosemary Street Hotel and nearby land uses and roadways. The project is expected to provide 110 parking spaces in an on-site underground parking garage. This report analyzes and presents the transportation impacts that the West Rosemary Street Hotel will have on the following intersections in the project study area:

- NC 86 (N. Columbia Street) and Rosemary Street
- NC 86 (Columbia Street) and Franklin Street
- N. Church Street and W. Rosemary Street
- N. Church Street and W. Franklin Street / University Square Driveway
- N. Church Street and Proposed Site Driveway (parking garage access)

The impacts of the proposed site at the study area intersections will be evaluated during the AM, noon, and PM peak hours of an average weekday. The following study is based on background traffic for the existing year, 2015, the year following the estimated site build out year of 2017, as well as the estimated site-generated traffic produced by the proposed hotel, adjusted for the elimination of current site traffic produced by the existing land uses on the three parcels scheduled for redevelopment.

### **Existing Conditions**

#### **Study Area**

The site is located in downtown Chapel Hill along W. Rosemary Street in the northwest quadrant of its intersection with N. Church Street. The study area contains four signalized intersections along W. Rosemary Street and W. Franklin Street as they intersect N. Church Street and NC 86 (N. Columbia Street). NC 86 and Franklin Street are major arterial facilities providing connectivity between the UNC Main Campus/downtown area, Carrboro, and the region. Remaining study area network roadways are either collector streets or local neighborhood access streets.

#### **Site Traffic Generation**

With the addition of new “net” peak hour trips during the weekday AM, noon, and PM peak hours, there are potential site traffic impacts to the study area intersections. **Table ES-1** shows the site trip generation details, with generation rates and methodologies taken from the *Institute*





of Transportation Engineers (ITE) Trip Generation Manual, Version 9 and adjusted, as appropriate, for the estimated existing traffic volumes from the existing commercial development that the hotel will replace, as well as the effects of transit/pedestrians/bicycles.

**Table ES-1**  
**Weekday Vehicle Trip Generation Summary - West Rosemary Street Hotel**

Trip Generation Statistic	Daily			AM Peak Hour			Noon Peak Hour*			PM Peak Hour		
	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total	Enter	Exit	Total
New Site Raw Totals	503	503	1,006	38	27	65	38	32	70	38	36	74
Existing Development Traffic Volumes	262	262	524	7	2	9	25	22	47	23	21	44
<b>Net Site Traffic Added to the Study Area</b>	<b>241</b>	<b>241</b>	<b>482</b>	<b>31</b>	<b>25</b>	<b>56</b>	<b>13</b>	<b>10</b>	<b>23</b>	<b>15</b>	<b>15</b>	<b>30</b>

\* - No ITE Data Available, Assumed Average of AM/PM Peak Hour Data

**Background Traffic**

Background traffic growth for the 2018 analysis year is expected to come from two sources - ambient regional traffic growth and specific development-related traffic growth. Based on existing information, three Town-approved development projects (The Lux, 123 West Franklin, Carolina North – Phase 1) near the project study area is planned to be contribute to background traffic growth by the 2018 analysis year. All remaining estimated traffic growth in the area is assumed to occur due to overall region-wide ambient growth. To account for this, an ambient area-wide traffic growth percentage of 1.0 percent per year was applied to existing traffic volumes based on information from the historic daily traffic growth patterns in the project study (NCDOT and Town of Chapel Hill daily traffic information) and consistent with recent traffic impact studies completed near the project study area.

**Impact Analysis**

**Peak Hour Intersection Level-of-Service (LOS)**

Study results indicate existing traffic operations at all study area intersections are acceptable during the AM and noon weekday peak hours, but drop to a LOS E for two study area intersections in the PM peak hour. 2018 build-out year+1 background traffic growth impacts are mitigated by recommended signal retimings (from other recent development traffic impact studies) throughout the project study area, and it is anticipated that overall intersection performance should improve at the two intersections that exhibit existing PM peak hour operational issues. The addition of peak hour site-generated trips to the projected 2018 background traffic volumes, do not cause study area intersections to experience deficient traffic operations in any peak hour. A summary of the traffic operations for each intersection, related to vehicular delays (intersection average as a whole if signalized, critical movement if stop-controlled) and the corresponding Level-of-Service (LOS) is shown in **Table ES-2**.



**Table ES-2. West Rosemary Street Hotel  
 LOS and Delay (Seconds/Vehicle) Summary**

Intersections	Peak Hour	2015 Existing		2018 No-Build		2018 Build		2018 Mitigated	
		LOS	Delay	LOS	Delay	LOS	Delay	LOS	Delay
W. Rosemary Street & N. Church Street	AM	A	8.1	A	8.6	B	10.2	N/A	N/A
	NOON	B	13.5	B	13.0	B	13.4	N/A	N/A
	PM	B	15.5	B	15.7	B	17.0	N/A	N/A
W. Rosemary Street & NC 86 (N. Columbia Street)	AM	C	31.1	C	25.7	C	25.8	N/A	N/A
	NOON	D	37.3	C	26.6	C	26.7	N/A	N/A
	PM	<b>E</b>	<b>63.0</b>	C	33.6	C	33.7	N/A	N/A
Franklin Street & NC 86 (Columbia Street)	AM	D	41.3	D	39.6	D	39.8	N/A	N/A
	NOON	D	50.1	D	47.5	D	47.5	N/A	N/A
	PM	<b>E</b>	<b>78.2</b>	D	53.6	D	53.9	N/A	N/A
W. Franklin Street & N. Church Street / 123 W. Franklin Site Dr	AM	A	5.4	A	8.1	A	8.9	N/A	N/A
	NOON	A	9.1	B	10.5	B	11.2	N/A	N/A
	PM	A	9.7	B	10.6	B	10.7	N/A	N/A
N. Church Street & Proposed Site Driveway	AM	N/A	N/A	N/A	N/A	A*	9.2*	N/A	N/A
	NOON	N/A	N/A	N/A	N/A	A*	9.5*	N/A	N/A
	PM	N/A	N/A	N/A	N/A	B*	10.3*	N/A	N/A

N/A – Not Applicable or No Improvements Necessary

**BOLD/ITALICS** – Critical Movement or Overall Intersection Requires Mitigation Per Town TIS Guidelines

\* - Worst-Case LOS/Delay for Unsignalized/Stop-Controlled Critical Movement

**Access Analysis**

Vehicular site access is to be accommodated via a proposed two-way site driveway connection to N. Church Street that will directly access the proposed hotel’s structured parking facilities. Driveway throat length as shown on the proposed site development plans (90 feet) is adequate for projected 2018 traffic conditions. Based on capacity analysis results, there is little chance of substantial queuing occurring within the two-way aisle. There is an additional driveway connection for inbound drop-offs that is proposed, along with a service access driveway at the back of the site parcel – both connecting to N. Church Street.

Driveway distances from the signalized intersection at W. Rosemary Street and N. Church Street are acceptable (110 feet), based on recommendations of 100 foot minimum corner clearance as set forth in the 2003 *NC DOT Policy on Street and Driveway Access to North Carolina Highways* and the 2005 *Town of Chapel Hill Design Manual*.

Access for pedestrians and bicyclists is currently excellent, with continuous sidewalk on both sides of all study area roadways and crosswalk/pedestrian signalization at all signalized study area intersections. Bicycle access is well planned to and from the site (which will have dedicated bicycle storage spaces in the parking area). Wider travel lanes on several study area roadways, including W. Rosemary Street adjacent to the proposed site, are also in existence.



**Crash Analysis**

Data from the NCDOT Traffic Safety Unit was requested for the recent five-year period for all existing major study area intersections and the W. Rosemary Street corridor. Results show that the Rosemary Street corridor in the vicinity of the site experienced 35 crashes in the last five years, though most were located near the NC 86 (N. Columbia Street) intersection. Of the four project study area intersections, the Franklin Street/Columbia Street intersection experiences the most crashes and highest crash rates.

**Other Transportation-Related Analyses**

Other transportation-related analyses relevant to the 2001 Town of Chapel Hill Guidelines for the preparation of Traffic Impact Studies were completed as appropriate. The following topics listed in **Table ES-3** are germane to the scope of this study.

**Table ES-3. Other Transportation-Related Analyses**

Analysis	Comment
Long-Range Planning Level Daily Volume-Capacity Analysis	Since the proposed site will add less than 500 new “net” daily trips to the study area network, no long-range planning-level analyses of daily traffic impacts were conducted for this study.
Signal Phasing Analysis	Signal phasing changes (potentially changing the phase order for left-turn signal phases to lead or lag depending on the time of day) may enhance traffic operation for 2018 future conditions at the signalized intersections along N. Columbia Street.
Progression Analysis	All current signalized intersections in the project study area feature weekday peak hour coordinated operations. Adjustments to existing signal timings to improve progressed arterial movements along both Franklin Street and N. Columbia Street may enhance traffic progression in 2018 future scenarios.
Turn Lane Storage Requirements	Storage bay lengths at study area intersections were analyzed using Synchro and HCS 95 <sup>th</sup> percentile (max) queue length estimates for the 2018 Build Scenario. In most cases, existing storage for turn lanes is adequate in the project study area, and can be managed with signal timing adjustments. The eastbound left-turn at the NC 86 (N. Columbia Street) intersection with Rosemary street is currently only 100 feet – but 2018 PM peak hour conditions suggest it may require up to 250 feet. Restriping of this approach to provide that distance should be feasible with the existing roadway cross-section along Rosemary Street.
Appropriateness of Acceleration/Deceleration Lanes	Given the existing location and configuration of site driveway, and the lane geometrics, traffic patterns and posted speeds on N. Church Street, no special acceleration or deceleration lanes are required due to the proposed W. Rosemary Street Hotel development.
Pedestrian and Bicycle Analysis	Existing pedestrian access and connectivity is excellent through the study area. No specific bicycle amenities exist in the study area, although the existing width of Rosemary and Franklin Streets is conducive to bicycling activities.
Public Transportation Analysis	Public transportation service to the site is excellent, with an on-street bus stops located less than 200 feet to the east and west of the site and multiple routes serving the study area.



## **Mitigation Measures/Recommendations**

### **Planned Improvements**

There are no Town of Chapel Hill or North Carolina Department of Transportation improvement projects for study area roadway facilities within the analysis year time frame of 2015-2018.

### **Background Committed Improvements**

The 123 West Franklin project is expected to provide geometric and signal timing operational improvements to the W. Franklin Street/N. Church Street intersection (see **Figure ES-3**). Several other recent traffic impact studies for developments in or near the downtown area have recommended corridor signal retiming and/or geometric improvements for other study area intersections. For the purposes of this study, only the optimized signal timing improvements are expected to be completed in the 2015-2018 time frame.

### **Applicant Committed Improvements**

Based on the preliminary site concept plans and supporting development information provided, there are no external transportation-related improvements proposed adjacent to the West Rosemary Street Hotel – other than the removal of two existing access driveways along the W. Rosemary Street frontage.

### **Necessary Improvements**

Based on the 2018 design year peak hour intersection capacity analyses, none of the study area intersections are expected to be over capacity (overall LOS E or F) in any of the three weekday peak hours studied in this report. Two intersections, W. Rosemary St/N. Columbia St and Franklin St/Columbia St, are currently operating at a LOS E in the 2015 PM peak hour, due to high existing traffic volumes and some approaches and existing coordinated signal timings.

With the recommended retiming of the study area intersections as a potential improvement listed in other recent traffic impact studies for nearby study area developments (the Lux, 123 West Franklin, Carolina North Phase 1), overall delays likely to be experienced in the 2018 design year are projected to be less than the current 2015 peak hour analysis results.




- Retiming of the four study area traffic signals by the 2018 build-out year to optimize traffic progression, and to investigate the usage of lead/lag left-turn phasing where appropriate, should be implemented whether or not the West Rosemary Street Hotel is constructed and was considered to be implemented for the 2018 analysis year to compare whether or not additional mitigation due to West Rosemary Street Hotel site traffic was needed.

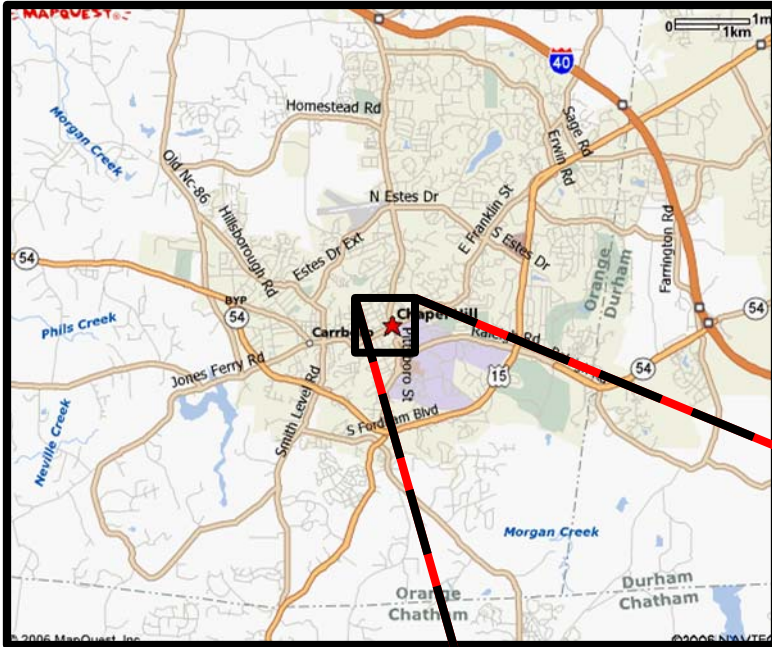
One recommended improvement, unrelated to intersection capacity analyses, is to lengthen the eastbound left-turn bay, via pavement marking upgrades, at the intersection of Rosemary Street and N. Columbia Street. This improvement would be necessary with or without site-related traffic from the West Rosemary Street Hotel development. Estimated 95<sup>th</sup> percentile queue lengths for this movement are up to 250 feet (PM peak hour) in the 2018 – With Site Traffic scenario. Field observation and review of existing aerial imagery of the W. Rosemary Street corridor indicate that adequate pavement width exists beyond the existing 100 foot eastbound left-turn bay to adjust pavement markings without geometric widening. This improvement would improve the overall efficiency of the intersection and is shown in **Figure ES-3**.

- This improvement is recommended whether or not the West Rosemary Street Hotel project is constructed.



**LEGEND**

-  = Existing Study Area Intersections
-  = Proposed Site Driveway
-  = Existing Building Footprints

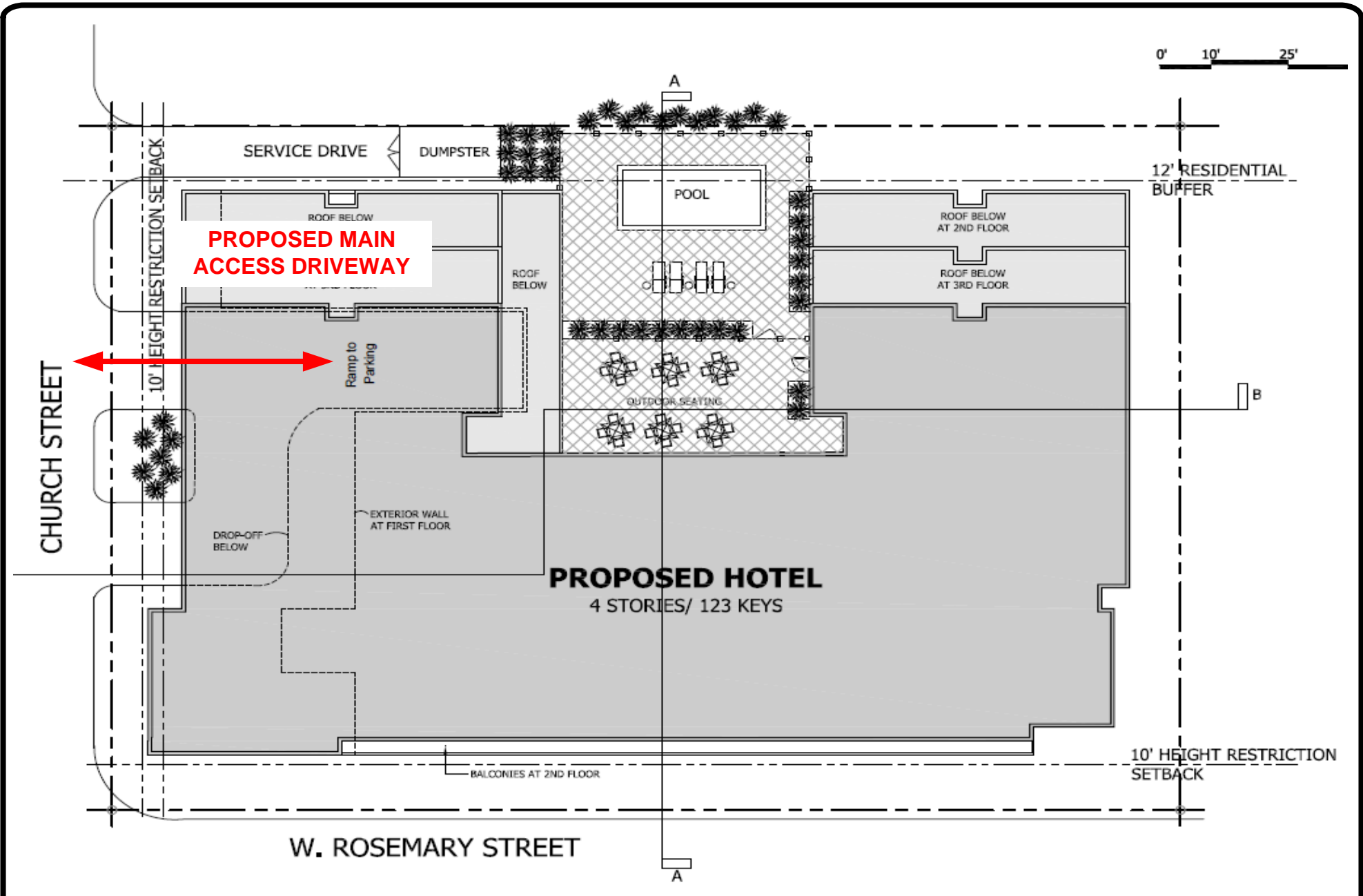


**West Rosemary Street Hotel  
Traffic Impact Study**

SITE LOCATION MAP

DATE: March 2015

**FIGURE ES-1**



**HNTB**



**West Rosemary Street Hotel  
Traffic Impact Study**

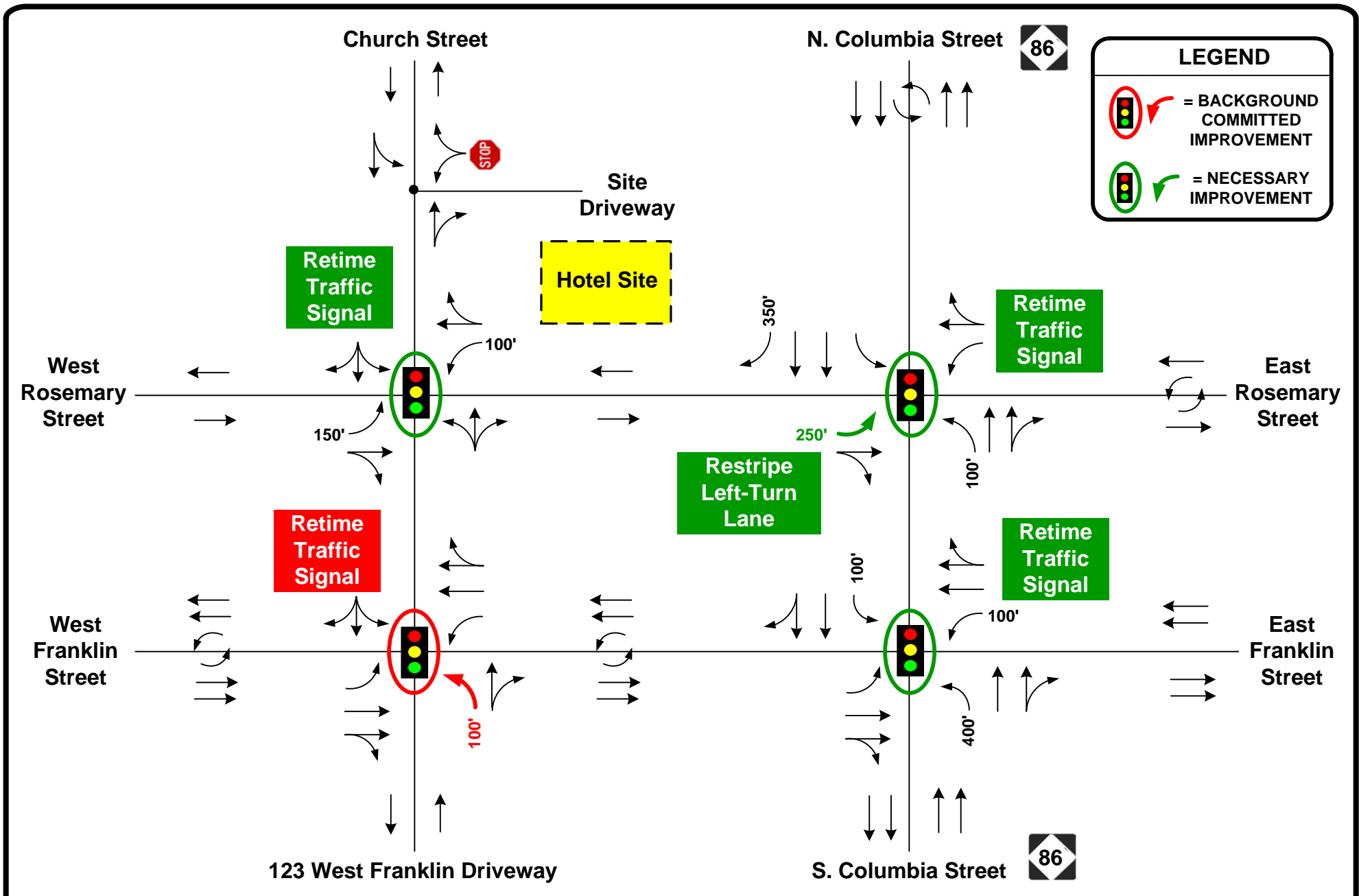
PRELIMINARY SITE PLAN



**NOT  
TO  
SCALE**

DATE: March 2015

**FIGURE ES-2**



# BOLAN SMART ASSOCIATES, INC.

1150 K STREET, NW, SUITE 1211, WASHINGTON, DC 20005 • (202) 371-1333

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April 2015

**RE: Fiscal Impact Analysis** (*per Special Use Permit Application Submittal Requirements*)  
**AC by Marriott, Chapel Hill, NC**

Bolan Smart Associates (BSA) has been asked to provide an overview of the fiscal impact to the Town of Chapel Hill (ToCH) associated with the development of a 68,000 gsf, 123 room, 112 parking space hotel. The on-site parking, food & beverage and other non-room spaces are all intended primarily to support hotel guest use. Construction is anticipated to take 18 months beginning in 2016. The building program and hotel operating assumptions have been provided by the project sponsor, OTO Development. The estimated municipal revenues and costs are based on assumptions prepared by Bolan Smart Associates, compiled from a combination of review of other recent ToCH fiscal impact analysis, discussion with Town staff, and BSA's background with fiscal impact analysis in other jurisdictions. Other than for a Town requested \$3,000 cost to reprogram traffic signals, it is assumed there are no upfront Town capital costs required for infrastructure or other site development support.

**A. ToCH Direct Annual General Fund Revenues (\$2015)** The principal direct general fund tax revenues to the Town of Chapel Hill resulting from the completion of this project total approximately \$229,000 per year (calculated in \$2015). The primary components, as summarized in Table 1, are estimated to include:

- 1) \$76,400 per year in real and personal property taxes;
- 2) \$151,000 in hotel use related taxes, derived from \$4,882,000 in room revenues and \$673,000 in primarily hotel guest limited food & beverage service and other revenues; and
- 3) \$1,700 in parking taxes from a 112 space parking component. ; and

Note that in addition to the on-site project direct revenues there are indirect Town of Chapel Hill tax revenues generated by virtue of the addition of a new downtown hotel, including off-site retail sales supported by hotel guests and hotel employees.

**B. Other ToCH Property Tax Revenues (\$2015)** (See Table 1)

Transit Fund	\$10,000
Debt Service Contribution	\$16,700
Downtown Service District	<u>\$14,000</u>
Total Other ToCH	\$40,700



**C. One-time Development Related Revenues** The combination of permits, tap fees, inspections, recordation fees and other development and initial occupancy municipal costs are budgeted at approximately \$825,000. This is at the lower end of the \$5.00 to \$10.00 per gsf of building area that is typically part of the development budget for a major, urban form commercial development. Assuming 85% of these one-time development costs are retained entirely by the Town of Chapel Hill, the net revenue approximates \$700,000. In addition, \$48,350 in estimated sales tax collected on construction materials are projected, producing a total one-time tax project direct revenue ToCH impact on the order of \$750,000.

**One-time Fees (2015 to 2017)**

permits, tap fees, inspections, recordation fees, etc. \$700,000 ToCH (85% of \$825,000 total)

**Construction Materials Sales Tax Revenue**

\$16,000,000 construction cost x 55% materials cost x 80% locally sourced taxed at 0.6870% TOCH sales tax rate, realizing \$48,350 in one-time construction materials sales tax.

**D. Overlapping Jurisdictions Direct Annual Tax Revenue (\$2015)** (See Table 1)

Orange County Real and Personal Tax Revenue	\$172,800
School District Real and Personal Tax Revenue	\$41,000
Orange County Occupancy Tax	\$146,500
Orange County Other Hotel Guest Sales Tax	\$11,200
Orange County Parking Revenue Tax	\$4,100
Other Non-Chapel Hill Tax and Fee Revenues	not considered
Total Other Municipal Related Annual Taxes	\$375,600

**E. Municipal Costs (\$2015)** The ToCH Special Use Permit (SUP) Application Submittal Requirements asks for a breakdown of the costs for providing Town services. Bolan Smart has reviewed two prior fiscal impact reports prepared in 2014 for The Edge and Obey Creek. These other examples are for large and complicated mixed use developments in different settings. Respectful of the many differences between the subject application and the other precedents, Bolan Smart has attempted to craft a cost allocation methodology specific to a new downtown hotel.

To meet the SUP Submittal requirements, two sets of municipal cost assumptions need to be established. One is to categorize the likely cost impacts, and the other is to assign some kind of monetary cost metrics.

Cost Category Impacts: The following provides an illustration of likely incremental impact on Town service costs compared with other types of land uses found in Chapel Hill. The key premise is to evaluate to what degree the existing civic infrastructure and provision of services will require further municipal expense in order to maintain current overall levels of performance.

<u>Service Area</u>	<u>Incremental Cost Impact</u>
Solid waste collection	<i>none, hotel contracted</i>
Street maintenance	<i>limited, no added impact beyond immediate streetfront. (Note project will fund adjacent sidewalk improvements.)</i>
Stormwater	<i>limited, on-site retention system</i>
Parks & Recreation	<i>minor</i>
Library Services	<i>minimal</i>
Police Services	<i>some, offset partly by special event on-site funded security</i>
Fire Services	<i>some</i>
Planning/Inspections	<i>yes</i>
General Government	<i>some</i>
Public Transportation/Transit	<i>some (assumed at \$4,000, or 50% of Transit Fund Revenue)</i>
Debt Service	<i>very limited (assumed at 20% of Debt Service Revenue)</i>
Downtown Service District	<i>some (assumed at 100% of Service District Revenue)</i>

Municipal Cost Source: Given the imprecision of allocating costs from a town-wide budget for the above service items, the following approach for assessing a per unit cost for a hotel is suggested. The \$58.3 million Town General Fund supported costs are more or less allocated in the following bundles:

<u>Town Data Point</u>	<u>General Fund Budget Assumption</u>	<u>Implied Cost per Unit</u>
60,000 Residents	<i>65%, or \$37.9 million</i>	<i>\$630 per resident</i>
30,000 Jobs & University	<i>35%, or \$20.4 million</i>	<i>variable measure</i>

Hotel Cost Equivalent: Assuming that one year of hotel nights represents 100% the cost of a Chapel Hill resident means that one annualized hotel room night costs \$630 in overall unallocated Town services. With a total number of annualized room nights of 92 (123 rooms at 75% occupancy), this suggests there would be an annual Town general fund cost of \$58,000 for the proposed hotel.

**F. ToCH Net Fiscal Impact (\$2015)**

General Fund Hotel Revenue	\$229,000
General Fund Cost	<u>\$58,000</u> (per above)
Net Impact	\$171,000

The above estimate is grounded in a formula that employs cost averaging across the spectrum of existing municipal servicing expenses. Were an incremental approach to be tracked, where the added service demand is on top of the existing level of service, it is likely that the marginal cost of accommodating the actual extra requirements imposed by a new hotel would be much less than were the costs to be reallocating on based on cost averaging.

Transit Fund Revenue	\$10,000
Transit Fund Cost (50% of revenue)	<u>\$5,000</u> (Bolan Smart assumption)
Net Impact	\$5,000

Debt Service Revenue	\$16,700
Additional Debt Service (20% of revenue)	<u>\$3,350</u> (Bolan Smart assumption)
Net Impact	\$13,350

Downtown Service District Revenue	\$14,000
Service District Cost	<u>\$14,000</u> (helps support hotel guest in lieu of Town services)
Net Impact	\$0

Overall TOCH Revenue	\$270,000
Overall Cost	<u>\$78,000</u>
Overall Net Impact	\$192,000

In sum, a new upscale hotel in downtown Chapel Hill represents a substantial gain of net revenue for the Town of Chapel Hill.

**Table 1 - AC Hotel, Chapel Hill, NC  
ANNUAL DIRECT DC TAX REVENUE NET OF MULTIPLIERS - \$2015**

**Project Description**

**Direct Project Full Time Employment (a)**

1) Hotel Space	68,000 gsf (net of parking)		
2)	123 rooms (550 gsf / room)	0.40 jobs / room	49 jobs
3) Food Service	included above	included above	
4) Other Spaces	included above	included above	
5) Parking	112 spaces	included above	

**Real and Personal Property Tax**

6) General Real Estate Value	\$235 per gsf	\$130,000	\$15,990,000
7) Parking (included in hotel)			NA
8) Business Personal Property	\$30,000 FF&E per room	\$30,000	\$3,690,000
9)	\$289 per gsf	\$160,000	\$19,680,000
10) Total Taxable Value (Franklin / Hampton approx \$160k / room)	100% of value	\$160,000	\$19,680,000
11) <b>ToCH Real and Personnel Tax General Fund Revenue</b>	0.3880% general fund tax rate	\$621	<b>\$76,358</b>

**Hotel Direct Tax Revenues**

12) Room Nights	75% occupancy	274	33,671
13) Room Revenue (net of parking)	\$145 average daily rate	\$39,694	\$4,882,331
14) ToCH Occupancy Tax	3.0000% tax rate	\$1,191	\$146,470
15) Hotel Guest Expenditures (food & beverage, other services)	\$20 per occupied room night	\$5,475	\$673,425
16) ToCH Other Hotel Guest Sales Tax	0.6870% tax rate	\$38	\$4,626
17) Gross Revenue (net of parking)		\$45,169	\$5,555,756
18) Other Corporate, Operating Taxes and Fees	not considered	\$0	\$0
19) <b>Total Direct Hotel Use Related General Fund Revenue</b>		\$161,228	<b>\$151,096</b>

**Parking**

20) Parking Income	\$8.00 per space per room night		
21)	\$2,190 per space per year	\$2,190	\$245,280
22) <b>ToCH Parking Tax General Fund Revenue</b>	0.6870% of gross revenue	\$15	<b>\$1,685</b>

**Employee Related Sales Tax Revenue**

23) FTE Employee Retail Expenditures Subject to Sales Tax	not considered		
24) <b>ToCH Employee Related Sales Tax General Fund Revenue</b>			

**Total Annual General Fund Tax Revenue**

25) <b>ToCH General Fund Related Revenues</b>	\$1,863	<b>\$229,140</b>
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**Other ToCH Direct Annual Property Tax Revenue**

26)	0.0510% transit fund tax rate	\$82	\$10,037
27)	0.0850% debt service tax rate	\$136	\$16,728
28)	0.0710% downtown service district	\$114	\$13,973
29) <b>Total Other ToCH Real &amp; Personal Property Tax Revenue</b>		\$331	<b>\$40,738</b>

**Total ToCH Annual Tax Revenue**

	\$2,194	<b>\$269,877</b>
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**Non-ToCH Municipal Annual Taxes**

30) Orange County Real and Personal Tax Revenue	0.8780% tax rate	\$1,405	\$172,790
31) School District Real and Personal Tax Revenue	0.2084% tax rate	\$333	\$41,013
32) Orange County Occupancy Tax	3.0000% tax rate	\$1,191	\$146,470
33) Orange County Other Hotel Guest Sales Tax	1.6660% tax rate	\$91	\$11,219
34) Orange County Parking Revenue Tax	1.6660% tax rate	\$33	\$4,086
35) Other Non-Chapel Hill Tax and Fee Revenues	not considered		
36) <b>Total Other Municipal Related Annual Taxes</b>		\$3,053	<b>\$375,579</b>

Notes:

(a) FTE is full time equivalent employee, assumed at 40 hours per week.

**Rosemary Street Hotel**  
# 212, 214, 220  
W. Rosemary St

1000' Notification Line

**AC HOTEL BY  
MARRIOTT**

